

NOTICE OF DECISION

PLANNING DIVISION
555 LIBERTY ST. SE, RM 305
SALEM, OREGON 97301
PHONE: 503-588-6173
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*Si necesita ayuda para comprender esta informacion, por favor llame
503-588-6173*

DECISION OF THE PLANNING ADMINISTRATOR

**CLASS 3 SITE PLAN REVIEW / CLASS 2 ADJUSTMENT / CLASS 2 DRIVEWAY
APPROACH PERMIT CASE NO.: SPR-ADJ-DAP20-03**

APPLICATION NO.: 19-121324-RP / 19-121325-ZO / 19-123268-ZO

NOTICE OF DECISION DATE: FEBRUARY 14, 2020

SUMMARY: Site plan review for the addition of classrooms, performing arts classrooms and theater, new auxiliary gym, field house and parking area of the existing South Salem High School with an adjustment for a reduction in to the setback abutting the street, amount of off-street parking, distance to primary entrance for bike parking and reduction in interior landscaping.

DESCRIPTION: Class 3 Site Plan Review for the addition of classrooms, performing arts classrooms and theater, new auxiliary gym, field house and parking area of the existing Salem High School. An alternative street standard is requested for Howard Street, Church Street and Rural Street abutting the subject property. Driveway Approach Permits for five driveways to be constructed or reconstructed. Two Class 2 Adjustments are requested for:

- 1) A reduction to minimum off-street parking requirements from 375 spaces to 253 spaces,
- 2) A reduction to minimum 20-foot setback abutting a street in order to construct an addition two feet from Howard Street and 10 feet abutting Church Street.
- 3) A reduction to the minimum distance from primary entrance for bicycle parking from 50-feet to 500-Feet.
- 4) A reduction to the requirement for interior landscaping for off-street parking areas.

For property approximately 22.09 acres in size, zoned PE (Public/Private Education) and located at 1910 Church Street SE - 97301 (Marion County Assessor Map and Tax Lot number: 073W34AC / 100, 073W34AD / 15500 AND 073W34AD / 15400).

APPLICANT: Salem Keizer School District

LOCATION: 1910 Church St SE

CRITERIA: Salem Revised Code (SRC) Chapters 220.005(f)(3) – Class 3 Site Plan Review; 250.005(d)(2) – Class 2 Adjustment; and 804.025(d) – Class 2 Driveway Approach Permit

FINDINGS: The findings are in the attached Decision dated February 14, 2020.

DECISION: The **Planning Administrator APPROVED** Class 3 Site Plan Review / Class 2 Adjustment / Class 2 Driveway Approach Permit Case No. SPR-ADJ-DAP20-03 subject to the following conditions of approval:

Condition 1: Three Months after final occupancy is provided by the Building and

Safety Division, all modular buildings shall be removed from the subject property or the applicant shall meet all development standards of the Salem Revised Code.

Condition 2: At building permit, the 188 bicycle parking spaces shall be located outside of setback areas and meeting the standards of SRC 806.060, except as approved below to be no further than 500 feet from the primary entrance.

Condition 3: Reconstruct the curb ramp at the intersection of Church Street SE and Howard Street SE, including AC pavement restoration as needed.

Condition 4: Convey land for dedication to equal a half-width right-of-way of 30 feet on the development side of Howard Street SE, including sufficient right-of-way to accommodate public infrastructure at the property corners.

Condition 5: Construct a 15-foot half-street improvement on Howard Street SE from Winter Street SE to Davidson Street SE to Local street standards as specified in the City Street Design Standards and consistent with the provisions of SRC Chapter 803.

Condition 6: Provide a public sidewalk easement along the development frontage of Howard Street SE and Church Street SE for portions of sidewalk that are not located within the right-of-way.

Condition 7: Construct sidewalk, as shown on site plan along the frontage of Howard Street SE and Rural Street SE. In addition, sidewalks not in conformance with current PWDS standards and ADA cross slope standards shall be replaced along the development frontage of Church Street SE.

Condition 8: The applicant shall plant shade trees abutting the frontage of Rural Avenue, on private property, every 40-feet to the greatest extent possible, as shown on the site plan. These trees are in addition to the on-site landscaping requirements of SRC 807.

Condition 9: Prior to issuance of building permits, excluding temporary modular buildings, a performance guarantee pursuant to SRC 77.122 or SRC 110.100 in an amount not to exceed \$15,000 for construction of traffic calming devices according to a traffic analysis performed within 1 year of occupancy of the proposed school additions. The performance guarantee shall be released a maximum of three (3) years after occupancy for the proposed use.

Condition 10: Design and construct a storm drainage system at the time of development in compliance with SRC Chapter 71 and PWDS.

Condition 11: Provide a restoration plan and performance security pursuant to SRC Chapter 110 for the temporary modular unit gravel pad proposed on Rural Street SE.

Condition 12: At least 50 of the required bicycle parking spaces shall be covered with a roof or awning that provides protection from the rain.

Condition 13: The proposed driveway at Winter Street SE and Howard Street SE shall be redesigned to meet PWDS.

The rights granted by the attached decision must be exercised, or an extension granted, by the below dates or this approval shall be null and void.

- Class 3 Site Plan Review: March 3, 2024
- Class 2 Adjustment and Driveway Approach Permit: March 3, 2022

Application Deemed Complete: January 22, 2020
Notice of Decision Mailing Date: February 14, 2020
Decision Effective Date: March 3, 2020
State Mandate Date: May 21, 2020

Case Manager: Olivia Dias, Planner III, 503-540-2343, odias@cityofsalem.net

This decision is final unless written appeal from an aggrieved party is filed with the City of Salem Planning Division, Room 320, 555 Liberty Street SE, Salem OR 97301, no later than 5:00 p.m., Monday, March 2, 2020. The notice of appeal must contain the information required by SRC 300.1020 and must state where the decision failed to conform to the provisions of the applicable code section, SRC Chapter(s) 220, 250, and 804. The appeal must be filed in duplicate with the City of Salem Planning Division. The appeal fee must be paid at the time of filing. If the appeal is untimely and/or lacks the proper fee, the appeal will be rejected. The Hearings Officer will review the appeal at a public hearing. After the hearing, the Hearings Officer may amend, rescind, or affirm the action, or refer the matter to staff for additional information.

The complete case file, including findings, conclusions and conditions of approval, if any, is available for review at the Planning Division office, Room 320, City Hall, 555 Liberty Street SE, during regular business hours.

<http://www.cityofsalem.net/planning>

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503-588-6173***

BEFORE THE PLANNING ADMINISTRATOR OF THE CITY OF SALEM

**SITE PLAN REVIEW / ADJUSTMENT / DRIVEWAY APPROACH CASE NO. SPR-
ADJ-DAP 20-03
DECISION**

**IN THE MATTER OF APPROVAL OF) CLASS 3 SITE PLAN REVIEW
SITE PLAN REVIEW, ADJUSTMENT) CLASS 2 ADJUSTMENT AND
DRIVEWAY APPROACH CASE NO. 20-03) CLASS 2 DRIVEWAY APPROACH
) PERMIT
)
)
1910 CHURCH STREET SE - 97301) FEBRUARY 14, 2020**

In the matter of the application for a Class 3 Site Plan Review, Class 2 Adjustment, Class 2 Driveway Approach Permits submitted by Salem-Keizer Public Schools, represented by Joel Smallwood, and Anderson Shirley Architects, represented by Pamela Triplett, the Planning Administrator, having received and reviewed evidence and the application materials, makes the following findings and adopts the following order as set forth herein.

REQUEST

Summary: Site plan review for the addition of classrooms, performing arts classrooms and theater, new auxiliary gym, field house and parking area of the existing South Salem High School with an adjustment for a reduction in to the setback abutting the street, amount of off-street parking, distance to primary entrance for bike parking and reduction in interior landscaping.

Description: Class 3 Site Plan Review for the addition of classrooms, performing arts classrooms and theater, new auxiliary gym, field house and parking area of the existing Salem High School. An alternative street standard is requested for Howard Street, Rural Street and Church Street abutting the subject property. Driveway Approach Permits for five driveways to be constructed or reconstructed. Four Class 2 Adjustments are requested for:

- 1) A reduction to minimum off-street parking requirements from 375 spaces to 267 spaces,
- 2) A reduction to minimum 20-foot setback abutting a street in order to construct an addition two feet from Howard Street and 10 feet abutting Church Street.
- 3) A reduction to the minimum distance from primary entrance for bicycle parking from 50-feet to 500-Feet.
- 4) A reduction to the requirement for interior landscaping for off-street parking areas.

For property approximately 22.09 acres in size, zoned PE (Public/Private Education) and located at 1910 Church Street SE - 97301 (Marion County Assessor Map and Tax Lot number: 073W34AC / 100, 073W34AD / 15500 AND 073W34AD / 15400).

A vicinity map illustrating the location of the property is attached hereto, and made a part of this staff report (**Attachment A**).

DECISION

APPROVED subject to the applicable standards of the Salem Revised Code, the findings contained herein, conformance with the approved site plans, and the following conditions of approval:

- Condition 1:** Three Months after final occupancy is provided by the Building and Safety Division, all modular buildings shall be removed from the subject property or the applicant shall meet all development standards of the Salem Revised Code.
- Condition 2:** At building permit, the 188 bicycle parking spaces shall be located outside of setback areas and meeting the standards of SRC 806.060, except as approved below to be no further than 500 feet from the primary entrance.
- Condition 3:** Reconstruct the curb ramp at the intersection of Church Street SE and Howard Street SE, including AC pavement restoration as needed.
- Condition 4:** Convey land for dedication to equal a half-width right-of-way of 30 feet on the development side of Howard Street SE, including sufficient right-of-way to accommodate public infrastructure at the property corners.
- Condition 5:** Construct a 15-foot half-street improvement on Howard Street SE from Winter Street SE to Davidson Street SE to Local street standards as specified in the City Street Design Standards and consistent with the provisions of SRC Chapter 803.
- Condition 6:** Provide a public sidewalk easement along the development frontage of Howard Street SE and Church Street SE for portions of sidewalk that are not located within the right-of-way.
- Condition 7:** Construct sidewalk, as shown on site plan along the frontage of Howard Street SE and Rural Street SE. In addition, sidewalks not in conformance with current PWDS standards and ADA cross slope standards shall be replaced along the development frontage of Church Street SE.
- Condition 8:** The applicant shall plant shade trees abutting the frontage of Rural Avenue, on private property, every 40-feet to the greatest extent

possible, as shown on the site plan. These trees are in addition to the on-site landscaping requirements of SRC 807.

Condition 9: Prior to issuance of building permits, excluding temporary modular buildings, a performance guarantee pursuant to SRC 77.122 or SRC 110.100 in an amount not to exceed \$15,000 for construction of traffic calming devices according to a traffic analysis performed within 1 year of occupancy of the proposed school additions. The performance guarantee shall be released a maximum of three (3) years after occupancy for the proposed use.

Condition 10: Design and construct a storm drainage system at the time of development in compliance with SRC Chapter 71 and PWDS.

Condition 11: Provide a restoration plan and performance security pursuant to SRC Chapter 110 for the temporary modular unit gravel pad proposed on Rural Street SE.

Condition 12: At least 50 of the required bicycle parking spaces shall be covered with a roof or awning that provides protection from the rain.

Condition 13: The proposed driveway at Winter Street SE and Howard Street SE shall be redesigned to meet PWDS.

FINDINGS

1. Class 3 Site Plan Review Applicability

Site plan review is intended to provide a unified, consistent, and efficient means to review proposed development that requires a building permit, other than single-family, duplex residential, and installation of signs, to ensure that such development meets all applicable requirements imposed by the Salem Revised Code (SRC). SRC 220.005(b)(3) requires Class 3 Site Plan Review for any development that requires a building permit, and that involves a land use decision or limited land use decision, as those terms are defined in ORS 197.015.

Class 3 Site Plan Review is required for this application pursuant to SRC 220.005(b)(3)(F) because a Class 2 Adjustment, and Class 2 Driveway Approach Permit has been requested to reduce the setback abutting a street requirement and reduction of off-street parking requirement for the proposed use.

2. Background

On October 7, 2019 Class 3 Site Plan Review and Class 2 Adjustment applications were filed for the proposed development. Five Class 2 Driveway Approach Permits were submitted on November 4, 2019. After additional information was provided, the applications were deemed complete for processing on January 22, 2020. The

applicant's proposed site plan is included as **Attachment B** and the applicant's written statement addressing the approval criteria is included as **Attachment C**.

Summary of Record:

The following items are submitted to the record and are available upon request: All materials submitted by the applicant, including any applicable professional studies such as traffic impact analysis, geologic assessments, and stormwater reports; any materials and comments from public agencies, City departments, neighborhood associations, and the public; and all documents referenced in this report.

Neighborhood and Citizen Comments:

Notice of the application was sent to the South Central Association of Neighbors (SCAN), and all property owners of record within 250 feet of the subject property. The SCAN Transportation Committee submitted comments, which were subsequently approved by the Board. In addition, one comment in support, two comments with neutral standing and 25 comments in opposition were received from surrounding property owners the following concerns are summarized below:

Construction Noise:

Citizen testimony received with concerns about construction hours and parking for construction crews.

