

## **REVISED**

**TO:** HEARINGS OFFICER

**FROM:** LISA ANDERSON-OGILVIE, AICP, DEPUTY COMMUNITY  
DEVELOPMENT DIRECTOR AND PLANNING ADMINISTRATOR

**SUBJECT:** **CONDITIONAL USE/CLASS 3 SITE PLAN REVIEW/CLASS 2  
DRIVEWAY APPROACH PERMIT CASE NO. CU-SPR-ADJ18-05  
4227 LONE OAK ROAD SE  
AMANDA NO. 18-106940-ZO & 18-106941-RP**

### **REQUEST**

Summary: A conditional use, site plan review, and driveway approach permit application for building additions, removal of a driveway, modification of a driveway, and reconfiguration of parking areas for a religious assembly.

Description: Conditional Use, Class 3 Site Plan Review, and Class 2 Driveway Approach Permit applications for construction of additions to the north and south elevations of a church structure, removal of a driveway, modification of a driveway, addition of parking areas, and reconfiguration of existing parking areas serving an existing religious assembly, on an 8.5-acre property in the RM2 (Multiple Family Residential) zone, at 4227 Lone Oak Road SE 97302 (Marion County Assessor Map and Tax Lot 083W10BB06200).

The Conditional Use is requested because the modified driveway on Friendship Avenue SE would exceed the maximum distance of 125 feet from Lone Oak Road SE that is allowed by the special use standards for a religious assembly in the RM2 zone

A vicinity map illustrating the location of the property is attached hereto, and made a part of this staff report (Attachment A).

**OWNER:** Queen of Peace Catholic Church

**APPLICANT:** Queen of Peace Catholic Church

**FILER:** Gretchen Stone, CB Two Architects

### **RECOMMENDATION**

Based upon the Facts and Findings contained in this staff report, staff recommends that the Hearings Officer APPROVE the request for a conditional use, site plan review, and driveway approach permit for construction of additions to the north and south elevations of a church structure, removal of a driveway, modification of a driveway, addition of parking areas, and reconfiguration of existing parking areas serving an existing religious assembly on property located at 4227 Lone Oak Road SE subject to the following conditions of approval:

**Condition 1:** The development shall be in substantial conformance with the site plan included as Attachment C of the staff report, as modified by conditions of approval.

**Condition 2:** Provide a landscaped setback of at least 15 feet between the new parking lot pavement and the north property line, landscaped with a minimum of one plant unit per 20 square feet and screened with a minimum 6-foot-tall sight-obscuring fence, wall, or hedge.

**Condition 3:** Remove the proposed triangular planting areas in the reconfigured south parking lot.

**Condition 4:** The eight new parking spaces in the northern row of the expanded north parking lot shall be designated as compact spaces.

**Condition 5:** The 20-foot-wide "Fire Lane" depicted on the site plan between the north and south parking areas shall be widened to a minimum of 22 feet to accommodate two-way traffic.

**Condition 6:** Bumper guards or wheel barriers shall be installed in the eight new parking spaces in the northern row of the expanded parking lot and all of the parking spaces abutting the proposed sidewalk that is oriented east to west through the reconfigured area of the southern parking lot.

**Condition 7:** Construct street lights, street trees, sidewalks, and curb ramps along the entire frontages of Lone Oak Road SE and Friendship Avenue SE where not compliant with Public Works Design Standards.

**Condition 8:** Design and construct a storm drainage system for the new and replaced impervious surfaces in compliance with SRC Chapter 71.

### **APPLICATION PROCESSING**

On March 23, 2018, the applicant submitted an application for a conditional use permit, Class 3 Site Plan Review, and Class 2 Driveway Approach Permit. The application was deemed complete for processing on June 19, 2018.

The public hearing before the City of Salem Hearings Officer is scheduled for July 11, 2018, at 5:30 p.m. Notice of public hearing was sent by mail to surrounding property owners pursuant to Salem Revised Code (SRC) requirements on June 21, 2018. Public hearing notice was also posted on the property by the applicant pursuant to SRC requirements.

### **BACKGROUND**

The subject property is developed with a church, administrative offices, elementary school, and ancillary common use spaces such as a gymnasium and covered outdoor

spaces. An existing conditions plan is included as Attachment B. The applicant proposes construction of additions to the north and south elevations of a church structure, removal of a driveway, modification of a driveway, addition of parking areas, and reconfiguration of existing parking areas. The proposed site plan is included as Attachment C.

## **PROPOSAL**

The applicant is requesting to construct a 5,192-square foot addition to the community room on the south side of the church building and a 4,078-square foot addition to the north side of the church building for a youth center, reconfigure parking areas to reduce driveway access on Friendship Avenue SE, add parking on the north side of the church, and enhance pedestrian paths and vehicle drives that facilitate better and more efficient connectivity between parking areas on the north and south sides of the church.

Religious assembly uses in the RM2 (Multiple Family Residential 2) zone are subject to Special Use standards of SRC (Salem Revised Code) 700.050. Where a proposal does not meet the Special Use standards, a Conditional Use permit is required. In this case, the Conditional Use permit is triggered because the proposal will remove one legally nonconforming driveway on Friendship Avenue SE and modify a second driveway on Friendship Avenue SE which does not meet the Special Use standard of a maximum distance of 125 feet from a collector street.

## **SUMMARY OF RECORD**

The following items are submitted to the record and are available upon request: All materials submitted by the applicant, including any applicable professional studies such as traffic impact analysis, geologic assessments, and stormwater reports; any materials and comments from public agencies, City departments, neighborhood associations, and the public; and all documents referenced in this report.

## **APPLICANT'S STATEMENT**

The applicant's statement addressing the applicable approval criteria for the conditional use request is included as Attachment D.

## **FACTS AND FINDINGS**

### **1. Salem Area Comprehensive Plan (SACP) designation**

The Salem Area Comprehensive Plan (SACP) map designation for the subject property is "Multi-Family Residential." The subject property is within the Urban Growth Boundary and the Urban Service Area.

### **2. Zoning and Surrounding Land Uses**

The subject property is zoned RM2 (Multiple Family Residential 2). Religious Assembly uses are Special Uses in the RS zone. SRC Chapter 700.005(d)

provides that modifications to special use standards shall be made pursuant to Conditional Use approval, as provided in SRC Chapter 240, and in no case shall Conditional Use approval allow a standard that is less restrictive than the development standards established in the zone or overlay zone. The requested Conditional Use permit is required for a driveway on Friendship Avenue SE within 125 feet of a Collector street.

The zoning of surrounding properties is as follows:

North: RS (Single Family Residential);

East: Across Lone Oak Road SE, RS (Single Family Residential);

South: Across Friendship Avenue SE, RS (Single Family Residential);

West: Across Pullman Avenue SE, RS (Single Family Residential).

### **3. Site Analysis**

The subject property consists of a lot approximately 600 feet wide from east to west and 620 feet from north to south. The lot is bounded on the east by Lone Oak Road SE, designated as a Collector in the Transportation System Plan. The subject property has street frontage on Friendship Avenue SE to the south and Pullman Avenue SE to the west, and Alana Avenue SE terminates on the north side of the property. The property has two driveways on Lone Oak Road SE and two driveways on Friendship Avenue SE, as well as a nonconforming parking area on Pullman Avenue SE (Attachment B).

The footprint of the existing church is near the center of the site, and the school and administrative buildings are in the southwestern area. Accessory buildings are located in the southwestern and northeastern areas. Open space and playground facilities are located in the northwestern area. Parking lots are located north, west, and south of the church building. The parking lots contain trees and other landscaping.

### **4. Neighborhood and Citizen Comments**

The subject property is located within the Faye Wright Neighborhood Association (Faye Wright). Notice was provided to Faye Wright and surrounding property owners within 250 feet of the subject property and adjoining lot under common ownership.

As of the date of this staff report, no comments have been received from Faye Wright or surrounding property owners.

## **5. City Department and Public Agency Comments**

The Salem Building and Safety Division reviewed the proposal and identified no issues.

The Salem Fire Department reviewed the proposal and commented,

It appears fire department access is being addressed, but Fire was not able to verify turn radii. Fire department access and water supply will be required. Items including these will be addressed at time of building permit plan review.

Fire department access is required to be provided to within 150 feet of all portions of the structure. Fire department access is required to be a minimum of 20 feet wide and 13-6 feet clearance in height. Turn radius are 28 feet inside and 48 feet outside. Fire will need verification of these dimensions (turn radii on the north and south ends of the fire lane shown on the west side of the project).

The Public Works Department reviewed the proposal and provided a memo which is included as Attachment E.

Portland General Electric (PGE) reviewed the proposal and indicated that development costs are determined by current tariff and service requirements, and PGE facilities will need to be relocated for the project.

## **6. Analysis of Conditional Use Criteria**

Religious Assembly uses and Basic Education uses are subject to special use standards in the RM2 (Multiple Family Residential 2) zone. SRC Chapter 700.005(d) provides:

Modifications to special use standards shall be made pursuant to Conditional Use approval, as provided in SRC Chapter 240, and in no case shall Conditional Use approval allow a standard that is less restrictive than the development standards established in the zone or overlay zone.

The proposed development requires Conditional Use approval because it does not meet the Special Use standard for a religious assembly for maximum distance from a driveway to a Collector street.

SRC Chapter 240.005(a)(1) provides that:

No building, structure, or land shall be used or developed for any use which is designated as a conditional use in the UDC unless a conditional use permit has been granted pursuant to this Chapter.

SRC Chapter 240.005(d) establishes the following approval criteria for a conditional use permit:

Criterion 1:

The proposed use is allowed as a conditional use in the zone.

**Applicant's Statement:** A Conditional Use permit for the remaining reconfigured driveway on Friendship is required for this development proposal due to the distance the driveway is from Lone Oak, a collector street. Currently there is a Conditional Use Permit in place for both of the driveways, since one of them is being removed and the other is being widened a new Conditional Use is appropriate given the proposed changes. This driveway (as well as two others) was allowed as a conditional use in a previous land use decision CPC/ZC93-14. In an effort to reduce the degree of nonconformance and improve circulation in vehicle use areas as well as access from Friendship one of the allowed driveways will be removed and the remaining driveway will be widened. The location of the remaining driveway does not change as a result of this request.

**Finding:** The Conditional Use permit is required in order for the remaining driveway on Friendship Avenue SE to deviate from the Special Use standard requiring a maximum distance of 125 feet from the collector street, Lone Oak Road SE.

The previous Comprehensive Plan Change / Zone Change Case No CPC/ZC93-14 contained conditions of approval limiting the property to two driveways onto Lone Oak Road SE, two driveways onto Friendship Avenue SE, and one driveway onto Pullman Road SE south of Chase Avenue SE; prohibiting vehicle access from Alana Avenue SE and from Pullman Road SE north of Chase Avenue SE; and limiting use of the property to elementary and secondary schools and religious organizations. While the conditions of the CPC/ZC93-14 decision limited the number of driveways, the decision did not constitute a previous Conditional Use approval for the driveways on Friendship Avenue SE to deviate from current Special Use standards. Therefore, a new Conditional Use approval is required for the proposed location of the remaining driveway on Friendship Avenue SE.

Staff finds that the proposal meets this criterion.

Criterion 2:

The reasonably likely adverse impacts of the use on the immediate neighborhood can be minimized through the imposition of conditions.

**Applicant's Statement:** As noted previously, this driveway already exists and is allowed as a conditional use; therefore no change or increased impacts to the immediate neighborhood are expected. Rather, it is our belief that adverse impacts will be minimized with this proposal as the degree of non-conformance

will be lessened by the removal of one of the previously approved driveways. In addition, improvements to the remaining driveway allow for better maneuvering and access, which is encouraged by City of Salem Public Works.

**Finding:** As explained in the Finding for Criterion 1, the driveway is one of two allowed onto Friendship Avenue SE under the conditions of Comprehensive Plan Change / Zone Change Case No CPC/ZC93-14, but no previous Conditional Use permit was approved for the driveway to exceed the Special Use standard of 125 feet from the Collector street.

Staff concurs that the proposed redevelopment with a single driveway onto Friendship Avenue SE minimizes adverse impacts, which are primarily related to traffic and safety.

Both existing driveways are nonconforming with the maximum 125-foot distance from Lone Oak Road SE. The existing driveway is 23.9 feet wide and located near the center of the block, approximately 345 feet from Lone Oak Road SE. The driveway that will be removed is 23.9 feet wide and located approximately 155 feet from Lone Oak Road SE.

The modified driveway will be 40 feet wide and 323'7" from Lone Oak Road SE. Reducing the number of driveways onto the local street, Friendship Avenue SE, reduces the potential points of conflict and improves safety for pedestrians and bicyclists.

In order to ensure that the impacts are mitigated as proposed, staff recommends the following condition:

**Condition 1:** The development shall be in substantial conformance with the site plan included as Attachment C of the staff report, as modified by conditions of approval.

Staff finds that the proposal meets this criterion as conditioned.

Criterion 3:

The proposed use will be reasonably compatible with and have minimal impact on the livability or appropriate development of surrounding property.

**Applicant's Statement:** The proposed improvements to the existing driveway are compatible with the development and are appropriate. No adverse "impacts to the livability or appropriate development of surrounding properties" are anticipated with this proposal.

**Finding:** The subject property has been developed as a religious assembly and elementary school for many years. The current church building, parking lots, and driveways were developed prior to 1995. The elimination of one driveway and

reconfiguration of another will be compatible with and have minimal impacts on the surrounding properties.

Staff finds that the proposal meets this criterion.

## **7. Analysis of Class 3 Site Plan Review Approval Criteria**

SRC 220.005(f)(3) establishes the following approval criteria for a Class 3 Site Plan Review:

### Criterion 1:

The application meets all applicable standards of the UDC.

**Finding:** The proposed development includes building additions and parking lots for a religious assembly use. The proposed development, with conditions of approval, complies with all applicable development standards of the Salem Revised Code.

### **Development Standards – RM2 Zone:**

#### *SRC 514.005(a) - Uses:*

The permitted, special, conditional and prohibited uses in the RM2 zone are set forth in Table 514-1.

**Finding:** The proposed development includes building additions, parking lot expansions, and parking lot reconfigurations for an existing religious assembly use. A religious assembly use is classified as allowed in the RM2 zone per SRC 514.005, Table 514-1 as a Special Use, subject to the standards of SRC 700.050. Findings for the Special Use are included later in this section.

The subject property also includes an elementary school, which is a Special Use, subject to the standards of SRC 700.010. No changes are proposed to the elementary school.

#### *SRC 514.010(b) – Lot Standards:*

In the RM2 zone, for a religious assembly use, the minimum lot area is 6,000 square feet; the minimum lot width is 40 feet; the minimum lot depth is 120 feet for double frontage lots; the maximum lot depth is 300 percent of average lot width; and the minimum street frontage is 40 feet.

**Finding:** The subject property complies with the applicable lot standards of the RM2 zone, with lot area exceeding 368,800 square feet and approximate lot width of 620 feet, lot depth of 600 feet, and street frontage of 1,900 feet of.

#### *SRC 514.010(d) – Setbacks:*

**South (abutting street):** Adjacent to the south is Friendship Avenue SE. A



minimum setback of 12 feet, plus one foot for each one foot of height over 12 feet, but not exceeding 20 feet is required for buildings and accessory structures. A minimum 12-foot setback is required for vehicle use areas.

**West (abutting street):** Adjacent to the west is Pullman Avenue SE. A minimum setback of 12 feet, plus one foot for each one foot of height over 12 feet, but not exceeding 20 feet is required for buildings and accessory structures. A minimum 12-foot setback is required for vehicle use areas.

**East (abutting street):** Adjacent to the east is Lone Oak Road SE. A minimum setback of 12 feet, plus one foot for each one foot of height over 12 feet, but not exceeding 20 feet is required for buildings and accessory structures. A minimum 12-foot setback is required for vehicle use areas.

**North (abutting street):** Adjacent to the north is Alana Avenue SE. A minimum setback of 12 feet, plus one foot for each one foot of height over 12 feet, but not exceeding 20 feet is required for buildings and accessory structures. A minimum 12-foot setback is required for vehicle use areas.

**North (side abutting RS zone):** The property is adjacent to RS-zoned properties. The minimum setback for buildings, accessory structures, and vehicle use areas is 10 feet with Type C landscaping and screening.

**Finding:** The proposed addition to the south side of the building is more than 150 feet from Friendship Avenue SE, 175 feet from Lone Oak Road SE, and 260 feet from Pullman Avenue NE. The proposed addition to the north side of the building is more than 190 feet from Lone Oak Road SE, 300 feet from Pullman Avenue NE, and 270 feet from Alana Avenue SE and the north property line. The reconfigured vehicle use area south of the building is more than 19 feet from Friendship Avenue SE. The proposed expansion of the parking lot north of the building is approximately 15 feet or more from the north property line and Alana Avenue SE. To ensure compliance with this setback and screening requirement, staff recommends the following condition:

**Condition 2:** Provide a landscaped setback of at least 15 feet between the new parking lot pavement and the north property line, landscaped with a minimum of one plant unit per 20 square feet and screened with a minimum 6-foot-tall sight-obscuring fence, wall, or hedge.

With conditions, the proposal complies with all applicable setback requirements of the RM2 zone.

*SRC 514.010(e) - Lot Coverage, Height:*

Maximum lot coverage is 50 percent. The maximum building height is 70 feet.

**Finding:** The proposed south addition is 29 feet in height and the proposed north addition is 17 feet 6 inches in height. The proposed development complies

with the maximum height standard.

***SRC 514.010(g) - Landscaping:***

- (1) Setbacks. Required setbacks shall be landscaped. Landscaping shall conform to the standards set forth in SRC Chapter 807.
- (2) Vehicle Use Areas. Vehicle use areas shall be landscaped as provided under SRC Chapter 806 and SRC Chapter 807.

**Finding:** The existing setbacks are landscaped. Where landscaping is removed as a result of the construction, it will be replaced. The reconfigured parking lots and expanded parking lots will meet the required setbacks as well as the landscaping standards of SRC Chapter 806 and 807. At the conclusion of the project, more than 50 percent of the site will be landscaped. The proposed development complies with the standards.

***SRC 514.010(h) – Outdoor storage:***

Outdoor storage shall be screened from streets and adjacent properties by a minimum six-foot-high sight-obscuring fence, wall, or hedge.

**Finding:** No outdoor storage is proposed.

**Special Use Standards SRC 700.005 (Generally)**

The special use standards set forth in Chapter 700 apply where a use is designated as a special use and are in addition to the standards that apply to uses generally within a zone or overlay zone. The special use standards set forth in Chapter 700 may be modified, unless otherwise specially provided in this Chapter. Modification to a special use standard shall be made pursuant to Conditional Use approval, as provided in SRC Chapter 240. In no case shall Conditional Use approval allow a standard that is less restrictive than the development standards established in the zone or overlay zone.

**Special Use Standards SRC 700.010 (Basic Education)**

Where designated as a special use, basic education shall comply with the additional standards set forth in this section.

***SRC 700.010(a) – Lot Area:***

Minimum lot area is three acres.

**Finding:** The elementary school is located on a property of 8.5 acres.

***SRC 700.010(b) – Setbacks:***

Buildings shall be set back from every lot line a minimum of one foot for each one foot of building height; provided, however, the setback need not exceed 35 feet in depth. Off-street parking and loading areas shall be setback a minimum of ten

feet from any abutting residentially zoned property or property used for a residential use.

**Finding:** The existing school building meets setbacks and is not being modified. The parking and loading areas on the property serve the school and the religious assembly use and are set back more than 10 feet from abutting residentially zoned property north of the subject property.

*SRC 700.010(a) – Lot Coverage:*

No main building or combination of classroom buildings shall occupy more than 30 percent of the lot area.

**Finding:** The school building footprint occupies four percent of the site.

**Special Use Standards SRC 700.050 (Religious Assembly)**

*SRC 700.050(c) - Seating Capacity:*

The seating capacity of the largest space within a religious assembly shall not exceed the standards set forth in Table 700-3. Maximum seating capacity cannot be modified through Conditional Use approval. In the RM2 zone, no maximum seating capacity is applicable to lots abutting a collector or arterial street with more than 250 feet of frontage on such street.

**Finding:** The development has more than 600 feet of frontage on Lone Oak Road SE, a collector. There are 637 seats in the principal worship area, and seating capacity is not anticipated to be changed with this proposal. The proposal meets the standard.

*SRC 700.050(d) - Locational Standards:*

Religious assemblies shall be located on streets as set forth in Table 700-4. In the RM2 zone, location on a local street is allowed for seating capacity of 500 seats or less. Location on a collector or arterial street is allowed, regardless of seating capacity, for a lot with 250 feet or more of frontage on a collector or arterial.

**Finding:** There are 637 seats in the principal worship area, and the facility has more than 600 feet of frontage on a collector street. The proposal meets the standard.

*SRC 700.050(e) - Access:*

Access to religious assemblies may be provided from local, collector, or arterial streets. Access provided from a local street shall conform to the standards set forth in Table 700-5. In the RM2 zone, access points on a local street shall be located not more than 125 feet from a collector or arterial street.

**Finding:** The existing development has two accesses on Friendship Avenue SE, both of which are located more than 125 from Lone Oak Road SE, a collector

street. The applicant has proposed to remove the access driveway closest to Lone Oak Road SE and modify the access driveway that is farther from Lone Oak Road SE. The applicant has requested Conditional Use approval for the modified access driveway that will be retained. Findings addressing the Conditional Use are included in Section 6 of this report.

*SRC 700.050(f) - Lot Standards:*

Maximum lot area and minimum street frontage requirements for religious assemblies are set forth in Table 700-6. In the RM2 zone, no maximum area applies to lots abutting a collector or arterial street that have 250 feet or more of frontage on such street; minimum street frontage is 150 feet per street.

**Finding:** The subject property is 8.5 acres. Street frontages exceed 150 feet on Lone Oak Road SE, Friendship Drive SE, and Pullman Avenue SE. The proposal meets applicable standards.

*SRC 700.050(g) – Setbacks:*

Setbacks for religious assemblies in the RM2 zone shall be provided as set forth in Table 700-7. The minimum setback abutting a street is 12 feet for vehicle use areas or buildings not more than 25 feet in height, and 12 feet plus two feet for each 5 feet in height over 25 feet for buildings over 25 feet in height; the minimum setback abutting an interior side or interior rear lot line is 15 feet for vehicle use areas abutting a residential zone, 5 feet for vehicle uses areas abutting non-residential zones, 12 feet for buildings not more than 25 feet in height, and 12 feet plus two feet for each 5 feet in height over 25 feet for buildings over 25 feet in height.

**Finding:** The proposal meets the applicable special use standards as follows:

**South (abutting street):** Adjacent to the south is Friendship Avenue SE. The proposed addition to the south side of the building is 29 feet in height and more than 150 feet from the street. The reconfigured vehicle use area south of the building is more than 19 feet from the street.

**West (abutting street):** Adjacent to the west is Pullman Avenue SE. The proposed addition to the south side of the building is more than 260 feet from the street. The proposed addition to the north side of the building is 17 feet 6 inches tall and more than 300 feet from the street. The south parking area is more than 290 feet from the street. The expansion of the north parking area is more than 200 feet from the street.

**East (abutting street):** Adjacent to the east is Lone Oak Road SE. The proposed addition to the south side of the building is more than 175 feet from the street. The proposed addition to the north side of the building is more than 190 feet from the street. The existing south parking area is more than 23 feet from the street. The existing north parking area is more than 30 feet from the street.