Staff Response: Noise disturbances are prohibited by SRC Chapter 93, and construction activities are specifically limited to the hours of 7 a.m. to 10 p.m. by SRC 93.020(d). The level of allowable noise during construction activities is also limited by state law. SRC Chapter 93 also prohibits idling engines on motor vehicles in a manner that is plainly audible within any dwelling unit for more than 10 minutes between the hours of 10 p.m. and 7 a.m.

Approval criteria for a site plan review does not specifically address noise levels, and no evidence has been provided that would indicate that the proposed development or construction would interfere with the safe and healthful use of neighboring properties.

SRC Chapter 51 also regulates noise levels, and the proposed development is subject to these regulations. Specifically, SRC 51.015 provides maximum sound levels based on the source and receiver of the sound. It is unlawful to exceed the maximum sound levels without an event sound permit. The Neighborhood Enhancement division of the Community Development Department enforces these noise regulations.

Pedestrian Connectivity and Sidewalks:

Citizen testimony about the width of sidewalks abutting the school being five-feet, instead of eight-feet was received. Comments also addressed the proposed parking areas, stating that they do not appear to meet the pedestrian standards within the parking area.

Staff Response: The new addition and existing building have six connections to abutting streets, including a connection from the auxiliary gym to Rural Ave. There is not a transit route or stop abutting the property. The new modulars have a connection to Rural Ave.

The proposal includes two new vehicles use areas. The north parking area includes more than four drive aisles and is greater than 124-feet in depth. The site plan indicates that pedestrian paths will be provided from streets to building entrances and across parking areas, meeting the standards.

The remainder of the site does not contain an off-street parking area greater than 124-feet and is not required to provide pedestrian connections.

Historic Significance:

Citizen testimony received about the historical significance of Leslie Junior High (1927) and Howard Street School, which are located on the site and proposed to be demolished or remodeled.

Staff Response: The district has applied for, and been granted, Historic Clearance from the State Historic Preservation Office, as required by state law. Additionally, at the Salem Historic Landmarks Commission (HLC) meeting on January 16, 2020, the Salem Historic Landmarks Commission reviewed the proposed mitigation submitted by the Salem-Keizer School District for the demolition of the original 1927 Leslie Middle School Building, and voted unanimously to support their proposal to develop a video and develop an interpretive panel to be installed on the site describing the history and significance of the school. The HLC voted to appoint HLC Commissioner Andrew Zimmerman to coordinate with the Salem-Keizer School District in the development of the video and other on-site interpretation.

Shadows impact existing homes:

Citizen testimony was received about the impact of the building on neighboring residents by casting shadows.

Staff Response: The proposed building meets the height requirements of the zoning district. Potential shadows are not an approval criterion.

Trees and Clark Creek:

Citizen testimony was received that the creek remain untouched and mature vegetation remain.

Staff Response: The proposal does not affect Clark Creek; the proposed addition and development is more than 100-feet from the Creek.

Adding Trees:

Citizen testimony was received requesting additional trees be planted along the frontages of the property.

Staff Response: The proposed site plan adds several additional trees on private property and within the right-of-way. As conditioned below, the applicant will be required to plant the maximum amount of Street Trees to meet SRC 86 along the frontage of the abutting streets. There is an additional condition of approval acknowledging the elimination of street trees along Rural Street, with a requirement to locate those on private property. The applicant will be adding a substantial amount of private property trees and street trees.

Traffic Impact Analysis (TIA):

Citizen testimony was received that a Traffic Impact Analysis is warranted. Traffic in the area is "gridlocked" or near standstill during events and pickup/drop-off times. Increase of "driving age students" from the proposal of removing middle school and adding more capacity for the High School.

Staff Response: According to Salem Revised Code 803.015(b)(1), TIAs are required for developments that generate over 1,000 daily vehicle trips on a Collector street. The Assistant City Traffic Engineer has determined that the proposed school addition generates less than 1,000 daily trips. Rural Avenue S is a Collector street as shown in the *Salem Transportation System Plan*. Street classification is based on the *Salem Transportation System Plan*, not the existing street condition.

Staff have reviewed the citizen testimony and will require security (money) for general traffic calming measures to address the potential impacts of the proposed development. Other improvements suggested by testimony will need to be evaluated under a separate review and are not included as part of this application.

Proposed Median at Church and Oxford Street

Citizen testimony objects to the proposed traffic mitigation measures at the intersection of Church Street SE and Oxford Street SE.

Staff response: The proposed traffic mitigation measures are based on a voluntary traffic analysis and recommended by the applicant's traffic engineer. The proposal will be further reviewed as part of the plan approval process pursuant to SRC 77.091. Staff is recommending the school district reevaluate one year after the expansion is complete to determine if the mitigation measures are still necessary.

Sidewalk and Driveway Improvements

Testimony suggests that driveway entrances should be brought into compliance to improve pedestrian safety and access.

Staff response: Sidewalks along the development frontages are being replaced to an 8-foot width as required by Public Works Design Standards and summarized in the

conditions of approval. New and replaced driveway approaches must be replaced to meet current Public Works Design Standards as specified in the conditions of approval.

Crosswalk Markings

Citizen testimony suggests that crosswalks at intersections along Rural Avenue, Church Street and Howards Street should marked on both sides of the intersections.

Staff response: Current markings are intentional to create an orderly crossing pattern around the school. Students are encouraged to cross at a single point rather than multiple locations for safety reasons. Multiple crossing in the area may create unnecessary traffic conflicts with pedestrians and vehicles. Staff does not support additional crossings at this time but will reevaluate after occupancy of the proposed use.

Traffic pattern modifications and Traffic Calming:

Citizen testimony received that Church Street and/or Oxford Street should be configured for one-way traffic, signage be placed along ally entrances "local access only", a three-way stop intersection at Church Street and Oxford Street and speed bumps, flashing crossing and signage be added for traffic calming.

Staff Response: Staff have reviewed the citizen testimony and will require security (money) for general traffic calming measures to address the potential impacts of the proposed development. Other improvements suggested by testimony will need to be evaluated under separate review and are not included as part of this application.

On-Site Parking Adjustment:

Citizen testimony received in opposition to the parking reduction, due to the impact on the surrounding neighborhood.

Staff Response: South Salem High School has historically used on-street parking to support the parking needs of visitors and students. The applicant is providing a total of 267 spaces, which includes an additional parking area along Howard Street, which will provide an additional 97 parking spaces. The existing school is located near mass transit stops, is located within a residential neighborhood and has on-street parking around the school. Students attending the school are able to walk to the school from the surrounding neighborhood or from the transit stop, less than a half mile from the school.

In addition, the applicant has provided evidence that there are 73 parking spaces along Church Street and Rural Street, 25 parallel parking spaces along Rural Street and 87 additional spaces within 500-feet of the school. The total spaces on-site and off-site are more than the required 375 spaces. The school will be served by on-street and off-street parking spaces totaling 452 spaces, which equally or better meets the intent of the Code.

Bicycle Parking:

There was one comment received requesting an increased adjustment to the distance for Bike Parking and a reduction in Bike Parking for the project. Comments indicated

that school children are in need of bike parking around the entire school, not just near the entrances and should be provided near the sports fields. In addition, several of the proposed spaces are located in the rear of the school which is adjacent to the bus route and vehicle use areas where there is a potential for a conflict between bicyclist and cars.

Staff Response: The applicant has applied to adjust the distance for the bike parking to no more than 500-feet from the primary entrance. The applicant provided a response (**Attachment D**), which disperses the bike parking across campus and eliminates the conflict with the driveway in the rear of the subject property. As described below, the adjustment could equally or better meet the standard with the condition of approval to cover 50 of the bicycle parking spaces.

The applicant provided response to individual comments summarized above in **Attachment D**.

City Department Comments:

The Public Works Department reviewed the proposal and provided a memo which is included as **Attachment E**.

The Building and Safety Division reviewed the proposal and indicated no site concerns. Fire rated construction is required at some locations, which will be addressed at plans review.

The Fire Department has reviewed the proposal and indicated that Aerial Fire Department access will be required from Howard Street to the proposed addition. The existing public streets and interior bus route will provide the remaining Fire Department access to the high school. The track will provide fire department access to the temporary modular buildings. Fire will provide additional comments at the time of building permit plan review.

Public Agency Comments:

Portland General Electric (PGE) reviewed the proposal and commented, "Development cost per current tariff and service requirements." The School District is working with PGE to relocate existing powerlines underground.

Cherriots commented that the existing sidewalks and driveway entrances should be brought to a state of good repair in order to improve pedestrian safety and access.

Staff Response: The applicant is proposing to replace sidewalks at the intersection of Church street and Howard Street, along the south side of Howard Street to Summer Street and improvements to the sidewalks along the north side of Rural Avenue. In addition, the applicant will be required to replace any non-compliant sidewalks along the development site.

3. Analysis of Class 3 Site Plan Review Approval Criteria

SRC 220.005(f)(3) states:

An application for Class 3 Site Plan Review shall be granted if:

- (1) The application meets all applicable standards of the UDC;
- (2) The transportation system provides for the safe, orderly, and efficient circulation of traffic into and out of the proposed development, and negative impacts to the transportation system are mitigated adequately;
- (3) Parking areas and driveways are designed to facilitate safe and efficient movement of vehicles, bicycles, and pedestrians; and
- (4) The proposed development will be adequately served with City water, sewer, stormwater facilities, and other utilities appropriate to the nature of the development.

Criterion 1:

The application meets all applicable standards of the UDC.

Finding: The proposed development includes an expansion of an existing high school, with a new building addition increasing the number of classrooms, new auxiliary gym and parking lot improvements. The application also includes four adjustment requests to reduce the number of required off-street parking spaces, to reduce the setback abutting the street, increase the distance of bicycle parking from main entrance and reduce interior landscaping.

Development Standards – PE (Public and Private Education Services) Zone:

SRC 542.005(a) – Uses:

Finding: The proposal includes an expansion of the existing South Salem High School, which is classified as a basic education use. Basic education is allowed as a permitted use in the PE zone per SRC Chapter 542, Table 542-1.

SRC 542.010(a) – Lot Standards:

There is a minimum lot area requirement of 10,000 square feet for all uses. The minimum lot width is 50 feet, minimum lot depth is 80 feet. All uses are required to have a minimum of 16 feet of street frontage.

Finding: The subject property is approximately 22 acres, has a width of approximately 790 feet and depth of approximately 1,100 feet. The lot has approximately 1,100 feet of frontage along Howard Street SE and Rural street SE and approximately 790 feet of frontage along Church street SE. The subject property is in compliance with the minimum lot standards of the PE zone.

SRC 542.010(b) – Setbacks:

North: Adjacent to the north is right-of-way for Howard Street SE. Buildings and accessory structures less than 35 feet in height require a minimum 20-foot setback

adjacent to a street or special setback line, and vehicle use areas require a minimum 6-10-foot vehicle use area setback adjacent to a street.

South: Adjacent to the south is right-of-way for Rural Street SE. Buildings and accessory structures less than 35 feet in height require a minimum 20-foot setback adjacent to a street or special setback line, and vehicle use areas require a minimum 6-10-foot vehicle use area setback adjacent to a street.

East: Adjacent to the east is a RM-II (Multiple Family Residential) zone. Buildings and accessory structures not more than 35 feet in height require a minimum 20-foot setback adjacent to a residential zone, buildings and accessory structures greater than 35 feet in height require a minimum 30-foot setback. Vehicle use areas require a minimum 20-foot setback adjacent to a residential zone.

West: Adjacent to the west is right-of-way for Church Street SE. Buildings and accessory structures less than 35 feet in height require a minimum 20-foot setback adjacent to a street or special setback line, and vehicle use areas require a minimum 6-10-foot vehicle use area setback adjacent to a street.

Finding: The applicant has applied for an alternative street standard for Howard Street and Church Street, which is addressed below. The alternative street standard removed the special setback for the abutting streets. Setbacks are reviewed to the new property line after right-of-way dedication required below.

The applicant is requesting an alternative street standard, discussed below. The Alternative Street Standard will eliminate the special setback requirement and the proposal is reviewed based on the new right-of-way width. The new building addition is approximately two feet from the property line abutting Howard Street and ten feet from the property line abutting Church Street. Findings for the Adjustment to reduce the setback from 20 feet are included in Section 4 of this report.

The proposed auxiliary gym meets applicable setbacks. The new parking area abutting Rural is located 10-feet from the property line, meeting the standard. As a condition of approval a half street improvement is required for the abutting portion of Howard Street. Any change in the site plan will still be required to maintain setbacks. The site has adequate space to meet all applicable setback requirements.

The new field house and new modular classrooms are over 20-feet from any property line thereby meeting the standard.

SRC 542.010(c) – Lot Coverage, Height:

The maximum lot coverage requirement for buildings and structures in the PE zone is 50 percent and the maximum height allowance for all buildings and structures is 70 feet.

Finding: The applicant's summary table indicates that the existing/proposed building coverage is approximately 962,270 square feet, covering approximately 20 percent of the total site, less than the maximum lot coverage requirement. New buildings will have a maximum height of approximately 52 feet, six inches, less than the maximum height allowance.

SRC 542.010(d) – Landscaping:

- (1) **Setbacks.** Required setbacks shall be landscaped. Landscaping shall conform to the standards set forth in SRC Chapter 807.
- (2) **Vehicle Use Areas.** Vehicle use areas shall be landscaped as provided under SRC Chapter 806 and SRC Chapter 807.

Finding: The proposal includes perimeter setbacks and landscaping, meeting or exceeding minimum requirements. At the time of building permit review, landscape and irrigation plans shall be provided that demonstrate compliance with the requirements of SRC Chapter 807.