**North (abutting street):** Adjacent to the north is Alana Avenue SE. The

proposed addition to the north side of the building is more than 270 feet from the street. The proposed expansion of the parking lot is more than 15 feet from the street.

**North (side abutting RS zone):** The proposed addition to the north side of the building is more than 270 feet from the north property line. The proposed expansion of the parking lot north of the building is approximately 15 feet or more from the north property line. Condition 3 requires a minimum 15-foot setback with Type C landscaping and screening for the proposed expansion of the parking lot. The existing parking area is less than 15 feet from the north property line and is legally nonconforming.

*SRC 700.050(h) - Lot Coverage; Height:*

Buildings and accessory structures for religious assemblies shall conform to the lot coverage and height standards set forth in Table 700-8. In the RM2 zone, the maximum lot coverage by buildings and accessory structures is 40%. In the RM2 zone, there are no special use standards for building height or height of spires, steeples, and towers without usable floor space.

**Finding:** With the proposed construction, the lot coverage will be 13.4 percent. The heights of the proposed building additions are 29 feet and 17 feet 6 inches, well below the maximum of 70 feet allowed under the standards of the underlying zone.

*SRC 700.050(i) - Off-Street Parking:*

(1) Location.

(A) Off-street parking may be located on-site or off-site. When parking is provided offsite, it shall be located:

- (i) On a lot or lots that are contiguous to the lot containing the main building or use; or
- (ii) Within 600 feet of the lot containing the main building or use, on a lot or lots within a non-residential zone.

(B) For the purposes of this paragraph, contiguous shall include a lot or lots that are separated from the lot containing the main building or use by an alley.

(2) Screening. Off-street parking areas shall be screened from abutting residential zoned property.

**Finding:** All of the required parking spaces are on the lot with the main building. The existing northern off-street parking lot is screened from abutting RS-zoned property, and the conditions of approval require screening for the expanded area of that parking lot.

*SRC 700.050(j) – Landscaping:*

All lot area not developed for buildings, structures, parking, loading, or driveways shall be landscaped as provided in SRC Chapter 807.

**Finding:** The existing campus is landscaped. The proposal includes removal of existing landscaping and replacement with new landscaping. The replacement

and new plantings must include at least one plant unit per 20 square feet of required landscape area and comply with other applicable standards of SRC chapter 807.

*SRC 700.050(k) – Related Uses:*

Schools, day care facilities, kindergartens, meeting facilities for clubs and organizations, and other similar activities operated primarily for the purpose of religious instruction, worship, government of the church, or the fellowship of its congregation shall be permitted. When such activities are not operated primarily for the purpose of religious instruction, worship, government of the church, or the fellowship of its congregation, the activities shall be allowed only if they are an allowed use in the zone.

**Finding:** The proposal does not include a change of activities.

**Off-Street Parking, Loading, and Driveways SRC 806**

*SRC 806.005 - Off-Street Parking; When Required.*

Off-street parking shall be provided and maintained for each proposed new use or activity; any change of use or activity, when such change of use or activity results in a parking ratio requiring a greater number of spaces than the previous use or activity; and for any intensification, expansion, or enlargement of a use or activity.

*SRC 806.010 - Proximity of Off-Street Parking to Use or Activity Served.*

Required off-street parking shall be located on the same development site as the use or activity it serves; or, within residential zones, required off-street parking may be located within 200 feet of the development site containing the use or activity it serves.

*SRC 806.015 - Amount of Off-Street Parking.*

- a) *Minimum Required Off-Street Parking.* The minimum number of off-street parking spaces required for a Religious Assembly use is 1 space per 5 seats or 10 feet of bench length within the principal worship area. The minimum number of parking spaces required for an elementary school, an activity within the Basic Education use, is 2 per classroom.
- b) *Compact Parking.* Up to 75 percent of the minimum off-street parking spaces required under this Chapter may be compact parking spaces.
- c) *Carpool and Vanpool Parking.* New developments with 60 or more required off-street parking spaces shall designate a minimum of 5 percent of their total off-street parking spaces for carpool or vanpool parking.
- d) *Maximum Off-Street Parking.* Unless otherwise provided in the SRC, off-street parking shall not exceed the amounts set forth in Table 806-2.

**Finding:** The principal worship area has 637 seats, requiring a minimum of 127 off-street parking spaces ( $637 / 5 = 127$ ). The elementary school has 8 classrooms, requirement a minimum of 4 spaces ( $8 \times 2 = 16$ ). The total minimum requirement is 143 spaces. The maximum off-street parking allowance for the use is 250 spaces ( $143 \times 1.75 = 250$ ). There are 246 existing spaces; after the proposed construction, the applicant's statement indicates that there would be 250 spaces, but the site plan depicts only 246 spaces. The proposal meets the standards.

**SRC 806.035 - Off-Street Parking and Vehicle Use Area Development Standards.**

- a) **General Applicability.** The off-street parking and vehicle use area development standards set forth in this section apply to the development of new off-street parking and vehicle use areas; the expansion of existing off-street parking and vehicle use areas, where additional paved surface is added; the alteration of existing off-street parking and vehicle use areas, where the existing paved surface is replaced with a new paved surface; and the paving of an un-paved area.
- b) **Location.** Off-street parking and vehicle use areas shall not be located within required setbacks.
- c) **Perimeter Setbacks and Landscaping.** Perimeter setbacks shall be required for off-street parking and vehicle use areas abutting streets, abutting interior front, side, and rear property lines, and adjacent to buildings and structures.

**Finding:** The proposed development involves alteration of existing parking and vehicle use areas, where the existing paved surface is replaced with a new paved surface, and expansion of parking areas. The off-street parking and vehicle use area development standards of SRC 806.035 apply to the proposed development.

SRC Chapter 806.035 would require setbacks of 6 to 10 feet from property lines abutting streets and 5 feet from property lines abutting the RS zone unless the underlying zone or Special Use standards require greater setbacks. Under the RM2 zone standards, the minimum setback abutting a street is 12 feet, and the minimum setback from an interior side or interior rear lot line is 10 feet abutting a residential zone. Under the Special Use standards for a religious assembly, the minimum setback abutting a street is 12 feet, and the minimum setback abutting an interior side or interior rear lot line is 15 feet abutting a residential zone. Under the Special Use standards for a school, the minimum setback is ten feet from any abutting residentially zoned property or property used for a residential use.

**South (abutting street):** Adjacent to the south is Friendship Avenue SE. The minimum setback is 12 feet, and the reconfigured vehicle use area south of the

building is more than 19 feet from the street.

**West (abutting street):** Adjacent to the west is Pullman Avenue SE. The minimum setback is 12 feet, and the expansion of the north parking area is more than 200 feet from the street.

**East (abutting street):** Adjacent to the east is Lone Oak Road SE. The minimum setback is 12 feet. The existing south parking area is more than 23 feet from the street. The existing north parking area is more than 30 feet from the street.

**North (abutting street):** Adjacent to the north is Alana Avenue SE. The minimum setback is 12 feet, and the proposed expansion of the parking lot is more than 15 feet from the street.

**North (side abutting RS zone):** Adjacent to the north are properties zoned RS (Single Family Residential) The minimum setback is 15 feet. The proposed expansion of the parking lot north of the building is approximately 15 feet or more from the north property line. Condition 3 requires a minimum 15-foot setback with Type C landscaping and screening for the proposed expansion of the parking lot. The existing parking area is less than 15 feet from the north property line and is legally nonconforming.

**Adjacent to Buildings and Structures:** The off-street parking or vehicle use area shall be setback from the exterior wall of the building or structure by a minimum 5 foot wide landscape strip or by a minimum 5 foot wide paved pedestrian walkway. All of the parking and vehicle use areas are separated from the existing and proposed buildings by paved pedestrian paths at least 5 feet in width.

**Finding:** The proposed vehicle use areas comply with the minimum perimeter setback standards of SRC Chapter 806. The proposal meets the setback requirements adjacent to a building or structure.

- d) Interior Landscaping. Interior landscaping shall be provided in amounts not less than those set forth in Table 806-5. For parking areas between 5,000 and 50,000 square feet in size a minimum of 5 percent of the interior parking area shall be landscaped. A minimum of one deciduous shade tree is required for every 12 parking spaces. The minimum planting area for a landscape island or planter bay is 25 square feet, with a minimum width of 5 feet.

**Finding:** The proposed site plan shows the north parking area, including existing and reconfigured areas, to be approximately 49,900 square feet with 136 spaces and the reconfigured south parking area to be approximately 40,000 square feet with 110 spaces. The minimum interior landscaping required in the reconfigured and expanded north parking lot is 2,495 square feet, including 11



trees. The minimum interior landscaping required in the reconfigured south parking lot is 2,000 square feet, including 9 trees. The proposed landscaping exceeds the minimum requirements. However, two of the proposed triangular tree wells in the reconfigured south parking lot are smaller than 25 square feet and also would prevent the adjacent parking spaces from meeting dimensional standards. These triangular planting areas are not required to meet the minimum interior landscaping standards. The following condition is required to ensure that the parking spaces meet dimensional standards:

**Condition 3:** Remove the proposed triangular planting areas in the reconfigured south parking lot.

- e) Off-Street Parking Area Dimensions. Off-street parking areas shall conform to the minimum dimensions set forth in Table 806-6.

**Finding:** The proposed parking spaces, driveway and drive aisle for the off-street parking area meet most of the minimum dimensional requirements of SRC Chapter 806. However, the eight northern spaces in the expanded north parking area are 9 feet wide and 17 feet long, where the standards would require a standard space to be 9 feet wide and 19 feet long. No overhang is allowed into the required 15-foot setback. Therefore, these spaces must be designated as compact spaces. Also, the drive aisle designated as a 20-foot wide fire lane must be at least 22 feet wide for two-way traffic. At the narrowest point, the distance between proposed buildings is 37 feet. Adequate area exists on both sides of this lane to widen the drive aisle to 22 feet and maintain a 5-foot paved pedestrian path and/or landscaped area for separation from existing and proposed buildings.

The proposal will meet dimensional standards with the following conditions:

**Condition 4:** The eight new parking spaces in the northern row of the expanded north parking lot shall be designated as compact spaces.

**Condition 5:** The 20-foot-wide "Fire Lane" depicted on the site plan between the north and south parking areas shall be widened to a minimum of 22 feet to accommodate two-way traffic.

- f) Additional Off-Street Parking Development Standards 806.035(f)-(m).

**Finding:** The reconfigured section of the southern parking area with new/improved paving and the expanded area of the northern parking area are subject to these standards for grade, surfacing, drainage, bumper guards or wheel barriers, parking area striping, marking and signage for one-way traffic and compact parking spaces, and lighting. The northern parking area is subject to screening from the abutting residentially zoned property. Bumper guards or wheel barriers are required so that vehicles do not overhang into required setbacks or pedestrian walkways. The site plan does not depict bumper guards or wheel barriers in all areas where they should be located.

With the proposed condition of approval for screening abutting the residentially zoned area and the following condition for bumper guards or wheel barriers, the proposal will meet the standards.

**Condition 6:** Bumper guards or wheel barriers shall be installed in the eight new parking spaces in the northern row of the expanded parking lot and all of the parking spaces abutting the proposed sidewalk that is oriented east to west through the reconfigured area of the southern parking lot.

SRC 806.040 - Driveway Development Standards.

- a) Access. Off-street parking and vehicle use areas shall have either separate driveways for ingress and egress, a single driveway for ingress and egress with an adequate turnaround that is always available or a loop to the single point of access.
- b) Location. Driveways shall not be located within required setbacks.
- c) Additional Development Standards 806.040(c)-(g).

**Finding:** The two driveways on Lone Oak Road SE and the driveway on Friendship Avenue SE conform to the driveway location and dimensional requirements of SRC 806.040. The driveways shall be paved with a hard surface, designed, graded, and drained according to the Public Works Design Standards (PWDS), or to the approval of the Public Works Director. The PWDS standards incorporate ADA accessibility.

### **Bicycle Parking**

*SRC 806.045 - General Applicability.*

Bicycle parking shall be provided and maintained for each proposed new use or activity; any change of use or activity, when such change of use or activity results in a parking ratio requiring a greater number of spaces than the previous use or activity; and for any intensification, expansion, or enlargement of a use or activity.

*SRC 806.055 - Amount of Bicycle Parking.*

A Religious Assembly use is required to have a minimum of 1 bicycle parking space per 30 vehicle parking spaces. An elementary school is required to have 2 bicycle parking spaces per classroom.

**Finding:** The elementary school is not being intensified, expanded, or enlarged; therefore, no additional bicycle parking is required for the elementary school. The religious assembly use is being enlarged. The minimum vehicle parking requirement for the religious assembly use is 127 spaces, and the minimum bicycle parking requirement is 4 spaces. The site plan indicates bicycle racks for 6 bicycles, meeting the standard.

## **Off-Street Loading Areas**

### *SRC 806.065 - General Applicability.*

Off-street loading shall be provided and maintained for each proposed new use or activity; any change of use or activity, when such change of use or activity results in a parking ratio requiring a greater number of spaces than the previous use or activity; and for any intensification, expansion, or enlargement of a use or activity.

### *SRC 806.075 - Amount of Off-Street Loading.*

A Religious Assembly use between 5,000 and 60,000 square feet is required to have a minimum of one 12 foot by 30 foot by 14 foot loading space. An elementary school of 5,000 to 60,000 square feet is required to have a minimum of one 12 foot by 30 foot by 14 foot loading space. An off-street parking space may be used in place of a required off-street loading space when the use or activity does not require a delivery vehicle which exceeds a maximum combined vehicle and load rating of 8,000 pounds and the off-street parking area is located within 25 feet of the building.

**Finding:** The elementary school is not being intensified, expanded, or enlarged; therefore, no additional bicycle parking is required for the elementary school. The religious assembly use is being enlarged but is still under 60,000 square feet. The proposed development does not result in any additional off-street loading spaces required for the development site.

## **Landscaping**

All required setbacks shall be landscaped with a minimum of 1 plant unit per 20 square feet of landscaped area. A minimum of 40 percent of the required number of plant units shall be a combination of mature trees, shade trees, evergreen/conifer trees, or ornamental trees. Plant materials and minimum plant unit values are defined in SRC Chapter 807, Table 807-2.

All building permit applications for development subject to landscaping requirements shall include landscape and irrigation plans meeting the requirements of SRC Chapter 807.

**Finding:** The applicant's site plan indicates that approximately 186,000 square feet of landscaping will be provided on the property, including 26,935 square feet within the parking areas. Within the reconfigured south parking area, approximately 3,000 square feet of landscaping is proposed outside of the required setbacks and 1,800 square feet of landscaping is proposed in the required setback adjacent to the street, requiring a minimum of 240 plant units ( $4,800 / 20 = 240$ ). At least 40 percent of the plant units, or 96 ( $240 \times 0.4 = 96$ ) shall be a combination of mature trees, shade trees, evergreen/conifer trees, or ornamental trees. Within the newly paved north parking area, approximately 4,300 square feet of landscaping is proposed outside of the required setbacks

and 1,600 square feet is proposed in the required setback adjacent to the north property line, requiring a minimum of 295 plant units ( $5,900 / 20 = 295$ ). At least 40 percent of the plant units, or 118 ( $295 \times 0.4 = 118$ ) shall be a combination of mature trees, shade trees, evergreen/conifer trees, or ornamental trees.

Landscape and irrigation plans will be reviewed for conformance with the requirements of SRC 807 at the time of building permit application review.

### **Natural Resources**

*SRC 808 - Preservation of Trees and Vegetation:* The City's tree preservation ordinance, under SRC Chapter 808, provides that no person shall remove a significant tree (Oregon White Oak greater than 24 inches in diameter at breast height) (SRC 808.015) or a tree or native vegetation in a riparian corridor (SRC 808.020), unless the removal is excepted under SRC 808.030(a)(2), undertaken pursuant to a permit issued under SRC 808.030(d), undertaken pursuant to a tree conservation plan approved under SRC 808.035, or permitted by a variance granted under SRC 808.045. No protected trees are identified for removal.

*SRC 809 - Wetlands:* The Salem-Keizer Local Wetland Inventory (LWI) does not show any wetlands on the subject property but does indicate hydric soil inclusions.

*SRC 810 - Landslide Hazards:* A geological assessment or report is required when regulated activity is proposed in a mapped landslide hazard area. The applicant's proposal does not disturb any portion of a mapped landslide hazard area; therefore, a geological assessment is not required.

**Finding:** The proposed development would not cause removal of protected trees or vegetation and would not occur in wetlands or landslide hazard areas.

### **Other Requirements**

*SRC 601 – Floodplain*

*SRC 802 – Public Improvements*

*SRC 803 – Streets and Right-of-Way Improvements*

*SRC 804 – Driveway Approaches*

*SRC 805 – Vision Clearance*

**Finding:** Upon completion of the conditions of approval, the subject property meets all applicable standards of the UDC, including 601 – Floodplain, 802 – Public Improvements, 803 – Streets and Right-of-Way Improvements, 804 – Driveway Approaches, and 805 – Vision Clearance.

Criterion 2:

The transportation system provides for the safe, orderly, and efficient circulation of traffic into and out of the proposed development, and negative impacts to the transportation system are mitigated adequately.

**Finding:** Lone Oak Road SE is designated as a Collector street in the Salem TSP (Transportation Systems Plan). The standard for this classification is a 34-foot-wide improvement within a 60-foot-wide right-of-way. The street has an approximate 34-foot improvement within a 60-foot-wide right-of-way abutting the subject property.

Friendship Avenue SE is designated as a Local street in the Salem TSP. The standard for this classification is a 30-foot-wide improvement within a 60-foot-wide right-of-way. The street has an approximate 36-foot improvement within a 60-foot-wide right-of-way abutting the subject property.

Pullman Avenue SE is designated as a Local street in the Salem TSP. The standard for this classification is a 30-foot-wide improvement within a 60-foot-wide right-of-way. The street has an approximate 18-foot improvement within a 60-foot-wide right-of-way abutting the subject property.

The existing configurations of Lone Oak Road SE, Friendship Avenue SE, and Pullman Avenue SE do not meet current standards for their classification of street per the Salem TSP. The applicant is required to mitigate transportation impacts by providing a proportional component of the required boundary street and right-of-way dedication as specified in SRC 803.040. Based on proportionality limitations in SRC 803.040(e)(3), the applicant shall construct street lights, street trees, sidewalk, and curb ramps as stated in the condition below as proportional mitigation for the proposed development. No street improvements are warranted along Pullman Avenue SE at this time because the project is only a portion of a development site or complex and the phase being developed does not abut Pullman Avenue SE.

No special setback is required along the development because the existing right-of-ways meet the standard for their respective classifications of street.

**Condition 7:** Construct street lights, street trees, sidewalks, and curb ramps along the entire frontages of Lone Oak Road SE and Friendship Avenue SE where not compliant with Public Works Design Standards.

Criterion 3:

Parking areas and driveways are designed to facilitate safe and efficient movement of vehicles, bicycles, and pedestrians.

**Finding:** No changes are proposed to the existing driveway approaches onto Lone Oak Road SE. The proposed driveway modification along Friendship Avenue SE provides for safe turning movements into and out of the property.

Criterion 4:

The proposed development will be adequately served with City water, sewer, stormwater facilities, and other utilities appropriate to the nature of the development.

**Finding:** The Public Works Department has reviewed the applicant's preliminary utility plan for this site. The water, sewer, and storm infrastructure are available within surrounding streets/areas and appear to be adequate to serve the proposed development. The applicant shall design and construct all utilities (sewer, water, and storm drainage) according to the PWDS and to the satisfaction of the Public Works Director. The applicant's engineer submitted a statement demonstrating compliance with the Stormwater PWDS Appendix 004-E(4)(a)-or-(b) and SRC Chapter 71. The preliminary stormwater design demonstrates the use of green stormwater infrastructure to the maximum extent feasible. Staff recommends the following condition to meet the criterion:

**Condition 8:** Design and construct a storm drainage system for the new and replaced impervious surfaces in compliance with SRC Chapter 71.

**8. Analysis of Class 2 Driveway Approach Permit Criteria**

Salem Revised Code (SRC) 804.025(d) sets forth the following criteria that must be met before approval can be granted to an application for a Driveway Approach Permit.

Criterion 1:

The proposed driveway approach meets the standards of this Chapter and the Public Works Design Standards.

**Finding:** The proposed driveway meets the standards for SRC 804 and PWDS.

Criterion 2:

No site conditions prevent placing the driveway approach in the required location.

**Finding:** There are no site conditions prohibiting the location of the proposed driveway.

Criterion 3:

The number of driveways onto an arterial is minimized.

**Finding:** The proposed driveway is not accessing onto an arterial street.

Criterion 4:

The proposed driveway approach, where possible:

- a) Is shared with an adjacent property; or
- b) Takes access from the lowest classification of street abutting the property.

**Finding:** The proposed driveway is modification is located on a local street, which is the lowest classification of street abutting the subject property.

Criterion 5:

The proposed driveway approach meets vision clearance standards.

**Finding:** The proposed driveway meets the PWDS vision clearance standards set forth in SRC Chapter 805.

Criterion 6:

The proposed driveway approach does not create traffic hazards and provides for safe turning movements and access.

**Finding:** The proposed driveway will not create a known traffic hazard and will provide for safe turning movements for access to the subject property.

Criterion 7:

The proposed driveway approach does not result in significant adverse impacts in the vicinity.

**Finding:** The location of the proposed driveway does not appear to have any adverse impacts to the adjacent properties or streets.

Criterion 8:

The proposed driveway approach minimizes impact to the functionality of adjacent streets and intersections.

**Finding:** The proposed driveway approach is located on a local street and does not create a significant impact to adjacent streets and intersections.

Criterion 9:

The proposed driveway approach balances the adverse impacts to residentially zoned property and the functionality of adjacent streets.

**Finding:** The proposed development is surrounded by residentially zoned property. The development proposes to close one of the existing two driveway approaches currently located across from a residentially zoned area. The remaining driveway approach is located across from a residentially zoned area. The driveway will not have an effect on the functionality of the adjacent streets.

## **RECOMMENDATION**

Based upon the Facts and Findings contained in this staff report, staff recommends that the Hearings Officer APPROVE the request for a conditional use, site plan review, and driveway approach permit for construction of additions to the north and south elevations of a church structure, removal of a driveway, modification of a driveway, addition of parking areas, and reconfiguration of existing parking areas serving an existing religious assembly on property located at 4227 Lone Oak Road SE subject to the following conditions of approval:

**Condition 1:** The development shall be in substantial conformance with the site plan included as Attachment C of the staff report, as modified by conditions of approval.

**Condition 2:** Provide a landscaped setback of at least 15 feet between the new parking lot pavement and the north property line, landscaped with a minimum of one plant unit per 20 square feet and screened with a minimum 6-foot-tall sight-obscuring fence, wall, or hedge.

**Condition 3:** Remove the proposed triangular planting areas in the reconfigured south parking lot.

**Condition 4:** The eight new parking spaces in the northern row of the expanded north parking lot shall be designated as compact spaces.

**Condition 5:** The 20-foot-wide "Fire Lane" depicted on the site plan between the north and south parking areas shall be widened to a minimum of 22 feet to accommodate two-way traffic.

**Condition 6:** Bumper guards or wheel barriers shall be installed in the eight new parking spaces in the northern row of the expanded parking lot and all of the parking spaces abutting the proposed sidewalk that is oriented east to west through the reconfigured area of the southern parking lot.