SRC 542.010(e) – Outdoor Storage:

Within the PE zone, outdoor storage shall be screened from streets and adjacent properties by a minimum six-foot-high sight-obscuring fence, wall, or hedge.

Finding: No outdoor storage areas are proposed, this standard is not applicable to the proposed development.

Pedestrian access SRC. 800.065

Pedestrian connections required.

The on-site pedestrian circulation system shall provide pedestrian connectivity throughout the development site as follows:

- (1) Connection between building entrances and streets.
 - (A) A pedestrian connection shall be provided between the primary building entrance of each building on the development site and each adjacent street. Where a building has more than one primary building entrance, a single pedestrian connection from one of the building's primary entrances to each adjacent street is allowed; provided each of the building's primary entrances are connected, via a pedestrian connection, to the required connection to the street.
 - (B) Where an adjacent street is a transit route and there is an existing or planned transit stop along street frontage of the development site, at least one of the required pedestrian connections shall connect to the street within 20 feet of the transit stop.
- (2) Connection between buildings on the same development site. Where there is more than one building on a development site, a pedestrian connection, or pedestrian connections, shall be provided to connect the primary building entrances of all of the buildings.
- (3) Connection through off-street parking areas.
 - (A) Surface parking areas. Except as provided under subsection (a)(3)(A)(iii) of this section, off-street surface parking areas greater than 25,000 square feet in size or including four or more consecutive parallel drive aisles shall include pedestrian connections through the parking area to the primary building entrance as provided in this subsection.
 - (i) The pedestrian connections:

- a. Shall be provided in a minimum amount of either one connection for every four drive aisles or one connection for every 250 feet (See Figure 800-8); provided, however, in no case shall less than one pedestrian connection be provided. Where the pedestrian connection requirements of this subsection result in a fractional number, any fractional number greater than 0.5 shall be round up to require an additional pedestrian connection;
 - b. Shall be spaced a minimum of two drive aisles apart; and
 - c. Shall connect to a pedestrian connection, or pedestrian connections, that lead to the primary building entrance.
- (iii) Regardless of the size of the off-street parking area, pedestrian connections are not required through off-street surface parking areas that have a depth, in all locations, of not more than 124 feet. For purposes of this subsection, parking area depth is measured through the parking area from its outside edge towards the building.

Design and materials.

Required pedestrian connections shall be in the form of a walkway or may be in the form of a plaza.

- (1) Walkways shall conform to the following:
 - (A) Material and width. Walkways shall be paved with a hard-surface material meeting the Public Works Design Standards, and shall be a minimum of five feet in width.
 - (B) Where a walkway crosses driveways, parking areas, parking lot drive aisles, and loading areas, the walkway shall be visually differentiated from such areas through the use of elevation changes, a physical separation, speed bumps, a different paving material, or other similar method. Striping does not meet this requirement, except when used in a parking structure or parking garage.
 - (C) Where a walkway is located adjacent to an auto travel lane, the walkway shall be raised above the auto travel lane or separated from it by a raised curb, bollards, landscaping or other physical separation. If the walkway is raised above the auto travel lane it must be raised a minimum of four inches in height and the ends of the raised portions must be equipped with curb ramps. If the walkway is separated from the auto travel lane with bollards, bollard spacing must be no further than five feet on center.
- (2) Wheel stops or extended curbs shall be provided along required pedestrian connections to prevent the encroachment of vehicles onto pedestrian connections.

Lighting. The on-site pedestrian circulation system shall be lighted to a level where the system can be used at night by employees, customers, and residents.

Finding: The new addition and existing building has six connections to abutting streets, including a connection from the auxiliary gym to Rural Street. There is not a transit route or stop abutting the property. The new modulars have a connection to Rural Ave.

The proposal includes two new vehicles use areas. The north parking area includes more than four drive aisles and is greater than 124-feet in depth. The site plan indicates that pedestrian paths will be provided from streets to building entrances and across parking areas, meeting the standards.

The remainder of the site does not contain an off-street parking area greater than 124-feet and is not required to provide pedestrian connections.

Off-Street Parking, Loading, and Driveways SRC 806

SRC 806.005 - Off-Street Parking; When Required.

Off-street parking shall be provided and maintained for any intensification, expansion, or enlargement of a use or activity.

SRC 806.010 - Proximity of Off-Street Parking to Use or Activity Served.

Required off-street parking shall be located on the same development site as the use or activity it serves, or per 806.010(b), within public zones, required off-street parking may be located within 500 feet of the development site containing the use or activity it serves.

Finding: All off-street parking provided for the use will be located on the same development site as the schools.

SRC 806.015 - Amount of Off-Street Parking.

- a) *Minimum Required Off-Street Parking.* Basic education uses, other than elementary schools, require a minimum parking requirement of 1 space per 6 students. The number of students shall be calculated based on the total number of students the school is designed to accommodate.
- b) *Compact Parking.* Up to 75 percent of the minimum off-street parking spaces required under this Chapter may be compact parking spaces.
- c) *Carpool and Vanpool Parking.* New developments with 60 or more required off-street parking spaces, and falling within the Public Services and Industrial use classifications, and the Business and Professional Services use category, shall designate a minimum of 5 percent of their total off-street parking spaces for carpool or vanpool parking.
- d) *Maximum Off-Street Parking.* Unless otherwise provided in the SRC, off-street parking shall not exceed the amounts set forth in Table 806-2.

Finding: The proposed development will increase the number of classrooms for the high school to 94, the maximum student capacity for the facility is 2,248 students, after temporary modular buildings are removed. A minimum of 375 off-street parking spaces are required for the proposed use ($2,248 / 6 = 374.6$). The applicant is requesting a reduction to the off-street parking requirement for the proposed use, reducing the amount of required spaces from 375 to 267 spaces. The existing school does have on-street parking located along Rural Street, Howard Street and Church Street. Findings

for the Adjustment are included in Section 4 of this report. According to the applicant's statement, the temporary modular buildings are to be removed once the remainder of the school is complete. To ensure the development site meet the standards, the following condition of approval applies:

Condition 1: Three Months after final occupancy is provided by the Building and Safety Division, all modular buildings shall be removed from the subject property or the applicant shall meet all development standards of the Salem Revised Code.

SRC 806.035 - Off-Street Parking and Vehicle Use Area Development Standards.

- a) *General Applicability.* The off-street parking and vehicle use area development standards set forth in this section apply to the development of new off-street parking and vehicle use areas.
- b) *Location.* Off-street parking and vehicle use areas shall not be located within required setbacks.
- c) *Perimeter Setbacks and Landscaping.* Perimeter setbacks shall be required for off-street parking and vehicle use areas abutting streets, abutting interior front, side, and rear property lines, and adjacent to buildings and structures.

Adjacent to Buildings and Structures: The off-street parking or vehicle use area shall be setback from the exterior wall of the building or structure by a minimum 5 foot wide landscape strip or by a minimum 5 foot wide paved pedestrian walkway.

Finding: The existing off-street parking area will remain. A new paved emergency vehicle access road is proposed which complies with the minimum perimeter vehicle use area setback standards identified in the PE zone development standards and by SRC Chapter 806.

- d) *Interior Landscaping.* Interior landscaping shall be provided in amounts not less than those set forth in Table 806-5. No interior landscaping is required for off-street parking areas less than 5,000 square feet in size.

Finding: Interior landscaping is required for the proposed new vehicle use area. The proposed new 20,000 square foot vehicle use area requires, 1,000 square feet of landscaping. The plans indicate more than 4,000 square feet of interior landscaping is being provided.

The proposed parking area adjacent to Rural Avenue is 33,533 with 750 landscaping; West of Howard Street is 11,694 square feet, with 1,147 landscaping. East of Howard Street, 35, 534 square feet with 4,390 square feet is proposed.

The proposed southern off-street parking area (10,200 square feet) is providing 360 square feet of landscaping, where 510 square feet is required. Findings for the Adjustment are included in Section 4 of this report.

- e) *Off-Street Parking Area Dimensions.* Off-street parking areas shall conform to the minimum dimensions set forth in Table 806-6.

Finding: No change to the existing off-street parking space striping is proposed, this standard is not applicable.

- f) *Additional Off-Street Parking Development Standards 806.035(f)-(m).*

Finding: The proposed off-street parking area is developed consistent with the additional development standards for grade, surfacing, and drainage. Bumper guards and wheel barriers are not required for the proposed vehicle use area. The parking area striping, marking, signage is not required for the emergency vehicle access road. Lighting shall comply with SRC 806.035(l). Off-street parking area screening per SRC 806.035(m) is required for the proposed emergency vehicle access road.

Bicycle Parking

SRC 806.045 - General Applicability.

Bicycle parking shall be provided and maintained for any intensification, expansion, or enlargement of a use or activity.

SRC 806.050 – Proximity of Bicycle Parking to use or Activity Served.

Bicycle parking shall be located on the same development site as the use or activity it serves.

SRC 806.055 - Amount of Bicycle Parking.

Per SRC Chapter 806, Table 806-8, basic education uses require a minimum of four bicycle parking spaces per classroom.

Finding: The proposed development will increase the number of classrooms for the high school to 94 classrooms. A minimum of 188 bicycle parking spaces are required for the proposed use ($94 \times 2 = 188$). The applicant is proposing a minimum of 188 spaces.

SRC 806.060 – Bicycle Parking Development Standards.

- (a) Location. Except as otherwise provided in this section, bicycle parking shall be located outside a building.
- (1) Bicycle parking located outside a building shall be located within a convenient distance of, and be clearly visible from, the primary building entrance. In no event shall bicycle parking be located more than 50 feet from the primary building entrance, as measured along a direct pedestrian access route.
 - (2) Where bicycle parking cannot be located outside a building, it may be located inside a building within a convenient distance of, and accessible from, the primary building entrance.
- (b) Access. Bicycle parking areas shall have direct and accessible access to the public right-of-way and the primary building entrance that is free of obstructions and any barriers, such as curbs or stairs, which would require users to lift their bikes in order to access the bicycle parking area.

- (c) Dimensions. Except as provided in subsection (f) of this section, bicycle parking areas shall meet the following dimension requirements:
 - (1) Bicycle parking spaces. Bicycle parking spaces shall be a minimum of six feet in length and two feet in width with the bicycle rack centered along the long edge of the bicycle parking space. Bicycle parking space width may be reduced, however, to a minimum of three feet between racks where the racks are located side-by-side.
 - (2) Access aisles. Bicycle parking spaces shall be served by a minimum four-foot-wide access aisle. Access aisles serving bicycle parking spaces may be located within the public right-of-way.
- (d) Surfacing. Where bicycle parking is located outside a building, the bicycle parking area shall consist of a hard surface material, such as concrete, asphalt pavement, pavers, or similar material, meeting the Public Works Design Standards.
- (e) Bicycle racks. Where bicycle parking is provided in racks, the racks may be floor, wall, or ceiling racks. Bicycle racks shall meet the following standards.
 - (1) Racks must support the bicycle frame in a stable position, in two or more places a minimum of six inches horizontally apart, without damage to wheels, frame, or components.
 - (2) Racks must allow the bicycle frame and at least one wheel to be locked to the rack with a high security, U-shaped shackle lock;
 - (3) Racks shall be of a material that resists cutting, rusting, and bending or deformation; and
 - (4) Racks shall be securely anchored.
 - (5) Examples of types of bicycle racks that do, and do not, meet these standards are shown in Figure 806-10.

Finding: The subject property is proposed to have 188 bicycle parking spaces. The applicant has proposed to locate the spaces in various locations on the school campus for the convenience of students. An adjustment to the minimum 50-foot distance from the primary entrance is addressed below in Section 4. The type, surfacing, and location standards are not clearly met. The applicant has not provided the type of bicycle rack proposed, which will be verified at building permit. Therefore, with the following condition the application meets SRC 806.060:

Condition 2: At building permit, the 188 bicycle parking spaces shall be located outside of setback areas and meeting the standards of SRC 806.060, except as approved below to be no further than 500 feet from the primary entrance.

Off-Street Loading Areas

SRC 806.065 - General Applicability.

Off-street loading areas shall be provided and maintained for any intensification, expansion, or enlargement of a use or activity.

SRC 806.070 – Proximity of Off-Street Loading Areas to use or Activity Served.

Off-street loading shall be located on the same development site as the use or activity it serves.

SRC 806.075 - Amount of Off-Street Loading.

A minimum of two off-street loading spaces are required for education services uses with a gross floor area between 60,001-250,000 square feet in size.

Finding: The proposed building additions will increase the floor area to approximately 132,052 square feet. No additional off-street loading spaces are required for the proposed development.

Landscaping

All required setbacks shall be landscaped with a minimum of 1 plant unit per 20 square feet of landscaped area. A minimum of 40 percent of the required number of plant units shall be a combination of mature trees, shade trees, evergreen/conifer trees, or ornamental trees. Plant materials and minimum plant unit values are defined in SRC Chapter 807, Table 807-2.

All building permit applications for development subject to landscaping requirements shall include landscape and irrigation plans meeting the requirements of SRC Chapter 807.

Finding: A minimum of 1 plant unit is required per 20 square feet of landscape area. A minimum of 40 percent of the required plant units shall be a combination of mature trees, shade trees, evergreen/conifer trees, or ornamental trees. Landscape and irrigation plans will be reviewed for conformance with the requirements of SRC 807 at the time of building permit application review.

Natural Resources

SRC 808 - Preservation of Trees and Vegetation: The City's tree preservation ordinance, under SRC Chapter 808, provides that no person shall remove a significant tree (Oregon White Oak greater than 24 inches in diameter at breast height) (SRC 808.015) or a tree or native vegetation in a riparian corridor (SRC 808.020), unless the removal is excepted under SRC 808.030(a)(2), undertaken pursuant to a permit issued under SRC 808.030(d), undertaken pursuant to a tree conservation plan approved under SRC 808.035, or permitted by a variance granted under SRC 808.045.

Finding: No protected riparian trees or significant trees have been identified on the site plan for removal.

SRC 809 - Wetlands: Grading and construction activities within wetlands are regulated by the Oregon Department of State Lands (DSL) and US Army Corps of Engineers. State and Federal wetland laws are also administered by the DSL and Army Corps, and potential impacts to jurisdictional wetlands are addressed through application and enforcement of appropriate mitigation measures.

Finding: According to the Salem-Keizer Local Wetland Inventory (LWI) there are no mapped wetlands or hydric soils found on the subject property.

SRC 810 - Landslide Hazards: A geological assessment or report is required when regulated activity is proposed in a mapped landslide hazard area. The development site contains 2-point mapped landslide hazards in the area of development. The proposed school development is assigned 3 activity points, which results in a total of five points. Therefore, the proposed development is classified as a moderate landslide risk and requires a geological assessment.

Finding: A Geotechnical Engineering Report, prepared by Professional Service Industries, Inc. and dated April 25, 2019, was submitted to the City of Salem. A supplemental Geotechnical Report Addendum dated June 26, 2019, was also submitted to the City of Salem. This assessment and subsequent addendum demonstrate the subject property can be developed without increasing the potential for slope hazard on the site or adjacent properties.

Public Works staff has reviewed the Flood Insurance Study and Flood Insurance Rate Maps and has determined that no floodplain or floodway areas exist on the subject property.

With completion of the conditions of approval, the subject property meets all applicable standards of the following chapters of the UDC: 601 – Floodplain, 802 – Public Improvements, 803 – Streets and Right-of-Way Improvements, 804 – Driveway Approaches, 805 – Vision Clearance, 809 – Wetlands, and 810 - Landslides.

Criterion 2:

The transportation system provides for the safe, orderly, and efficient circulation of traffic into and out of the proposed development, and negative impacts to the transportation system are mitigated adequately.

Finding: The subject property is bound by Howard Street to the north, Church Street to the west and Rural Street to the south. The existing configuration of all three streets do not meet current standards for their classification of street per the Salem Transportation System Plan (TSP).

Therefore, the applicant has requested an alternative street standard for Church Street, Rural Avenue and Howard Street.

Sec. 803.065. - Alternative street standards.

- a) The Director may authorize the use of one or more alternative street standards:
 1. Where existing development or physical constraints make compliance with the standards set forth in this chapter impracticable;
 2. Where the development site is served by fully developed streets that met the standards in effect at the time the streets were originally constructed;
or
 3. Where topography or other conditions make the construction that conforms to the standards impossible or undesirable.
- b) Authorization of an alternative street standard may require additional or alternative right-of-way width, easements, and improvements to accommodate the design and construction using the alternative standard.

The applicant has requested an alternative sidewalk standard to allow for portions of replaced sidewalk to be built as curb line sidewalks to accommodate the existing right-of-way configuration; due to the existing right-of-way the proposed configuration does not allow for street trees to be planted pursuant to SRC 86.015(e), therefore, they also request to eliminate all street trees.

Howard Street:

Howard Street is classified as a Local Street in the Salem Transportation System Plan, which requires a sixty-foot right-of-way, eight-foot sidewalks abutting property line, street trees, street lights and 30-feet of improvements. The existing configuration of Howard Street SE does not meet current standards for its classification of street per the Salem Transportation System Plan.

Howard Street has a varied 21-foot to 40-foot improvement within a 40-foot-wide right-of-way abutting the subject property. The applicant has requested an alternative street standard to maintain the existing 20-foot width on Howard Street SE from Winter Street SE to Davidson Street SE. In addition, an alternative street standard is requested to maintain the existing improvement width and sidewalk location on Howard Street SE from Church Street SE to Winter Street SE and that sidewalks not be located parallel to and one foot from the adjacent right-of-way (SRC 803.035(l)(2)(A)) but rather abutting the curb and eliminating the planter strip. The applicant has requested an alternative street standard to allow for the existing streetscape to remain with the exception of the curb ramp at Church Street SE and Howard Street SE.

The proposed site plans include new and replaced portions of 8-foot sidewalk. Sidewalks along the development frontage not in conformance with current Public Works Design Standards (PWDS) shall be replaced. The proposed curb ramp replacement at the corner of Church Street SE and Howard Street SE shall be replaced in conformance with PWDS, including Asphaltic Concrete (AC) pavement restoration as needed. To ensure the improvements are completed, the following conditions apply:

Condition 3: Reconstruct the curb ramp at the intersection of Church Street SE and Howard Street SE, including AC pavement restoration as needed.

Condition 4: Convey land for dedication to equal a half-width right-of-way of 30 feet on the development side of Howard Street SE, including sufficient right-of-way to accommodate public infrastructure at the property corners.

The City Traffic Engineer has reviewed the proposal to maintain the current pavement width and does not support this alternative street standard. The existing pavement width is not sufficient to accommodate for safe turning movements into and out of the proposed parking lot improvements on Howard Street SE. As specified in the conditions of approval, the applicant shall construct a half-street improvement to Local street standards as specified in the Public Works Design Standards and consistent with the provisions of SRC Chapter 803, including an 8-foot-wide sidewalk and street trees.

Condition 5: Construct a 15-foot half-street improvement on Howard Street SE from

Winter Street SE to Davidson Street SE to Local street standards as specified in the City Street Design Standards and consistent with the provisions of SRC Chapter 803.

As shown on the proposed site plan, the applicant shall convey land for dedication to equal a half-width right-of-way of 30 feet on the development side of Howard Street SE, including sufficient right-of-way to accommodate public infrastructure at the property corners. The applicant has requested an alternative street standard to provide a public sidewalk easement along the development frontage of Howard Street SE and Church Street SE for portions of sidewalk that are not located within the right-of-way.

Street standards require that sidewalks shall be located parallel to and one foot from the adjacent right-of-way (SRC 803.035(l)(2)(A)); however, findings provided by the School District demonstrate the need for a curb line sidewalk along Howard Street SE between Church St SE and Winter Street SE to accommodate accessible school bus drop off areas. The alternative street standard request to maintain the existing improvement width and sidewalk location on Howard Street SE from Church Street SE to Winter Street SE is approved. The alternative street standard is approved based pursuant to SRC 803.065(a)(1) and the need for an accessible bus drop off location.

Therefore, the sidewalk may be located along the curb line as shown on the proposed plan pursuant to SRC 803.035(l)(2)(B). New and replaced sidewalks shall be constructed a minimum of 8-feet in width pursuant to SRC 803.035(l)(2)(D).

Condition 6: Provide a public sidewalk easement along the development frontage of Howard Street SE and Church Street SE for portions of sidewalk that are not located within the right-of-way.

Pursuant to SRC 86.015(e), the applicant shall install street trees to the maximum extent feasible along all development frontages.

Church Street: Church Street is classified as a Local Street in the Salem Transportation System Plan, which requires a sixty-foot right-of-way, eight-foot sidewalks abutting property line, street trees, street lights and 30-feet of improvements. Church Street has an approximate 30-foot improvement within a 60-foot-wide right-of-way abutting the subject property, meeting the right-of-way width and pavement width standards per the Salem TSP.

The applicant has requested an alternative street standard to allow for the existing streetscape to remain with the exception of the curb ramp at Church Street SE and Howard Street SE. Staff has reviewed the proposed request and recommend sidewalks along the development frontage of Church Street SE not in conformance with current ADA cross slope standards be replaced. The proposed curb ramp replacement at the corner of Church Street SE and Howard Street SE shall be replaced in conformance with PWDS, including AC pavement restoration as needed to correct excessive running slope in the existing crosswalk. The alternative street standard request on Church Street SE to maintain the existing sidewalk location and width pursuant to SRC 803.065(a)(1) is approved.

Condition 7: Construct sidewalk, as shown on site plan along the frontage of Howard Street SE and Rural Street SE. In addition, sidewalks not in conformance with current PWDS standards and ADA cross slope standards shall be replaced along the development frontage of Church Street SE.

Rural Avenue: Rural Ave is classified as a Collector Street in the Salem Transportation System Plan, which requires a sixty-foot right-of-way, eight-foot sidewalks abutting property line, street trees, street lights and 40-feet of improvements. Rural Ave currently has a 60-foot right-of-way and 40-foot improvements, meeting the the right-of-way width and pavement width standards. The applicant has requested an alternative sidewalk standard to allow for portion of replaced sidewalk to be built as curb line sidewalk to accommodate the existing right-of-way configuration. The sidewalk may be located along the curb line as shown on the proposed plan pursuant to SRC 803.035(l)(2)(B). The existing right-of-way configuration does not allow for street trees to be planted pursuant to SRC 86.015(e), therefore, the alternative street standard to allow trees to be planted on private property is approved. Due to the site constraints and location of the sidewalks, the alterative street standard is approved with the following condition:

Condition 8: The applicant shall plant shade trees abutting the frontage of Rural Avenue, on private property, every 40-feet to the greatest extent possible, as shown on the site plan. These trees are in addition to the on-site landscaping requirements of SRC 807.

No special setback is required along the frontage of Church Street, Howard Street or Rural Avenue because of the land dedication or alternative street standards approved. However, all development, unless approved for an adjustment below, must meet the setbacks based on the new right-of-way lines, post dedication.

Criterion 3:

Parking areas and driveways are designed to facilitate safe and efficient movement of vehicles, bicycles, and pedestrians.

Finding: The proposed driveways on Rural Ave SE provide for safe turning movements into and out of the property. As specified in the conditions of approval, the proposed driveway approaches to Howard Street SE shall be modified to meet current PWDS and accommodate the required boundary street improvement along the eastern portion of Howard Street SE.

Staff have reviewed the relocation of the main parking area from Rural Street SE to Howard Street SE. The City Traffic Engineer determined that the relocation of the driveway creates a potential impact and change to the traffic pattern due to the relocation. The voluntary Traffic Impact Analysis supports this determination and recommends general traffic calming measures. To ensure the impacts are mitigated the following condition applies:

Condition 9: Prior to issuance of building permits, excluding temporary modular buildings, a performance guarantee pursuant to SRC 77.122 or SRC 110.100 in an

amount not to exceed \$15,000 for construction of traffic calming devices according to a traffic analysis performed within 1 year of occupancy of the proposed school additions. The performance guarantee shall be released a maximum of three (3) years after occupancy for the proposed use.

Criterion 4:

The proposed development will be adequately served with City water, sewer, stormwater facilities, and other utilities appropriate to the nature of the development.

Finding: The Public Works Department has reviewed the applicant's preliminary plan for this site. The water, sewer, and storm infrastructure are available within surrounding streets/areas and are adequate to serve the proposed development, with the exception of public sewer mains that bisect the subject property. The applicant shall provide easements for public utility mains within the subject property pursuant to PWDS.

The applicant's engineer submitted a statement demonstrating compliance with Stormwater PWDS Appendix 004-E(4)(b) and SRC Chapter 71. The preliminary stormwater design demonstrates the use of green stormwater infrastructure to the maximum extent feasible for all areas of new or replaced impervious surface, but requests an exception for the proposed temporary modular unit site on Rural Ave SE. This area is proposed to be a temporary pad for placement of modular units during construction. Due to the temporary nature of this impervious surface, staff supports alternative storm design standards under the condition the applicant provides a restoration plan and performance security pursuant to SRC 110.100. The performance security shall include an approved amount that ensures this area is amended to restore infiltration abilities of the soil and include contingencies for discovery after the modular units are removed.

The applicant shall design and construct all utilities (sewer, water, and storm drainage) according to the PWDS and to the satisfaction of the Public Works Director.

Condition 10: Design and construct a storm drainage system at the time of development in compliance with SRC Chapter 71 and PWDS.

Condition 11: Provide a restoration plan and performance security pursuant to SRC Chapter 110 for the temporary modular unit gravel pad proposed on Rural Street SE.

4. Analysis of Class 2 Adjustment Criteria

SRC Chapter 250.005(d)(2) provides that an applicant for a Class 2 Adjustment shall be granted if all of the following criteria are met:

Criterion 1:

The purpose underlying the specific development standard proposed for adjustment is:

- (i) Clearly inapplicable to the proposed development; or
- (ii) Equally or better met by the proposed development.

Finding: The applicant is requesting four Class 2 Adjustments to:

- 1) A reduction to minimum off-street parking requirements from 375 spaces to 267 spaces,
- 2) A reduction to minimum 20-foot setback abutting a street in order to construct an addition two feet from Howard Street and 10 feet abutting Church Street.
- 3) A reduction to the minimum distance from primary entrance for bicycle parking from 50-feet to 500-Feet.
- 4) A reduction to the requirement for interior landscaping for off-street parking areas.