**Condition 7:** Construct street lights, street trees, sidewalks, and curb ramps along the entire frontages of Lone Oak Road SE and Friendship Avenue SE where not compliant with Public Works Design Standards.

**Condition 8:** Design and construct a storm drainage system for the new and replaced impervious surfaces in compliance with SRC Chapter 71.

Prepared by Pamela Cole, Planner II

Application Deemed Complete Date: June 19, 2018  
State Mandated Decision Date: October 17, 2018

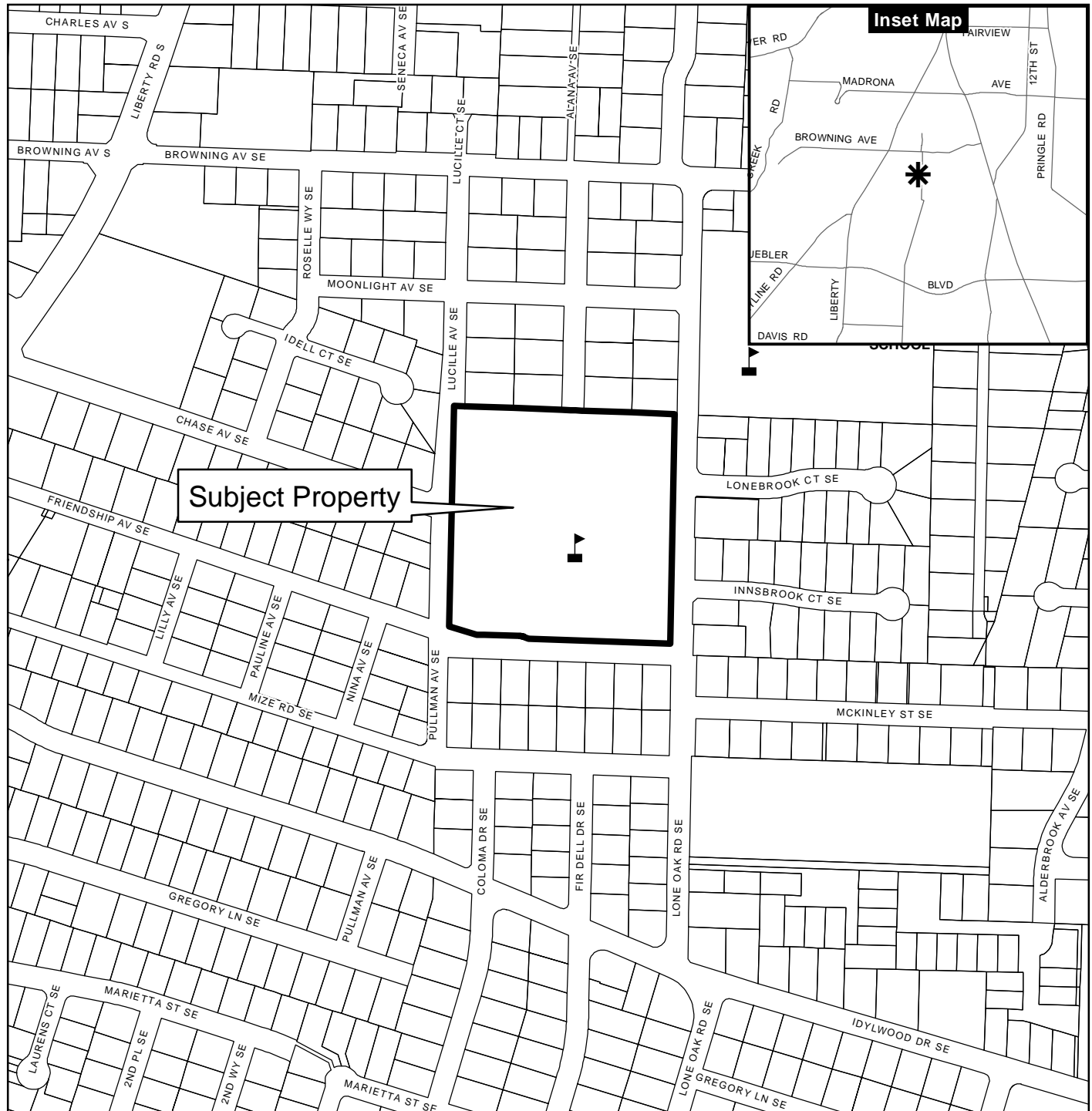
Attachments: A. Vicinity Map



- B. Existing Conditions Plan
- C. Proposed Site Plan
- D. Applicant's Statement Addressing Approval Criteria
- E. Public Works Memo

# Vicinity Map

## 4227 Lone Oak Road SE



### Legend

- Taxlots
- Urban Growth Boundary
- City Limits
- Outside Salem City Limits
- Historic District
- Schools
- Parks

0 100 200 400 Feet



**CITY OF Salem**  
AT YOUR SERVICE  
Community Development Dept.

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UDELL ENGINEERING  
AND  
LAND SURVEYING, LLC  
63 EAST ASH ST.  
LEBANON, OREGON 97355  
(541) 451-5125 PH.  
(541) 451-1366 FAX

EXISTING CONDITIONS  
QUEEN OF PEACE CATHOLIC  
CHURCH  
4227 LONE OAK RD SE  
SALEM, OREGON

DATE: 05/25/2017 rev. 6/6/2018  
PROJECT: 17-092 QUEEN OF PEACE CH.  
DRAWN BY: GSR  
CHECKED BY: KWL

REGISTERED  
PROFESSIONAL  
LAND SURVEYOR

OREGON  
JUNE 12, 2013  
KYLE W. LATIMER  
#80442  
EXPIRES 12-31-2018

Sheet  
1  
1

SCALE: SEE BARSCALE

## PROJECT VERTICAL DATUM - NGVD29

ALL ELEVATIONS ARE SHOWN ON THE VERTICAL DATUM OF NGVD 29. THE VERTICAL BENCHMARK USED FOR ESTABLISHING ELEVATIONS IS A POST-PROCESSED GPS OBSERVATION.

ATTENTION: OREGON LAW REQUIRES YOU TO FOLLOW RULES ADOPTED BY THE OREGON UTILITY NOTIFICATION CENTER. THOSE RULES ARE SET FORTH IN OAR 952-001-0010 THROUGH OAR 952-001-0090. YOU MAY OBTAIN COPIES OF THE RULES BY CALLING THE CENTER. (NOTE: THE TELEPHONE NUMBER FOR THE OREGON UTILITY NOTIFICATION CENTER IS (503) 232-1987).

THE LOCATION AND DESCRIPTIONS OF EXISTING UTILITIES SHOWN ON THE DRAWINGS ARE COMPILED FROM AVAILABLE RECORDS AND/OR FIELD SURVEYS. THE ENGINEER OR UTILITY COMPANIES DO NOT GUARANTEE THE ACCURACY OR THE COMPLETENESS OF SUCH RECORDS. CONTRACTOR SHALL FIELD VERIFY LOCATIONS OF ALL EXISTING UTILITIES PRIOR TO CONSTRUCTION.

### PROPERTY SUMMARY:

APPLICATION TYPE: CLASS 3, SITE PLAN REVIEW, CONDITIONAL USE PERMIT

OVERALL AREA: 368,506 SQUARE FEET (8.46 ACRES)

TAX LOT: 6200

TAX MAP: 083W10BB

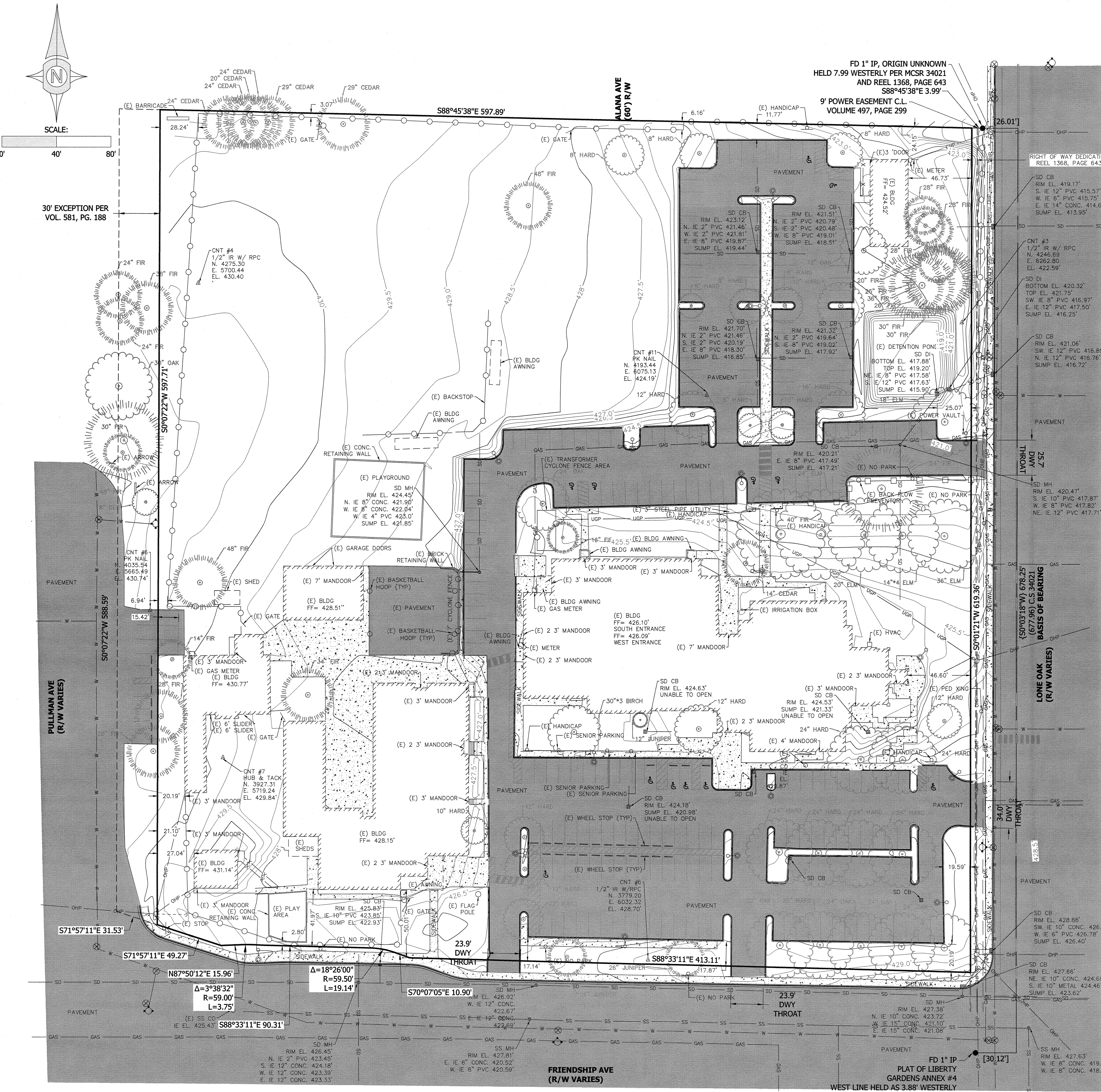
TITLE REPORT UTILIZED: AMERITITLE TITLE NO. 176039AM, DATED JUNE 17, 2017

### EXISTING SYMBOL LEGEND

- (E) - EXISTING CONTROL
- (P) - EXISTING GAS VALVE
- SS - EXISTING WATER VALVE
- SD - EXISTING WATER METER
- MH - EXISTING FIRE HYDRANT
- CB - EXISTING MAILBOX
- B.O. - EXISTING SIGN
- BW - EXISTING POWER POLE W/ CONDUIT
- BLDG - EXISTING LIGHT POLE
- CI - SEWER MANHOLE
- DI - PHONE PEDESTAL
- CO - POWER PEDESTAL
- C.S. - EXISTING CONCRETE SURFACE
- FD - FOUND MONUMENT AS NOTED
- IR - EXISTING 6' CYCLONE FENCE
- FF - EXISTING 3' CHAIN FENCE
- FH - EXISTING 4' HAND RAIL
- FL - EXISTING SD LINE LOCATE
- LP/PP - EXISTING SS LINE LOCATE
- PP - EXISTING WATER LINE LOCATE
- PUE - EXISTING PHONE LINE LOCATE
- IE - EXISTING UNDERGROUND POWER LOCATE
- IR - EXISTING OVERHEAD POWER
- R - EXISTING GAS LINE LOCATE
- R.O.W. - EXISTING COMMUNICATION LINE LOCATE