Adjustment to reduce the minimum off-street parking requirement:

Basic Education (High School) uses are required to provide a minimum of one off-street parking space for every six students the school is designed to accommodate. The proposed development will increase the number of classrooms for the high school which will result in a maximum student capacity for the facility of 2,248 students. The student capacity is with removal of the modular buildings used during construction. A minimum of 375 off-street parking spaces are required for the proposed use ($2,248 / 6 = 374.6$). The applicant is requesting to reduce the minimum off-street parking requirement from 374 spaces to 267 spaces.

South Salem High School has historically used on-street parking to support the parking needs of visitors and students. The applicant providing a total of 267 spaces, which includes an additional parking area along Howard Street, which will provide an additional 97 parking spaces. The existing school is located near mass transit stops, is located within a residential neighborhood and has on-street parking around the school. Students attending the school is able to walk to the school from the surrounding neighborhood or from the transit stop, less than a half mile from the school.

In addition, the applicant has provided evidence that there are 73 parking spaces along Church Street and Rural Street, 25 parallel parking spaces along Rural Street and 87 additional spaces within 500-feet of the school. The total spaces on-site and off-site are more than the required 375 spaces. The school will be served by on-street and off-street parking spaces totaling 452 spaces, which equally or better meets the intent of the Code.

Adjustment to reduce the minimum setbacks abutting a street requirement:

The applicant is requesting a reduction to minimum 20-foot setback abutting a street in order to construct an addition two feet from Howard Street and 10 feet abutting Church Street. Without the adjustment, the applicant would have to construct the addition along the rear of the building, eliminating the exiting circulation pattern and additional parking. The applicant has provided written testimony that the need for the addition near Church Street and Howard Street is due to the operational aspects of the school. The portion of the school near Church and Howard Streets will serve students who will be dropped off along Howard Street. In addition, an addition in the rear of the building would eliminate

the current circulation pattern for the majority of the school buses which service the school.

The proposal equally or better meets the intent of the code.

Adjustment to reduce the minimum distance from primary entrance for bicycle parking from 50-feet to 500-Feet requirement:

The site plan indicates bicycle racks within 50-feet of three entrances of the school. Due to the design of the building on the site and the minimized setbacks to Howard Street, all 188 bicycle spaces required for the school cannot be accommodated at the primary entrance. As such, an adjustment is needed in order to allow these bicycle parking spaces to be located in several locations around campus. The parking spaces are all located near eight entrances of the school.

The underlying intent of the bicycle parking standards included under SRC Chapter 806 is to ensure that the bicycle parking spaces are provided in a sufficient number to serve the development and that they are conveniently located within proximity to the main entrance of a building.

Because the building has several entrances, the bike spaces near several entrances will be located within close proximity to the entrances and therefore equally meet the intent of the standard as if they were located within 50-feet. Some of the spaces are proposed further away from entrances. Typically, bike racks would be located near a primary entrance which is covered. Testimony was received that bike parking is used more frequently, year-round, when parking spaces are covered. Since the request is to located spaces further away from the primary entrance, which may provide additional covered parking spaces, the following condition applies:

Condition 12: At least 50 of the required bicycle parking spaces shall be covered with a roof or awning that provides protection from the rain.

The underlying purpose of this standard is to ensure that bicycle parking spaces are located in a convenient, visible, and easily accessible location on the site. As conditioned, the location of the bicycle spaces equally meets the purpose of this standard. This criterion is met.

Adjustment to reduce the requirement for interior landscaping for off-street parking areas:

The applicant is requesting to provide no interior parking lot landscaping, while SRC 806.035(d) would require interior landscaping of 5 percent of the parking lot. Without the adjustment, the applicant would be required to reduce the number of parking spaces. The intent and purpose of the interior parking lot landscaping is to provide trees and shade to break up large expanses of pavement. The existing building divides the parking lot into two smaller areas separated by the driveway. Although no interior landscaping would be provided, the widening of the perimeter landscape strip abutting the north property line and south property line abutting the new vehicle use area will be

planted with trees and require street trees to be planted to provide shade near the street. The proposal equally or better meets the intent of the code.

Criterion 2:

If located within a residential zone, the proposed development will not detract from the livability or appearance of the residential area.

Finding: The subject property is not located within a residential zone, therefore, this criterion is not applicable.

Criterion 3:

If more than one adjustment has been requested, the cumulative effect of all the adjustments result in a project which is still consistent with the overall purpose of the zone.

Finding: Four separate adjustments have been requested with this development. Each of the adjustments has been evaluated separately for conformance with the Adjustment approval criteria. The cumulative impact of the adjustments results in an overall project which is consistent with the intent and purpose of the zoning code.

Any future development, beyond what is shown in the proposed plans, shall conform to the off-street parking and bicycle parking requirements of the UDC, unless adjusted through a future land use action.

5. Analysis of Class 2 Driveway Approach Permit Criteria

SRC 804.025(d) states:

A Class 2 Driveway Approach Permit shall be granted if:

- 1) The proposed driveway approach meets the standards of this Chapter and the Public Works Design Standards;
- 2) No site conditions prevent placing the driveway approach in the required location;
- 3) The number of driveway approaches onto an arterial are minimized;
- 4) The proposed driveway approach, where possible:
 - i. Is shared with an adjacent property; or
 - ii. Takes access from the lowest classification of street abutting the property;
- 5) The proposed driveway approach meets vision clearance standards;
- 6) The proposed driveway approach does not create traffic hazards and provides for safe turning movements and access;
- 7) The proposed driveway approach does not result in significant adverse impacts to the vicinity;
- 8) The proposed driveway approach minimizes impact to the functionality of adjacent streets and intersections; and
- 9) The proposed driveway approach balances the adverse impacts to residentially zoned property and the functionality of adjacent streets.

Criterion 1:

The proposed driveway approach meets the standards of this Chapter and the Public Works Design Standards (PWDS).

Finding: The applicant proposes five driveways: Howard Street (East); Howard Street (West); two on Rural Avenue (Auxiliary Gym); and Rural Avenue (Fire Access); The proposed four driveway meets the standards in SRC 804 and Public Works Design Standards (PWDS) for two-way driveway approaches on to the respective street classifications. The proposed driveway located Winter Street and Howard Street will be reviewed for compliance with PWDS standards at development permit.

Condition 13: The proposed driveway at Winter Street SE and Howard Street SE shall be redesigned to meet PWDS.

Criterion 2:

No site conditions prevent placing the driveway approach in the required location.

Finding: There are no site conditions prohibiting the location of the five proposed driveways.

Criterion 3:

The number of driveway approaches onto an arterial are minimized.

Finding: The proposed five driveways are proposed to a local street or a collector street. No access to an arterial street is proposed.

Criterion 4:

The proposed driveway approach, where possible:

- a) Is shared with an adjacent property; or
- b) Takes access from the lowest classification of street abutting the property.

Finding: The driveways are proposed to be located with access to the lowest classification of street abutting the proposed development.

Criterion 5:

The proposed driveway approach meets vision clearance standards.

Finding: The proposed driveways meets the PWDS vision clearance standards set forth in SRC Chapter 805.

Criterion 6:

The proposed driveway approach does not create traffic hazards and provides for safe turning movements and access.

Finding: The proposed driveways will not create a known traffic hazard and will provide for safe turning movements for access to the subject property. No evidence has been submitted to indicate that the proposed driveway will create traffic hazards or unsafe turning movements if constructed pursuant to PWDS.

Criterion 7:

The proposed driveway approach does not result in significant adverse impacts to the vicinity.

Finding: The location of the proposed driveways does not appear to have any adverse impacts to the adjacent properties or streets. Staff analysis of the proposed driveway, and the evidence that has been submitted, indicate that the location of the proposed driveway will not have any adverse impacts to the adjacent properties or streets if constructed as part of the required half-street improvement on Howard Street SE.

Criterion 8:

The proposed driveway approach minimizes impact to the functionality of adjacent streets and intersections.

Finding: The proposed driveways do not appear to create a significant impact to adjacent streets and intersections and reduce the number of existing driveways in the vicinity. The eastern driveway access onto Howard Street SE is located on a Local street and does not create a significant impact to adjacent streets and intersections.

Criterion 9:

The proposed driveway approach balances the adverse impacts to residentially zoned property and the functionality of adjacent streets.

Finding: The proposed development is surrounded by residentially zoned property. The proposed driveways on Rural Avenue are replacing three existing one-way driveways and provide improved traffic circulation, balancing the adverse impacts to residentially zoned property and functionality of the adjacent streets. The proposed driveways on Howard Street are taken from the lowest classification street abutting the subject property. The driveway balances the adverse impacts to residentially zoned property and will not have an effect on the functionality of the adjacent streets if constructed as part of the required half-street improvement on Howard Street SE, as conditioned above.

6. Based upon review of SRC Chapters 220, 250 and 804, the applicable standards of the Salem Revised Code, the findings contained herein, and due consideration of comments received, the application complies with the requirements for an affirmative decision.

ORDER

Final approval of Class 3 Site Plan Review, Class 2 Adjustment and Class 2 Driveway Approach Permit Case No. 20-03 is hereby **APPROVED** subject to SRC Chapters 220, 250 and 804, the applicable standards of the Salem Revised Code, conformance with the approved site plan included as Attachment B, and the following conditions of approval:

- Condition 1:** Three Months after final occupancy is provided by the Building and Safety Division, all modular buildings shall be removed from the subject property or the applicant shall meet all development standards of the Salem Revised Code.
- Condition 2:** At building permit, the 188 bicycle parking spaces shall be located outside of setback areas and meeting the standards of SRC 806.060, except as approved below to be no further than 500 feet from the primary entrance.
- Condition 3:** Reconstruct the curb ramp at the intersection of Church Street SE and Howard Street SE, including AC pavement restoration as needed.
- Condition 4:** Convey land for dedication to equal a half-width right-of-way of 30 feet on the development side of Howard Street SE, including sufficient right-of-way to accommodate public infrastructure at the property corners.
- Condition 5:** Construct a 15-foot half-street improvement on Howard Street SE from Winter Street SE to Davidson Street SE to Local street standards as specified in the City Street Design Standards and consistent with the provisions of SRC Chapter 803.
- Condition 6:** Provide a public sidewalk easement along the development frontage of Howard Street SE and Church Street SE for portions of sidewalk that are not located within the right-of-way.
- Condition 7:** Construct sidewalk, as shown on site plan along the frontage of Howard Street SE and Rural Street SE. In addition, sidewalks not in conformance with current PWDS standards and ADA cross slope standards shall be replaced along the development frontage of Church Street SE.
- Condition 8:** The applicant shall plant shade trees abutting the frontage of Rural Avenue, on private property, every 40-feet to the greatest extent possible, as shown on the site plan. These trees are in addition to the on-site landscaping requirements of SRC 807.
- Condition 9:** Prior to issuance of building permits, excluding temporary modular buildings, a performance guarantee pursuant to SRC 77.122 or SRC 110.100 in an amount not to exceed \$15,000 for construction of traffic

calming devices according to a traffic analysis performed within 1 year of occupancy of the proposed school additions. The performance guarantee shall be released a maximum of three (3) years after occupancy for the proposed use.

Condition 10: Design and construct a storm drainage system at the time of development in compliance with SRC Chapter 71 and PWDS.

Condition 11: Provide a restoration plan and performance security pursuant to SRC Chapter 110 for the temporary modular unit gravel pad proposed on Rural Street SE.

Condition 12: At least 50 of the required bicycle parking spaces shall be covered with a roof or awning that provides protection from the rain.

Condition 13: The proposed driveway at Winter Street SE and Howard Street SE shall be redesigned to meet PWDS.

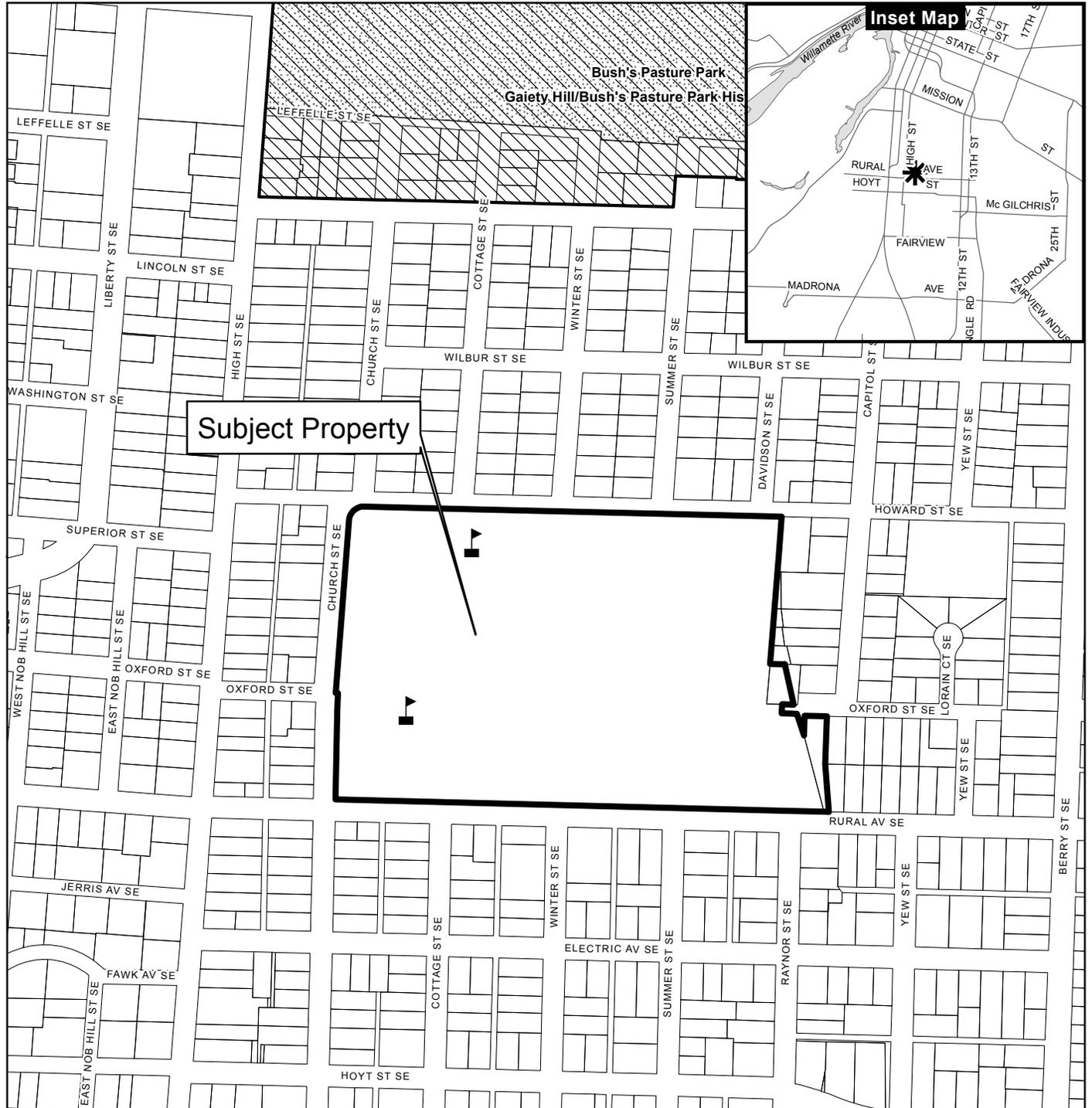


Olivia Dias, Planner III, on behalf of
Lisa Anderson-Ogilvie, AICP
Planning Administrator

- Attachments:
- A. Vicinity Map
 - B. Proposed Site Plan
 - C. Applicant's Written Statement
 - D. Applicant's Response to Comments
 - E. Public Works Memo

<http://www.cityofsalem.net/planning>

Vicinity Map 1910 Church Street SE



Subject Property

Legend

-  Taxlots
-  Urban Growth Boundary
-  City Limits
-  Outside Salem City Limits
-  Historic District
-  Schools
-  Parks



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LEGEND

- α FIRE HYDRANTS
- ▬ PROPERTY LINE
- + APPROX. ELEVATION
- SB ZONING SETBACK
- SSB SPECIAL SETBACK
- ⊕ STREET CENTER LINE
- C.B. CATCH BASIN
- FLOW OF BUS TRAFFIC
- ⌒ FENCE AND GATE
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- R.D. RAIN DRAIN, COORDINATE W/ CIVIL
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- NEW ASPHALT PAVING
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- (E)= TREE TO REMAIN
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- CRITICAL TREE ZONE
- LANDSCAPE BED/PLANTER

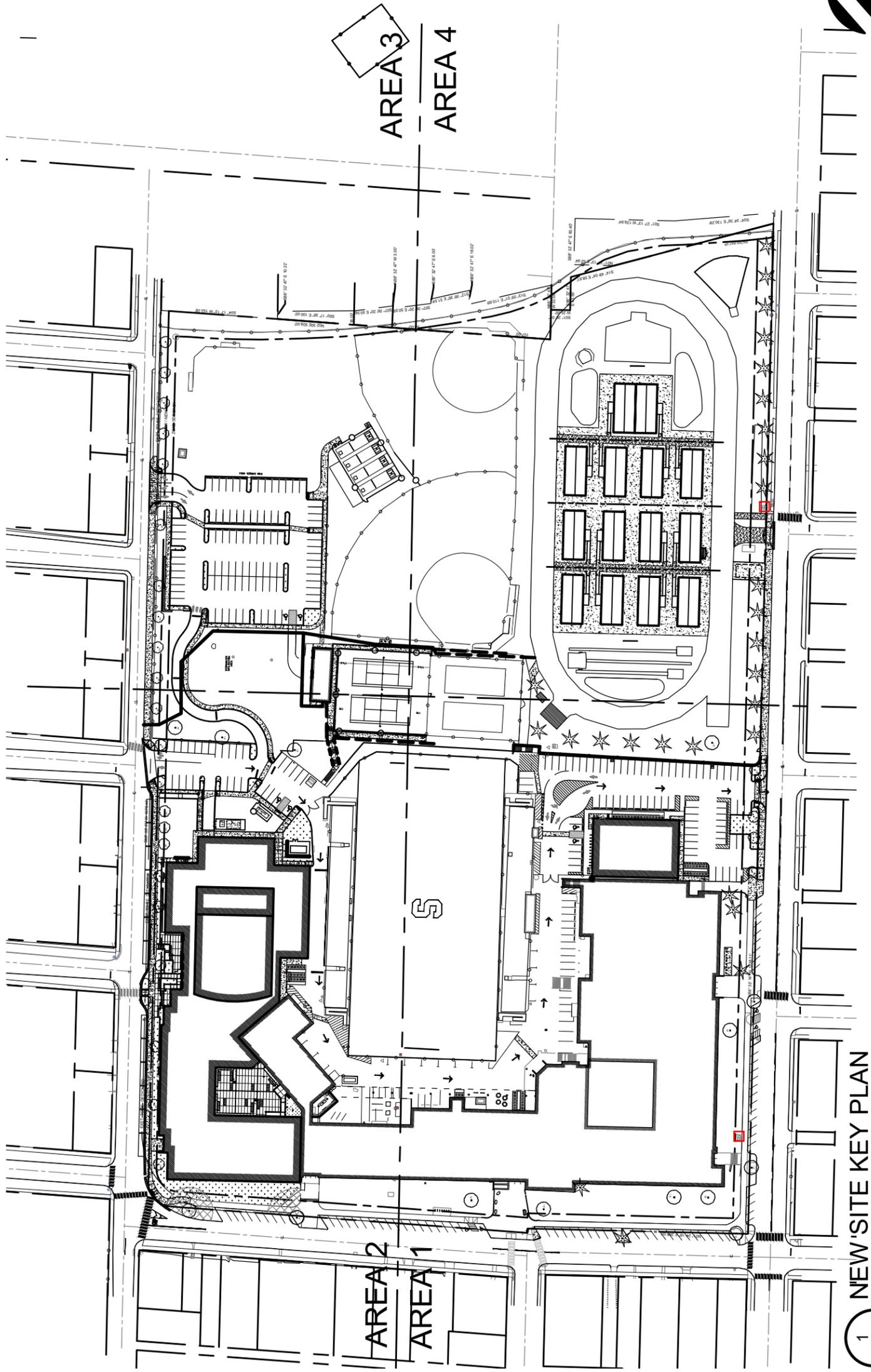
GENERAL INFORMATION

TOTAL SITE AREA: 22.09 ACRES (962,240 SF)
 NEW BUILDING FOOTPRINT: 158,260 SF
 NEW FIELD HOUSE FOOTPRINT: 1820 SF
 NEW AUX GYM FOOTPRINT: 8558 SF
 SPRT COURTS/ UTILITY PADS: 18,918 SF
 NEW PAVED PATHS: 22,048 SF
 PHASE 1 PAVED PATH: 6,310 SF
 PHASE 1 TEMP. PORTABLES: 25,088SF
 PROPOSED PARKING: 330 ON-SITE
 122 OFFSITE
 PROPOSED BIKE PARKING: 188
 EXIST. COVERED WALK: TO BOTTOM OF ROOF (E) 9'-11"



ISS: 2020 ADDITION AND RENOVATION
 PROJECT: SOUTH SALEM HIGH SCHOOL
 ADDRESS: 1910 CHURCH ST. SE SALEM, OR 97302

JOB: 1901
 DRAWN: AMJ
 DATE: 01-12-20
 TITLE: NEW SITE KEY PLAN
 SHEET: A0.01
 ISSUE: PERMIT SET



1
 A0.01
NEW SITE KEY PLAN
 SCALE: 1"=150'-0"

LEGEND

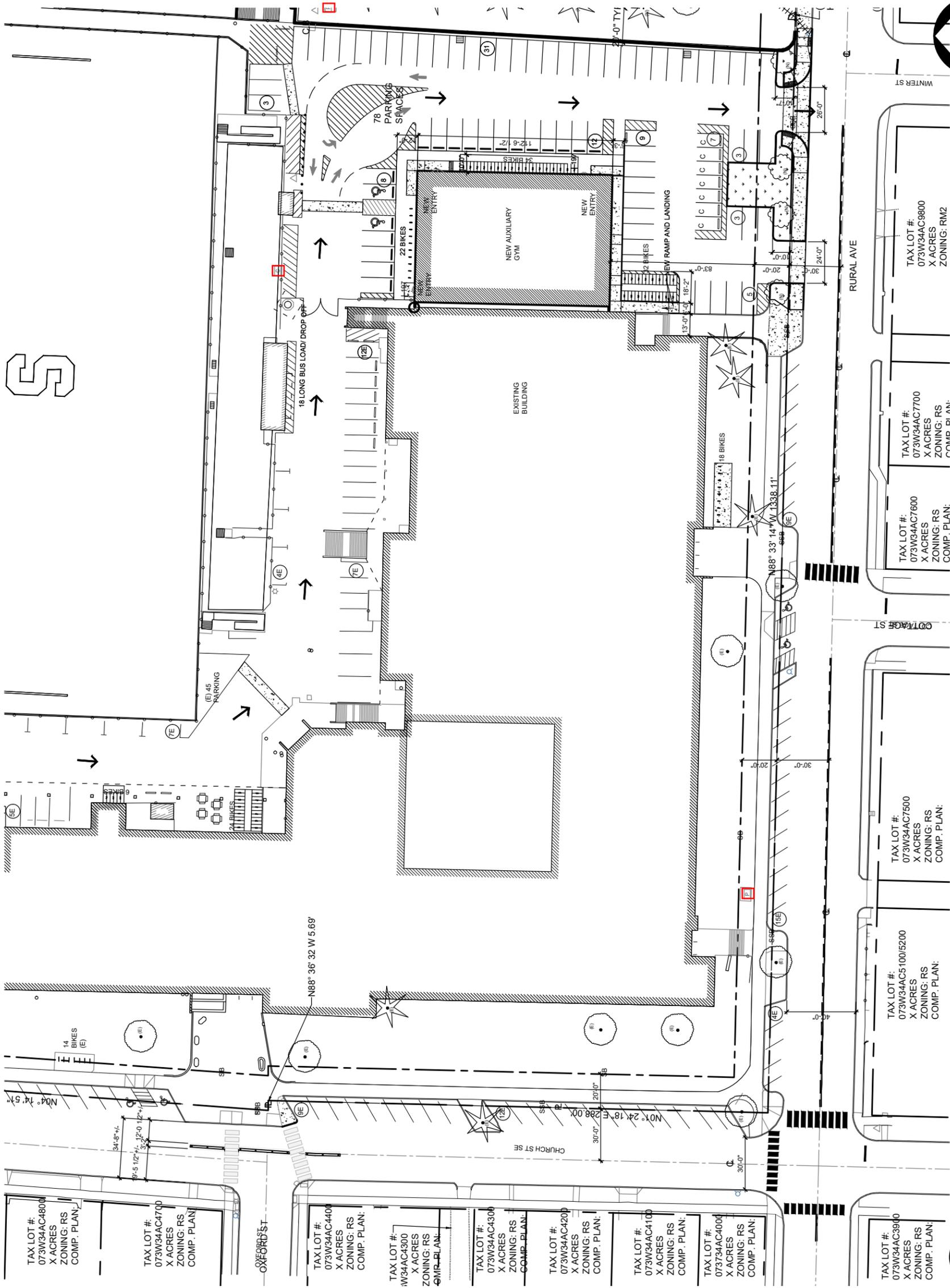
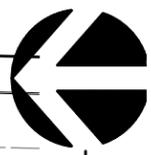
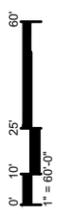
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NEW SITE PLAN
AREA 1
A0.02
 SHEET PERMIT SET



9

TAX LOT #: 073W34AC4800 X ACRES ZONING: RS COMP. PLAN:	TAX LOT #: 073W34AC4700 X ACRES ZONING: RS COMP. PLAN:	TAX LOT #: 073W34AC4400 X ACRES ZONING: RS COMP. PLAN:	TAX LOT #: W34AC4300 X ACRES ZONING: RS COMP. PLAN:	TAX LOT #: 073W34AC4300 X ACRES ZONING: RS COMP. PLAN:	TAX LOT #: 073W34AC4200 X ACRES ZONING: RS COMP. PLAN:	TAX LOT #: 073W34AC4100 X ACRES ZONING: RS COMP. PLAN:	TAX LOT #: 073734AC4000 X ACRES ZONING: RS COMP. PLAN:	TAX LOT #: 073W34AC3900 X ACRES ZONING: RS COMP. PLAN:
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1
A0.02
SCALE: 1"=60'-0"