### ABBREVIATIONS LEGEND

- (E) - EXISTING
- (P) - PROPOSED
- SS - SANITARY SEWER
- SD - STORM DRAIN
- MH - MANHOLE
- CB - CATCH BASIN
- B.O. - BLOWOFF
- BW - BARB WIRE
- BLDG - BUILDING
- CI - CURB INLET
- DI - DITCH INLET
- CO - CLEAN OUT
- C.S. - COUNTY SURVEY
- FD - FOUND
- IR - IRON ROD
- FF - FINISH FLOOR
- FH - FIRE HYDRANT
- FL - FLOW LINE
- LP/PP - POWER POLE WITH STREET LAMP
- PP - UTILITY POLE
- PUE - PUBLIC UTILITY EASEMENT
- IE - INVERT ELEVATION (FLOW LINE)
- IR - IRON ROD
- R - RADIUS
- R.O.W. - RIGHT-OF-WAY
- S.L. - STREET LIGHT
- TC - TOP FACE OF CURB
- TOP - TOP OF PIPE
- P.P. - PARTITION PLAT
- W/ - WITH
- OPC - ORANGE PLASTIC CAP
- YPC - YELLOW PLASTIC CAP
- RPC - RED PLASTIC CAP





PROJECT INFORMATION	
DESCRIPTION	
ZONING DESIGNATION	RM-II (MULTIPLE FAMILY RESIDENTIAL)
ALLOWABLE LOT COVERAGE	MAX 40%
BUILDING SETBACKS	ABUTTING STREET: MIN 20'-0" ZONE-TO-ZONE: MIN 10'-0"
PARKING SETBACKS	ABUTTING PUBLIC R.O.W.: MIN 12'-0" ABUTTING LOT: MIN 15'-0" ABUTTING BUILDING: MIN 5'-0"
HEIGHT LIMITATION	MAX 70'-0"
OCCUPANCY TYPE	GROSS FLOOR AREA
ACCESSORY STORAGE	1,634 SF
ASSEMBLY	12,005 SF
BUSINESS	1,691 SF
DAY CARE	341 SF
KITCHENS, COMMERCIAL	612 SF
LIBRARY, READING ROOM	245 SF
TOTAL	16,528 SF

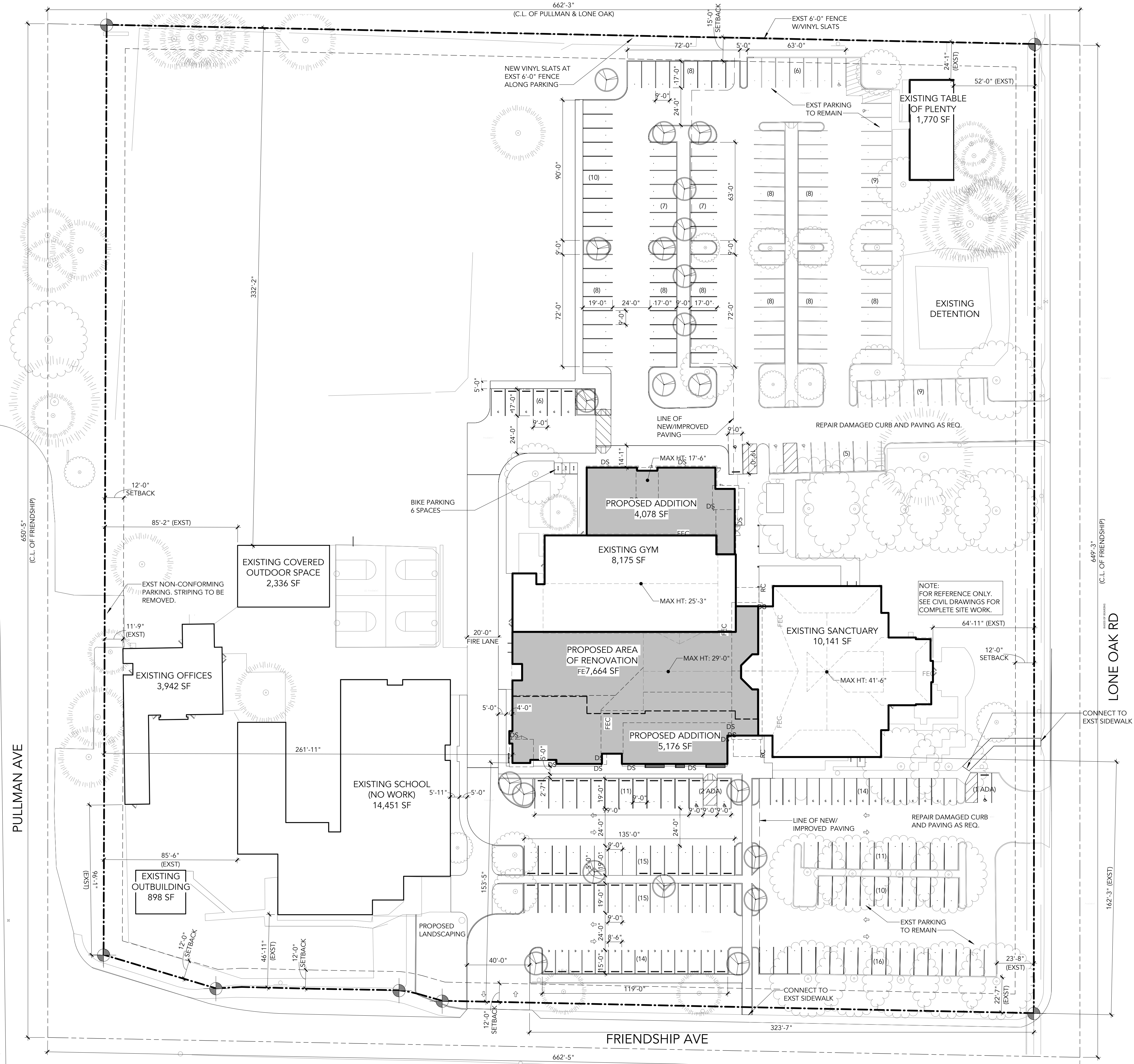
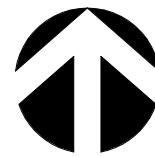
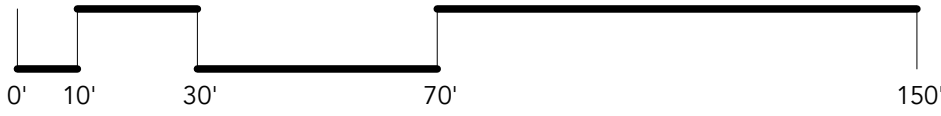
SITE STATISTICS		
DESCRIPTION	AREA (SF)	% OF SITE
EXST CHURCH (FOOTPRINT)	10,141	3%
EXST SCHOOL (FOOTPRINT)	14,451	4%
EXST OFFICES (FOOTPRINT)	3,942	1%
EXST OUTBUILDING (FOOTPRINT)	898	.25%
EXST TABLE OF PLENTY (FOOTPRINT)	1,770	.5%
EXST COVERED PLAY AREA (FOOTPRINT)	2,336	.75%
AREA OF RENOVATION (FOOTPRINT)	7,664	2%
PROPOSED ADDITION (FOOTPRINT)	9,270	1.5%
TOTAL LOT COVERAGE (ALL STRUCTURES)	50,472	13.5%
PAVING (EXST TO REMAIN)	50,151	13.5%
PAVING (NEW/IMPROVED)	56,701	15.5%
PAVING (TOTAL)	106,852	29%
SIDEWALKS (EXST TO REMAIN)	14,494	4%
SIDEWALKS (NEW/IMPROVED)	13,242	3.5%
SIDEWALKS TOTAL	27,736	7.5%
GENERAL LANDSCAPE AREA	156,818	43%
PARKING LANDSCAPE AREA	26,935	7%
LANDSCAPE AREA TOTAL	183,753	50%
TOTAL SITE AREA	368,813 (8.5 ACRES)	100%

PARKING STATISTICS		
DESCRIPTION	# REQUIRED	# PROVIDED
TOTAL EXISTING PARKING	143 MIN, 250 MAX	246
FULL-SIZE PARKING STALLS	143 MIN, 250 MAX	200
COMPACT PARKING STALLS	MAX 75%	42 (17%)
ACCESSIBLE PARKING STALLS	7	8
CLEAN-AIR/VANPOOL/EV STALLS	N/A	N/A
TOTAL PROPOSED (ALL) PARKING	143*	250

\*RATIO OF 1 / 5 SEATS WITHIN THE PRINCIPLE WORSHIP AREA (637 SEATS) = 637 / 5 = 127  
\*RATIO OF 2 / CLASSROOM (8 CLASSROOMS) = 2 x 8 = 16

1 SITE PLAN

SCALE: 1/32" = 1'-0"





June 7, 2018

Pamela Cole  
Community Development Department  
City of Salem  
555 Liberty Street SE, Room 305  
Salem, OR 97301

Re: Class 2 Site Plan Review  
Queen of Peace Catholic Church  
4227 Lone Oak Road SE

## I. PROJECT DESCRIPTION AND PROPOSAL REQUESTS

Queen of Peace Catholic Church with CB Two Architects respectfully submits this Consolidated Application for a Class 3 Site Plan Review and a Conditional Use Permit for the Church's proposed building expansions along with reconfiguration of parking and vehicle use areas, and landscaping modifications. Improvements will include a 5,192 square foot addition to the community room on the south side of the church building, a 4,078 square foot addition to the north side of the church building for a youth center, reconfiguring parking areas to reduce driveway access on Friendship, and enhance pedestrian paths and vehicle drives that facilitate better and more efficient connectivity between parking areas on the north and south sides of the church. Landscaping updates are anticipated as part of this proposal.

Queen of Peace Catholic Church is located on approximately 8.5 acres that is zoned RM-II – Multiple Family Residential. Class 3 Site Plan Review development standards for this project are found in Salem Revised Code (SRC), this narrative provides the applicable standards and project information that confirms compliance with standards as required. Standards for this project are found in SRC 514 RM-II – Multiple Family Residential; however the church and its elementary school are allowed as a Special Use, "Religious assembly", which has additional standards that must be complied with in order to receive approval, those standards are found in SRC 700 Special Use Provisions. Generally the standard that is the more restrictive will apply. Additionally both SRC 514 and SRC 700 reference other chapters, such as parking and landscaping, which have requirements which are also met by the proposal.

The Conditional Use Permit is triggered by the modifications to the existing parking area that is accessed from Lone Oak and Friendship; the two driveways on Friendship are non-conforming driveways as they are more than 125 feet from a collector. The proposed changes to this parking area will in fact remove one of the non-conforming driveways thereby reducing the amount of non-conformance; however the improved driveway will still be more than 125 feet from Lone Oak therefore requiring conditional approval. It should be noted that non-conforming parking accessed from Pullman will be removed as part of the site work, this will further reduce the degree of non-conforming conditions that currently exist.

## II SITE PLAN REVIEW

The following text contains the applicable standards and project information. The project information sections furnish detailed information about the proposal and will confirm that the proposal complies with the required development standards, save the remaining driveway on Friendship which will allow staff to approve a Class 3 Site Plan Review. Information as to how the Conditional Use Permit criteria are satisfied through this proposal is also included in this narrative.

**SEATING CAPACITY** – The seating capacity of the largest space within a religious assembly shall not exceed the standards set forth in Table 700-3.(Table 700-3) -

Standard:

Max. 500	Lots abutting a local street. Lots abutting a collector or arterial street with less than 250 feet of frontage on such street.
No Max	Lots abutting a collector or arterial street with 250 feet or more of frontage on such street; Lots located at the intersection of a collector and arterial street, 2 collector streets, or 2 arterial streets; or double frontage lots with both frontages on collector or arterial streets.

**Project information:** Development has frontage on three streets – Lone Oak on the east, Friendship to the south, and Pullman for a portion of the western frontage. Lone Oak, a collector, has over 600 feet of street frontage. Friendship, a local street, also has over 600 feet of street frontage. Pullman, which has frontage along a portion of the developments western boundary, has about 300 feet of street frontage. While there is no restriction on seating capacity due to the amount of street frontage on Lone Oak, seating within the existing sanctuary, the largest assembly space, is not anticipated to change as a result of this application. The current worship area has a capacity of 637 seats.

**LOCATIONAL STANDARDS.** Religious assemblies shall be located on streets as set forth in Table 700-4. The standard for these types of uses within the RM-II zone are as follows:

Applicable Standard

Local street	Allowed for seating capacity of 500 seats or less.
Collector or arterial street	Allowed for seating capacity of 500 seats or less, when located on lot with less than 250 of frontage on a collector or arterial streets. Allowed , regardless of seating capacity, when: <ul style="list-style-type: none"> <li>▪ Located on a lot with 250 feet or more of frontage on a collector or arterial street.</li> <li>▪ Located at the intersection of a collector and arterial street, 2 collector streets, or 2 arterial streets.</li> <li>▪ Double frontage lots with both frontages on collector or arterial streets.</li> </ul>



**Project Information:** As noted previously, Queen of Peace is located along which is a collector and has more than 600 feet of street frontage, the minimum requirement is 250 feet. Therefore the standard is met.

**ACCESS.** Access to religious assemblies may be provided from local, collector, or arterial streets. Access provided from a local street shall conform to the standards set forth in Table 700-5 –

**Applicable Standard**

RM-II zones	Access points shall be located not more than 125 feet from a collector or arterial street.
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**Project Information:** As noted in previous sections, the development has street frontage on a collector and two local streets. There are two existing driveways on Lone Oak (a collector) and there are two existing driveways on Friendship (a local street). The two driveways which are accessed from Lone Oak will remain as part of this proposal; however the current plan shows that the easterly driveway on Friendship will be closed and the remaining driveway will be improved and will meet the driveway standards found in SRC 806, this is further described later in this narrative.

The existing driveways on Friendship are non-conforming and allowed as a conditional use. The removal of one of those lessens the degree of non-conformance but does not eliminate it completely. Therefore the remaining driveway will continue to require a Conditional Use Permit which is being requested as part of the consolidated land use application. More information and evidence of applicable criteria is provided later in this narrative. Please see the proposed project Site Plan provided with the application materials for the location of existing and modified driveways.

**LOT STANDARDS.**

From Table 514.2

Requirement	Standard	Limitations & Qualifications
Lot area		
Uses other than Single Family	Min. 6,000 sq. ft.	
Lot Width		
Uses other than Single Family	Min. 40 ft.	
Lot Depth		
Uses other than Single Family	Min. 80 ft.	
	Min. 120 ft.	Applicable to double frontage lots.
	Max. 300% of average lot width	
Street Frontage		
Uses other than Single Family	Min. 40 ft.	

From Table 700-6

Maximum Lot Area Standards	
Max. 3.5 acres	Applicable to: <ul style="list-style-type: none"> <li>Lot abutting a local street.</li> <li>Lot abutting a collector or arterial street with less than 250 feet of frontage on such street.</li> </ul>
No Max.	Applicable to: <ul style="list-style-type: none"> <li>Lot abutting a collector or arterial street with 250 feet or more of frontage on such street.</li> <li>Lot located at the intersection of a collector and arterial street, 2 collector streets, or 2 arterial streets.</li> <li>Double frontage lot with both frontages on collector or arterial streets.</li> </ul>
Street Frontage	
Min. 150 ft. per street	

**Project Information:** Development exceeds the minimum standards found in both SRC 514 – Table 514.2 and SRC 700 – Table 700-6. First the project lot area is 8.5 acres or more than 370,000 square feet. The minimum required is 6,000 and there is no maximum for our site as it is located on a collector with more than 250 feet of street frontage. The minimum lot width is 40 feet, the site is at least 600 feet wide. The minimum lot depth is 120 feet for sites that have double frontage (there is frontage on Pullman as well as Lone Oak), our lot is more than the minimum with a depth of approximately 600 feet. We exceed the 150 foot minimum (SRC 700) of required street frontage as noted.

**SETBACKS.** Setbacks for new main buildings are found in both Table 513-4 and & 700.7. The standards applicable to uses other than Single Family (Table 513-4) and Religious Assembly in the RM-II zone (Table 700-7) are provided.

Table 513-4

Requirement	Standard	Limitations & Qualifications
Abutting Street		
Buildings		
Uses other than Single Family	Min. 12 ft., plus 1 ft. for each 1 ft. of height over 12 ft., but need not exceed 20 ft. in depth.	
Accessory Structures		
Uses other than Single Family and Two Family	None	Applicable to accessory structures not more than 4 ft. in height.
	Min. 12 ft., plus 1 ft. for each 1 ft. of height over 12 ft.	Applicable to accessory structures greater than 4 ft. in height.
Vehicle Use Areas		
Uses other than Single Family and Two Family	Min. 12 ft.	



Interior Front		
Buildings		
Uses other than Single Family and Two Family	Zone-to-zone setback (Table 514-5)	
Vehicle Use Areas		
Uses other than Single family and Two Family	Zone-to-zone setback (Table 514-4)	
Interior Side		
Buildings		
Uses other than Single Family and Two Family	Zone-to-zone setback (Table 514-5)	
Accessory Structures		
Uses other than Single Family and Two Family	Zone-to-zone setback (Table 514-5)	
Vehicle Use Areas		
Uses other than Single Family and Two Family	Zone-to-zone setback (Table 514-5)	
Interior Rear		
Buildings		
Uses other than Single Family and Two Family	Zone-to-zone setback (Table 514-5)	
Accessory Structures		
Accessory to uses other than Single Family and Two Family	Zone-to-zone setback (Table 514-5)	
Vehicle Use Areas		
Uses other than Single Family and Two Family	Zone-to-zone setback (Table 514-5)	

## Setback Standards for Special Uses in RM-II zone Table 700-7

Standard	Limitations & Qualifications
Abutting Street	
Buildings and Accessory Structures	
Min. 12 ft.	Applicable to buildings and accessory structures not more than 25 ft. in height.
Min. 12 ft., plus 2 feet for each 5 feet of height over 25 ft.	Applicable to building and accessory structures greater than 25 ft. in height.
Vehicle Use Areas	
Min. 12 ft.	
Interior Front	
Buildings and accessory Structures	
Min. 12 ft.	Applicable to buildings and accessory structures not more than 25 ft. in height.
Min. 12 ft. plus 2 feet for each 5 feet of height over 25 ft.	Applicable to building and accessory structures greater than 25 ft. in height.

Vehicle Use Areas	
Min. 5 ft.	
Min. 15 ft.	Applicable when abutting a residential zone.
Interior Side	
Buildings and accessory Structures	
Min. 12 ft.	Applicable to buildings and accessory structures not more than 25 ft. in height.
Min. 12 ft., plus 2 feet for each 5 feet of height over 25 ft.	Applicable to building and accessory structures greater than 25 ft. in height.
Vehicle Use Areas	
Min. 5 ft.	
Min. 15 ft.	Applicable when abutting a residential zone.
Interior Rear	
Buildings and Accessory Structures	
Min. 12 ft.	Applicable to buildings accessory structures not more than 25 ft. in height.
Min. 12 ft., plus 2 feet for each 5 feet of height over 25 ft.	Applicable to building and accessory structures greater than 25 ft. in height.
Vehicle Use Areas	
Min. 5 ft.	
Min. 15 ft.	Applicable when abutting a residential zone.

Table 514-5. Zone-to-Zone Setbacks

Abutting Zone	Type of Improvement	Setback (1)	Landscaping & Screening
Residential Zone	Buildings and accessory structures	Min. 10 ft.	Type C
	Vehicle use areas		

**Project Information:** The existing church structure is currently setback from Lone Oak by more than 50 feet, and the addition which faces Friendship will be setback from that street by more than 150 feet. The proposed addition to the north (our Interior Side) will have a building setback by more than 200 feet. The existing parking areas along Lone Oak and Friendship are setback by more than 12 feet and the new parking area that will adjoin the existing on the north side of the church structure will be setback from the property line by 15 feet as required. Therefore building and parking additions and updates meet the setback requirements.

LOT COVERAGE; HEIGHT. Standards for Lot Coverage and Height are found in both Table 514-6 and Table 700-8.

Table 514-6

Requirement	Standard	Limitations & Qualifications
Lot Coverage		
Buildings and Accessory Structures		
All uses	Max. 50%	
Rear Yard Coverage		

Buildings		
All uses	N/A	
Accessory Structures		
Accessory to uses other than Single Family and Two Family	No Max.	
Height		
Buildings		
Uses other than Residential, Residential Care, Nursing Care and Short-term Lodging	Max. 70 ft.	
Accessory Structures		
Accessory to all uses	Max. 15 ft.	

#### Special Use Standards in RMII Zone - Table 700-8

Standard		Limitations & Qualifications
Lot Coverage		
Buildings and Accessory Structures		
RA, RS, RD, RM-I, RM-II, RH, and CO zones	Max. 40%	Applicable to an individual lot, or contiguous lots when multiple lots are combined under a single ownership to accommodate the development. For the purpose of this requirement, contiguous shall include lots that are separated by an alley.
Height		
Buildings and Accessory Structures		
RA and RS zones	Max. 35 ft.	
	Max. 50 ft.	Applicable to spires, steeples, and towers without useable floor space.

**Project Information:** The proposed 9,270 square feet of additions to the primary structure will increase overall lot coverage, which will be 13.5% at completion. This is well below the 40% maximum allowed as a special use in the RM-II zone.

The proposed additions are below the 70 feet allowed height by code. The taller of the two will be the southern addition with a building height of 29 feet; the north addition is planned with a building height of 17 feet 6 inches. Both of these additions are less than the allowable maximum height.

**MAXIMUM SQUARE FOOTAGE FOR ALL ACCESSORY STRUCTURES.** In addition to the maximum coverage requirements established in Table 514-6, accessory structures to single family and two family uses shall be limited to the maximum aggregate total square footage set forth in Table 514-7.

From Table 514-7

Maximum Square Footage For All Accessory Structures	
Main Building Gross Area	Maximum Total Square Footage for All Accessory Structures
Greater than 1,200 square feet.	1,000 sq. ft. or 50% of main building gross area, whichever is less.

**Project Information:** Standard noted. This development project does not include any accessory structures therefore the standard does not apply to this proposal.

**LANDSCAPING.** All lot area not developed for buildings, structures, parking, loading, or driveways shall be landscaped as provided in SRC Chapter 807. The landscape screening type is identified in SRC 514, Table 514.5

Table 514-5. Zone-to-Zone Setbacks

Abutting Zone	Type of Improvement	Setback (1)	Landscaping & Screening
Residential Zone	Buildings and accessory structures	Min. 10 ft.	Type C
	Vehicle use areas		

“Unless otherwise provided under the UCD, required landscaping and screening shall conform to the standards set forth in this section”

(a) Landscaping types. Table 807-1.