AREA 1

LEGEND

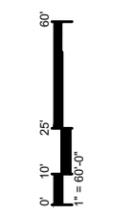
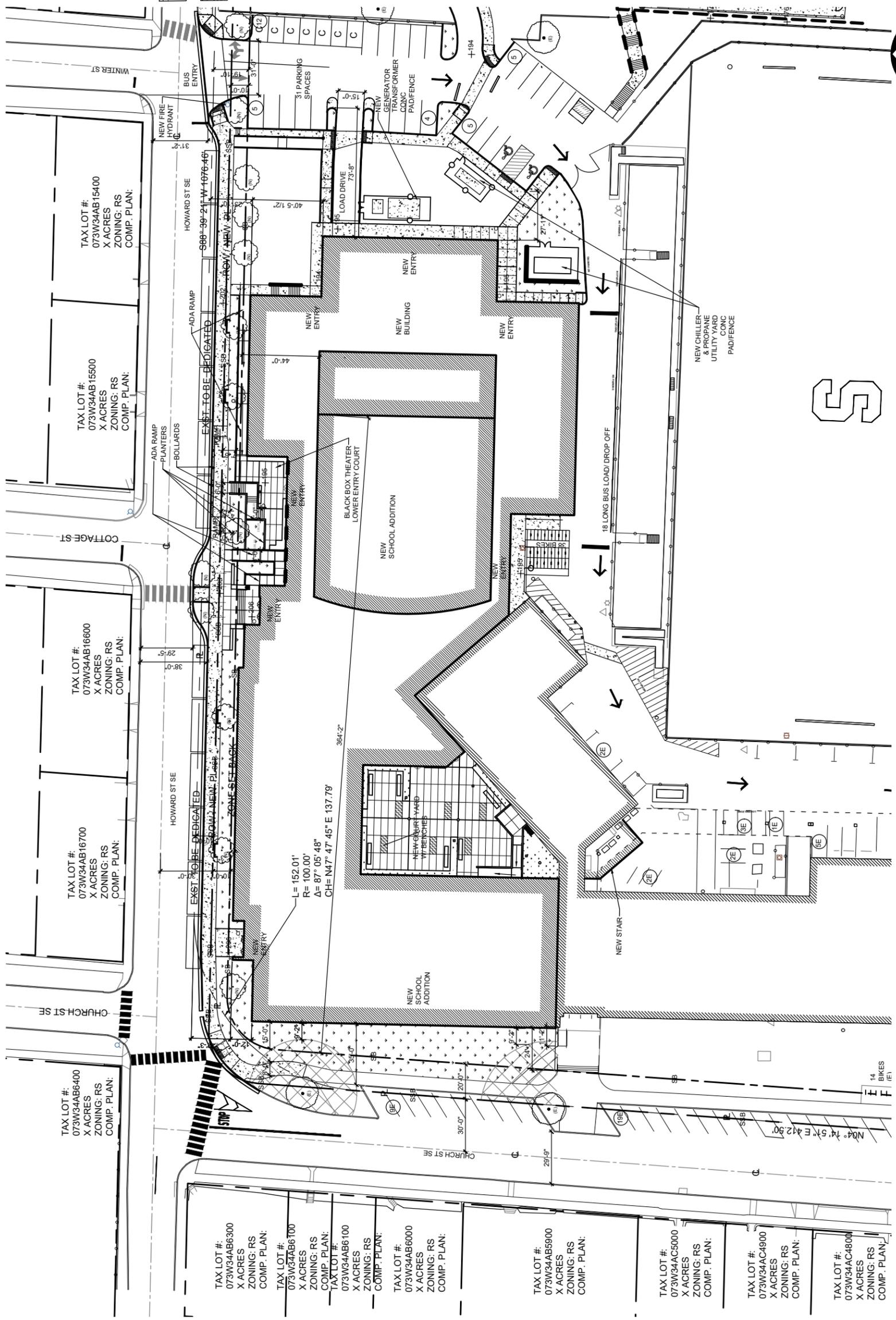
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- ▬ CROSS WALK
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 JOB 1901 DRAWN: AWS DATE: 01-12-20
 TITLE: NEW SITE PLAN AREA 2
 SHEET: A0.03
 ISSUE: PERMIT SET



1 AREA 2
 A0.03 SCALE: 1"=60'-0"

LEGEND

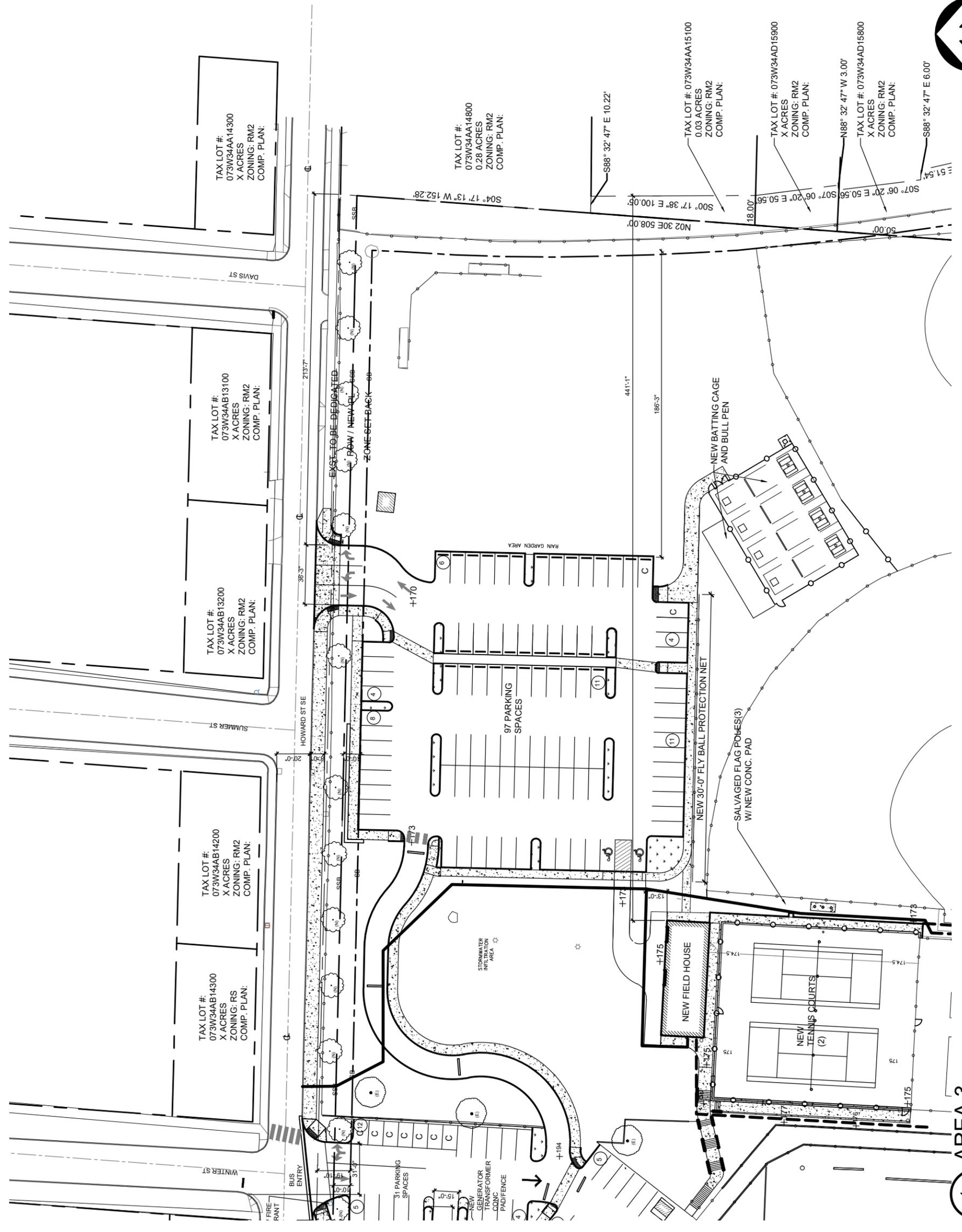
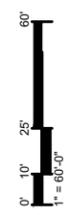
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 ADDRESS: 1910 CHURCH ST. SE SALEM, OR 97302
 JOB 1901 DRAWN: AMJ DATE: 01-12-20
 TITLE: NEW SITE PLAN AREA 3
 SHEET: A0.04
 ISSUE: PERMIT SET



1 AREA 3
 A0.04 SCALE: 1"=60'-0"

Site Plan Review – Class 3
 South Salem High School 2018 Bond Measure Project
 1910 Church Street SE, Salem OR, 97302

Tax Lot # 073W34AC00100

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Introduction

The 2018 Bond measure is intended to fund school renovations and expansions to address crowding/safety at various school campuses. The student population at South has increased to 2,248 students from 1,950 students. South Salem High School will be expanding career-technical education spaces, general education classrooms, science labs and special education classrooms. Demolition of the original 1926 building and 1952 auditorium and music classrooms (105,603 sf) will lead to construction of new performing arts teaching spaces, as well as general education, special education and career-technical education spaces (115,898 sf.) The library and gymnasium will be seismically upgraded triggering remodeling to the cafeterias. Physical education space will be increased by adding an independent auxiliary gymnasium. Additionally, security will be increased at the main entry. Demolition will begin the first quarter of 2020. Construction is planned to start as soon as the students are released for summer 2020. To accommodate the return of students the District will provide portables and repurpose existing spaces by placement of interim walls. These temporary spaces used to accommodate school functions during construction, will be on independent permitting tracks. Phase 1 of the portables, beginning in spring of 2020, includes altering the existing field in the center of the track to accommodate 14 portable classrooms with asphalt walkways. Phase 5, beginning in summer 2021

removes the portable classrooms and walks, restoring the field for use. Interior interim walls located within the current building will be permitted as tenant improvements. The interim walls will be removed to restore original space function when the main construction is complete.

Use: Basic Education (SRC Sec. 400.070.- Education services)

PE Zone Standards (SRC Chapter 542)

The subject property is currently zoned PE (Public and Private Educational Services). This project will be creating additional classroom space for an increased student body. While 105,603 square feet will be demolished 115,898 square feet will be added. Some existing building will be voluntarily seismically upgraded. Security upgrades will also trigger remodels within the building.

Sec. 542.010.a. Lot Standards (table 542-2)

PE lot standards are as follows:

	Required	073W34AC00100
Lot Area	10,000 sf min.	22.09 Acres (962,240 sf)
Lot Width	50' min.	795.0'
Lot Depth	80' min.	1324.40'
Street Frontage	16' min.	728'-0"

Sec. 542.010.b. Setbacks (table 542-3 and 542-4)

Abutting Street

Buildings and accessory structures, minimum 20 feet plus one foot for each one foot of height over thirty-five feet but need not exceed 50 feet. 52'-6" tallest point – 35' = 22'-6" x 1' = 22' + 20' = 42'-0" abutting street setback for theater fly.

Sec.800.040 Special Setback locates the origination point 10'-0" south of the property line.

Proposed

- Church St: At the closest point the proposed design is 15'-0" from the zone setback line. **Alternate Street Standard used to propose a 5'-0" reduction in zone setback in lieu of 20' for the Church St building frontage.** (Classroom bldg. ht.= 30'-4") Provided in SPLASH site package labeled, "84a-AltStStand-Written Statement-Church".
- Rural St: 83'-0" to new auxiliary gym (Aux. gym. bldg. ht.= 36'-0"); 64'-1" to portable (phase 1) (Portable bldg. ht.=15'-0")
- Howard St: 2'-3" to new structure. **Alternate Street Standard used to propose 10'-0" zone setback in lieu of 20' from the special setback.** Provided in SPLASH site package labeled, "84c-AltStStand-Written Statement-Howard".
 - 2'-3" from the zone setback line (Classroom bldg. ht.= 30'-4");
 - 44'-0" to theater fly wall (Theater fly bldg. ht.=52'-6")
 - 10'-0" dedication of property to City is required.

Vehicle use areas (per Ch. 806) See heading below "Off-Street Parking, Loading, & Driveways SRC Chapter 806"

Interior Rear

Zone-to-zone setbacks (table 542-4)

RS/RM2 on south property line

Residential Zone:

Building and accessory structure, minimum 20 feet; Applicable to buildings and accessory structures not more than 35 ft in height.

Building and accessory structure, minimum 30 feet; Applicable to buildings and accessory structures greater than 35 feet in height.

Proposed

- Field house: 441'-0" to south property line
- Portables (phase 1): 131'-2" to south property line

Vehicle use areas: min. 20 feet; Landscape screening Type A

Proposed

- East Howard St. Student parking lot: 186'-3" to south property line
Landscape screening will be Type A See heading below "Landscape screening SRC Chapter 807"

Sec. 86.015.-Administration; rulemaking; responsibilities(e) Planting street trees. Pursuant to SRC 803.035(k), any person undertaking development adjacent to public streets shall provide street trees to the maximum extent feasible in accordance with the standards and specifications set forth in this chapter and applicable administrative rules.

Proposed

- Church St: Existing mature trees to remain. In area of new construction, existing street trees will be protected. **Alternate Street Standard used to propose existing trees remain in lieu of planting new.** Provided in SPLASH site package labeled, "84a-AltStStand-Written Statement-Church".
- Rural St: 19 new street trees including 4 new deciduous trees at the auxiliary gym parking area and 15 coniferous along the south side of the track. 7 new coniferous trees along the existing retaining wall running north/south between the parking and the track. A total of 22 coniferous trees planted around the perimeter of the track. The main school project demolishes 11 trees. The applicant proposes to provide 22 trees within the outer perimeter of the track. The applicant would like to plant a coniferous species if allowed by the City. Due to the athletic use of the site the applicant prefers a coniferous species in lieu of deciduous to minimize the accumulation of leaves on multiple fields. This will also provide shade and screen the south face providing an enhanced natural view for neighbors. **Alternate Street Standard used to propose new trees planted in area of portables and auxiliary gym.** Provided in SPLASH site package labeled, "84b-AltStStand-Written Statement-Rural".
- Howard St: Twenty-four (24) new street trees. **Alternate Street Standard used to propose twenty-four (24) new street trees.** Provided in SPLASH site package labeled, "84c-AltStStand-Written Statement-Howard".