Landscaping Type	Required Plan Units (PU)	Required Screening
C	Min. 1 PU per 20 sq. ft. of landscaped area	Min. 6-foot-tall fence, wall, or hedge

**Project Information:** Currently the Queen of Peace campus is landscaped; once the building and parking additions included in this proposal are made landscaping percentage for the site will be in excess of 50 percent. Where landscaping is to be removed as a result of construction it will be replaced as required. All setbacks will be landscaped as required. Vehicle use areas are setback from property lines and will be landscaped as needed. The existing parking area on the north side of the church structure is screened as required; the additional parking area will also be screened by the exiting 6’ fence with privacy slats as required.

#### OFF- STREET PARKING.

Required Parking Spaces: SRC 806.015, Table 806-1, requires off-street parking as follows: 1 per 5 seats or 10 feet of bench length within the principal worship area; or 1 per 80 sq. ft. within the principal worship area, when no fixed seating or benches are provided.

**Project Information:** Parking is provided for the existing church and school on site. Additional parking is planned as part of this application and will be on site. Based on our calculations (please see Parking Statistics on the Site Plan for a breakdown of how parking is calculated for this development) the minimum number of required parking spaces is 143, with a maximum of 250 which serve the Church, the school, the nonprofit membership use and the office. Currently there are 246 parking spaces with an increase of 4 with the proposed additions and new and improved parking for a total of 250 off-street parking spaces upon completion.

Development standards found in SRC 806.035 and 806.040 apply to new and improved parking areas. These standards are met as required, including landscaped setbacks and screening. There will be up to 7 percent interior landscaping in the affected parking and vehicle use areas, the minimum requirement is 5 percent. For details please reference the proposed Site included in the application materials.

## NATURAL RESOURCES

## Trees SRC 808 –

**Project information:** There are no heritage trees, significant trees or wetlands on this site; however, Church's campus is larger than 20,000 square feet therefore identifying trees that have a DBH of 10" or greater is required as part of our site plan approval. There are three trees on the south side of the existing church structure with a DBH of 10" or greater that will be removed as part of the 5,192 square foot addition, they are a 30" clump of birch, a 12" juniper, and a 12" deciduous tree adjacent to the main entry doors.

## III. CONDITIONAL USE

As indicted in our request, a Conditional Use Permit for the remaining reconfigured driveway on Friendship is required for this development proposal due to the distance the driveway is from Lone Oak, a collector street. Currently there is a Conditional Use Permit in place for both of the driveways, since one of them is being removed and the other is being widened a new Conditional Use request is appropriate given the proposed changes. In order for the Hearings Officer to render a decision and grant the Conditional Use Permit we the applicant must show that the proposal meets the following criteria found in SRC 240.005(d):

- (1) The proposed use is allowed as a conditional use in the zone;

**Project Information:** Yes, this driveway (as well as two others) was allowed as a conditional use in a previous land use decision CPC/ZC93-14. In an effort to reduce the degree of non-conformance and improve circulation in vehicle use areas as well as access from Friendship one of the allowed driveways will be removed and the remaining driveway will be widened. The location of the remaining driveway does not change as a result of this request.

- (2) The reasonably likely adverse impacts of the use on the immediate neighborhood can be minimized through the imposition of conditions; and

**Project Information:** As noted previously, this driveway already exists and is allowed as a conditional use; therefore no change or increase impacts to the immediate neighborhood are expected. Rather, it is our belief that adverse impacts are minimized with this proposal as the degree of non-conformance will be lessened by the removal of one of the previously approved driveways. In addition, improvements to the remaining driveway allow for better maneuvering and access, which is encouraged by City of Salem Public Works.

- (3) The proposed use will be reasonably compatible with and have minimal impact on the livability or appropriate development of surrounding property.

**Project Information:** The proposed improvements to the existing driveway are compatible with the development and are appropriate. No adverse "impacts to the livability or appropriate development of surrounding properties" is anticipated with this proposal.

#### IV. CONCLUSION

This narrative and the accompanying documents and plans should allow the review authorities to approve this proposal. The addition of 9,270 square feet to the existing church structure, the addition of 6 off-street parking spaces, improved vehicle use areas and pedestrian paths, coupled with increased landscaping in parking areas meets the standards of the applicable chapters of the SRC, save the need for a new Conditional Use Permit for the improved driveway on Friendship which is the minimum needed. Enhancements to the Church and site not only benefit the Queen of Peace community but the broader neighborhood. The development team is looking forward to working with the City of Salem Staff on this project.



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JUN 27 2018

COMMUNITY DEVELOPMENT

## MEMO

**TO:** Pamela Cole, Planner II  
Community Development Department

**FROM:** Glenn J. Davis, PE, CFM, Chief Development Engineer  
Public Works Department

**DATE:** June 26, 2018

**SUBJECT:** PUBLIC WORKS RECOMMENDATIONS  
CU-SPR-DAP18-05 (18-106941-RP)  
4227 LONE OAK ROAD SE  
DRIVEWAY AND PARKING LOT MODIFICATIONS

**PROPOSAL**

A Class 3 Site Plan Review and a Class 2 Driveway Approach Permit to construct additions to the north and south elevations of a church structure, removal of a driveway, modification of a driveway, addition of parking areas, and reconfiguration of existing parking areas serving an existing religious assembly on an 8.5-acre property in the RM2 (Multiple Family Residential) zone at 4227 Lone Oak Road SE 97302 (Marion County Assessor Map and Tax Lot 083W10BB06200).

**RECOMMENDED CONDITIONS OF APPROVAL**

1. Construct street lights, street trees, sidewalks, and curb ramps along the entire frontages of Lone Oak Road SE and Friendship Avenue SE where not compliant with Public Works Design Standards.
2. Design and construct a storm drainage system for the new and replaced impervious surfaces in compliance with SRC Chapter 71.

**FACTS****Streets**

1. Lone Oak Road SE
  - a. Existing Conditions—This street has an approximate 34-foot improvement within a 60-foot-wide right-of-way abutting the subject property.
  - b. Standard—This street is designated as a Collector street in the Salem TSP. The standard for this street classification is a 34-foot-wide improvement within a 60-foot-wide right-of-way.

2. Friendship Avenue SE

- a. Existing Conditions—This street has an approximate 36-foot improvement within a 60-foot-wide right-of-way abutting the subject property.
- b. Standard—This street is designated as a Local street in the Salem TSP. The standard for this street classification is a 30-foot-wide improvement within a 60-foot-wide right-of-way.

3. Pullman Avenue SE

- a. Existing Conditions—This street has an approximate 18-foot improvement within a 60-foot-wide right-of-way abutting the subject property.
- b. Standard—This street is designated as a Local street in the Salem TSP. The standard for this street classification is a 30-foot-wide improvement within a 60-foot-wide right-of-way.

**Storm Drainage**

1. Existing Conditions

- a. An onsite, private stormwater detention system serves the property and discharges to a 12-inch storm main located in Lone Oak Road SE.
- b. A 15-inch storm main is located in Friendship Avenue SE.

**Water**

1. Existing Conditions

- a. The subject property is located in the S-2 water service level.
- b. An 8-inch water main is located in Lone Oak Road SE. Mains of this size generally convey flows of 1,100 to 2,900 gallons per minute.
- c. An 8-inch water main is located in Friendship Avenue SE.
- d. An 8-inch water main is located in Pullman Avenue SE.

**Sanitary Sewer**

1. Existing Conditions

- a. An 8-inch sewer line is located in Lone Oak Road SE.



- b. An 8-inch sewer line is located in Friendship Avenue SE.

### **CRITERIA AND FINDINGS**

Analysis of the development based on relevant criteria in SRC 220.005(f)(3) is as follows:

**Criteria: SRC 220.005(f)(3)(A) – The application meets all applicable standards of the UDC (*Unified Development Code*)**

**Finding:** Upon completion of the conditions of approval, the subject property meets all applicable standards of the following chapters of the UDC: 601 – Floodplain, 802 – Public Improvements, 803 – Streets and Right-of-Way Improvements, 804 – Driveway Approaches, 805 – Vision Clearance, 809 – Wetlands, and 810 – Landslides.

**Criteria: SRC 220.005(f)(3)(B) – The transportation system provides for the safe, orderly, and efficient circulation of traffic into and out of the proposed development, and negative impacts to the transportation system are mitigated adequately**

**Finding:** The existing configuration of Lone Oak Road SE, Friendship Avenue SE, and Pullman Avenue SE do not meet current standards for their classification of street per the Salem TSP. The applicant is required to mitigate transportation impacts by providing a proportional component of the required boundary street and right-of-way dedication as specified in SRC 803.040. Based on proportionality limitations in SRC 803.040(e)(3), the applicant shall construct street lights, street trees, sidewalk, and curb ramps as stated in the conditions above as proportional mitigation for the proposed development. No street improvements are warranted along Pullman Avenue SE at this time because the project is only a portion of a development site or complex and the phase being developed does not abut Pullman Avenue SE.

No special setback is required along the development because the existing right-of-ways meet the standard for their respective classifications of street.

**Criteria: SRC 220.005(f)(3)(C) – Parking areas and driveways are designed to facilitate safe and efficient movement of vehicles, bicycles, and pedestrians**

**Finding:** No changes are proposed to the existing driveway approaches onto Lone Oak Road SE. The proposed driveway modification along Friendship Avenue SE provides for safe turning movements into and out of the property.

**Criteria: SRC 220.005(f)(3)(D) – The proposed development will be adequately served with City water, sewer, storm drainage, and other utilities appropriate to the nature of the development**

**Finding:** The Public Works Department has reviewed the applicant's preliminary utility plan for this site. The water, sewer, and storm infrastructure are available within surrounding streets / areas and appear to be adequate to serve the proposed development. The applicant shall design and construct all utilities (sewer, water, and storm drainage) according to the PWDS and to the satisfaction of the Public Works Director.

The applicant's engineer submitted a statement demonstrating compliance with Stormwater PWDS Appendix 004-E(4)(a)-or-(b) and SRC Chapter 71. The preliminary stormwater design demonstrates the use of green stormwater infrastructure to the maximum extent feasible.

**Criteria: SRC 804.025(d) - A Class 2 Driveway Approach Permit shall be granted if:**

- (1) The proposed driveway approach meets the standards of this Chapter and the Public Works Design Standards;**

**Finding—**The proposed driveway meet the standards for SRC 804 and PWDS.

- (2) No site conditions prevent placing the driveway approach in the required location;**

**Finding—**There are no site conditions prohibiting the location of the proposed driveway.

- (3) The number of driveway approaches onto an arterial are minimized;**

**Finding—**The proposed driveway is not accessing onto an arterial street.

- (4) The proposed driveway approach, where possible:**

**(a) Is shared with an adjacent property; or**

**(b) Takes access from the lowest classification of street abutting the property;**

**Finding—**The proposed driveway modification is located on a local street, which is the lowest classification of street abutting the subject property.

- (5) Proposed driveway approach meets vision clearance standards;**

**Finding—**The proposed driveway meets the PWDS vision clearance standards set forth in SRC Chapter 805.

- (6) The proposed driveway approach does not create traffic hazards and provides for safe turning movements and access;**

**Finding**—The proposed driveway will not create a known traffic hazard and will provide for safe turning movements for access to the subject property.

- (7) The proposed driveway approach does not result in significant adverse impacts to the vicinity;**

**Finding**—The location of the proposed driveway does not appear to have any adverse impacts to the adjacent properties or streets.

- (8) The proposed driveway approach minimizes impact to the functionality of adjacent streets and intersections; and**

**Finding**—The proposed driveway approach is located on a local street and does not create a significant impact to adjacent streets and intersections.

- (9) The proposed driveway approach balances the adverse impacts to residentially zoned property and the functionality of adjacent streets.**

**Finding**—The proposed development is surrounded by residentially zoned property. The development proposes to close one of the existing two driveway approaches currently located across from a residentially zoned area. The remaining driveway approach is located across from a residentially zoned area. The driveway will not have an effect on the functionality of the adjacent streets.

Prepared by: Jennifer Scott, Project Coordinator  
cc: File