Sec. 542.010.c. Lot Coverage; Height (table 542-5)

Maximum lot coverage per Table 542-5 is 50 percent for buildings and accessory structures.

Property is 22.09 acres or 962,240 gross sf.

Proposed

Structures:

Final (Phase 1):	194,533 (20%)
Spring 2020	
(Breakout)	
School:	143,701 sf
Courtyd:	14,559 sf

Aux. Gym:	8,558 sf
Field House:	1,820 sf
Storage Sheds:	807 sf
Portables:	25,088 sf

Final (Phase 2): **169,445 (18%)**
 Summer 2021

(Breakout)
 School: 143,701 sf
 Courtyd: 14,559 sf
 Aux. Gym: 8,558 sf
 Field House: 1,820 sf
 Storage Sheds: 807 sf

Sec. 542.010.c. Lot Coverage; Height (table 542-5)

Maximum height for buildings and accessory structures per Table 542-5 is 70 feet.

Proposed

- Church St: Classroom wing = 30'-4"
Theater fly= 52'-6"
- Rural Ave: Auxiliary gym = 36'-0"
Portables = 15'-0" (phase 1/bid package 1)
- Howard St: Classroom wing = 30'-4"
Theater fly parapet = 52'-6"

Sec. 542.010.d. Landscaping

Setbacks shall conform to Ch. 807. See heading below "Landscaping and Screening (SRC Chapter 807)"

Vehicle use areas shall conform to Ch. 806 and 807. See heading below "Off-Street Parking, Loading, & Driveways (SRC Chapter 806) and Landscaping and Screening (SRC Chapter 807)"

Sec. 542.020.a. Other provisions; Trees and Shrubs (SRC chapter 86)

Sec. 86.050. Activities requiring permits

(a)(2) Permit required for construction within Critical Tree Zone

Proposed

A permit will be obtained to establish two critical tree zones and to perform construction adjacent to the zones. The critical tree zones will begin 9'-2" from proposed building to beginning of Critical Tree Zone established around two mature trees on north east Church St.

(6) Permit required to plant trees on City property.

Proposed

Permit will be obtained to plant trees in ROW on Rural Ave. at new street frontage for the auxiliary gym parking lot. Permit will be obtained to plant trees in setback on Rural Ave. to the south of the portables. Permit will be obtained to plant trees in setback on Howard St.

Wireless Communications Facilities (SRC chapter 703)

Existing cell tower to remain

General Development Standards (SRC chapter 800)

Sec.800.005 Applicability – *The standards set forth in this chapter apply to all development in every zone unless otherwise exempted by the UDC. In the event of a conflict between the standards set forth in this chapter and any other provision of the UDC, the more restrictive provision shall apply.*

Sec.800.035 Setbacks

Permitted projections into required setbacks per Table 800-2 indicates steps are not limited for front/side abutting streets or interior front streets. Ramps are not limited provided the floor area does not exceed 4 feet above grade and in no case shall the wheelchair ramp come closer than 10 feet to the property line.

Proposed

- Church St. steps and ramps are existing to remain. Existing steps to the north entrance are located approximately 20'-0" from the special setback line. The new building does not have an entrance on Church St. The special setback line and the property line are stacked.
- Rural Ave. All temporary portables and ramps are in excess of 62'-0" from the setback line. The special setback line and the property line are stacked.
- Howard St. theater and black box entrance has steps at 4'-11" from the special setback line. Main floor entrance ramp is 7'-0" from the special setback line. The north edge of the ramp for the lowest floor is 7'-6" from the special setback line. The special setback line and the property line are stacked.

Sec.800.040 Special Setbacks (b) *Setback distance required; how measured. The special setback shall equal one-half of the right-of-way width specified in the Salem Transportation System Plan for the street's applicable classification. Special setbacks shall be measured at right angles to the centerline of the street, or where there is no street, from the centerline of the right-of-way. Where the center line is not designated, the Director shall designate the location of the centerline.*

(c) Relationship to other required setbacks. The special setback shall apply in addition to other setbacks required under the UDC. Setbacks required elsewhere under the UDC shall be measured from the special setback line.

Salem Transportation System Plan Table 3-1; Local Street: Min. Right-of-Way= 60 feet (with 30' improvement) May be reduced in some instances per SRC.

Proposed

- Church St: Special setback line is measured 30'-0" from center line of Church St. Special setback is in the same location as the property line. The proposed design is 15'-0" from the zone setback line. (Classroom bldg. ht.= 30'-4") Provided in SPLASH site package labeled, "84a-AltStStand-Written Statement-Church".
- Rural St: Special setback line is measured 30'-0" from center line of Rural Ave. Special setback is in the same location as the property line. 83'-0" from proposed auxiliary gym to zone setback. (Aux. gym. bldg. ht.= 36'-0"); 64'-1" from portables to zone setback (phase 1/bid package 1.) (Portable bldg. ht.=15'-0")
- Howard St: Special setback line is measured 30'-0" from center line of Howard St. Special setback line is 10'-0" south of the property line. Proposed classroom building is 2'-3" from zone setback. 10'-0" dedication is required along this street. Provided in SPLASH site package labeled, "84-c-AltStStand-Written Statement-Howard".

Sec.800.055 Solid waste service areas- Trash and compactor are existing and not in scope. The height of the roof above is approximately 9'-11" to bottom/ 10'-10" to top.

Sec.800.060 Exterior lighting- *Exterior lighting shall not shine or reflect onto adjacent properties or cast glare onto the public right-of-way.*

Exterior light fixtures shall be located and designed so that the light source, when viewed at a height of five feet above the ground at a distance of five feet outside the boundary of the lot, shall be either: 1. Completely shielded from direct view or 2. No greater than five foot-candles in illumination.

Proposed

- General: Ramps and stairs will be lighted close to the walking surface. Wall mounted fixtures will be mounted near the building entry points and in soffits at entries. Photometric drawings will be submitted along with construction documents for building permits.
- East Howard St. student parking lot: 5 pole mounted fixtures
- Road between Howard St. parking: 2 pole mounted fixtures
- West Howard St. fine arts parking lot: 2 pole mounted fixtures
- Rural Ave. auxiliary gym parking lot: 2 pole mounted fixtures
- Rural Ave. temporary portables: Ramps and stairs will be lighted from wall packs mounted to the exterior walls of the portable buildings.

Sec.800.065 Pedestrian access-*The on-site pedestrian circulation system shall provide connectivity through the development site as follows:*

(1) Connection between building entrances and streets.

(2) Connection between buildings on the same development site.

Proposed

- Paved paths are included from the main school building to the field house, auxiliary gym, portables (phase 1/bid package 1), batting cages, tennis courts and new parking lots.
- Rural Ave. field/portables: Paved paths are included from the main school building to the portables. At Rural Ave. there will be a paved path from the sidewalk adjacent to the fire lane up to the track. On the north side of the track there will be a paved path from the south east corner of the existing tennis court to the track. Both these paths lead to the existing school building. (See drawings in file 60-Site-Plan-SSHS-REV01.)

(3) Connection through off-street parking areas.

Proposed

- East Howard St. student parking lot: 5'-0" wide, connected, striped, paved paths are on north, west and south of parking lot. There are five (5) locations where new pedestrian paths cross vehicle paths. Four (4) of the paths will be constructed of concrete to provide a distinct difference from the asphalt driving area. One (1) of the crossings will include a speed bump and painted striping.
- Existing internal site parking areas will be enhanced by the addition of four (4) concrete pedestrian paths through existing asphalt driving areas. Bollards at 5'-0" on center will be placed adjacent to drive aisles in two locations.

Public Improvements (SRC chapter 802)

Sec.802.025 Utilities to be placed underground- *Except as otherwise provided in this section, all utility service shall be provided by underground facilities. Stormwater management shall be provided by above ground and below ground facilities.*

Proposed

Power:

- New classroom/theater- New service in new classroom / theater addition. Service is underground.
- Auxiliary Gym- Fed from existing service
- Field House- Fed underground from new service in new classroom/ theater addition
- Portables (phase 1)- Fed from temporary service (likely from Rural Ave). The power company is putting together prices to supply power underground and overhead. There will be a new transformer set and possibly a new support pole with guide wire in the grass between the track and Rural Ave.
- Tennis Courts - fed underground from new service in new classroom/ theater addition
- Batting Cages- Fed underground from new service in new classroom/ theater addition
- Parking Lots- Fed underground from new service in new classroom/ theater addition

Proposed

Domestic water: An existing 4" meter at Rural street serves the existing school. All domestic water service for new and existing building areas will continue to be fed by this meter. The plumbing fixtures sum to a 1569 Total Water Fixture Units, which equates to an expected demand of 280 gal/min. There is a connection to irrigation near the existing meter, and a second connection to irrigation for the courtyard in Custodial-129.

- New classroom/theater- a new 3" water line will serve the new construction and connect to the existing service at the domestic water header located in the existing boiler room.
- Auxiliary Gym- from the existing gym to the west a 1-1/2" domestic water line will provide service
- Field House- from the new addition a 2" domestic water line will run underground, no meter
- Portables (phase 1)- will not be connected to the domestic water service. Landscape irrigation will be from existing meters located to the north west of the track.
- Replacement of athletic field (phase 2)- irrigation will be supplied from the meter on Howard St. via a 4" pvc pipe run on the east side of the field house, past the tennis courts, to the east of the new path to 4 new control boxes in the west end of the field.
- Tennis Courts- no water supply needed
- Batting Cages- no water supply needed
- Parking Lots- no water supply needed

Proposed

Sanitary sewer:

- New classroom/theater- The new addition has a total of 420 Drain Fixture Units. A single 6" sanitary line meets code requirements for the listed DFU's. A 4" line from the grease interceptor will join the 6" sanitary sewer line prior to the single connection point. This will connect to existing site waste lines and not require a new connection to the public utility. Sanitary sewer lines will all converge to a single connection on the south side.
- Auxiliary Gym- The Aux Gym has only 13.5 DFU's and requires a 3" minimum sanitary waste line. The design will have a 4" line as this is the preferred size for connection to public water closets. The 4" line will exit south toward Rural Ave for a new connection to the public utility.

- Field House- A new 4" waste line will be installed and connect to an existing sanitary sewer to the east of the football field.
- Portables (phase 1)- The applicant is in discussions with Public Works to develop a fee that will be refunded when the portables are removed.
- Tennis Courts- N/A
- Batting Cages- N/A
- Parking Lots- N/A

Proposed

Gas:

- New classroom/theater - will use the current service.
- Auxiliary Gym - will be supplied from existing gas line.
- Portables (phase 1)- N/A
- Tennis Courts - N/A
- Batting Cages - N/A
- Parking Lots - N/A

Streets and Right-of-Way Improvements (SRC chapter 803)

Sec.803.010 Streets, generally- *All streets shall be improved to include the following: adequate right-of-way, paving, curbing, sidewalks, stormwater facilities; utility easements, construction strips, landscape strips, adequate right-of-way geometry, paving width, grade, structural sections and monumentation, that conforms to the Public Works Design Standards.*

(paraphrased)

Proposed

- Improvements are proposed via Alternate Street Standards.
- Driveway cuts and parking lots at Rural St. and Howard St. will meet Public Works Design Standards for right-of-way geometry, paving, curbing, landscaping, grade, structural, etc.
- New sidewalks/ramps on Rural St. and Howard St. and within the property lines will meet Public Works Design Standards.

Sec.803.015 Traffic impact analysis- *Not required based on the expected net change in traffic as a result of the demolition of the 1926 building and its use.*

Proposed

The District has engaged DKS Associates to provide an "Expansion Transportation Evaluation" to address concerns related to student drop off. The report dated 09/10/2019 is included, reference file "76-Transportation-Evaluation-SSHS-REV01." The applicant is proposing an Alternate Street Standard to provide improvements to traffic control at the intersection of Church St and Oxford St. The improvement will consist of a raised concrete median running parallel with Church St. The median will contain flexible barricades from beginning to end. This barrier is intended to prohibit left hand turns from north bound traffic on Church St and for east bound traffic on Oxford St. Provided in SPLASH site package labeled, "84a-AltStStand-Written Statement-Church".

Sec.803.020 Public and private streets

Proposed

The connection between the east Howard St. parking lot (fine arts lot) and the west Howard St. parking lot (student lot) are connected by a private street that will conform to the Public Works

Design Standard. The applicant considers this a connector which links all the parking areas essentially creating one large parking area.

Sec. 803.025.a Right-of-way width (table 803-1) *Collector and Local street, min. 60-foot width without limitations & qualifications.*

Proposed

Existing to remain. The right-of-way and the special setback align with the property line on Church St. and Rural Ave. Howard St. property line is 10'-0" South of the special setback line.

Sec. 803.025.b Pavement width (table 803-2) *Collector, min. 34 feet, Type A and C; min. 40 feet, Type B; Local Street, min. 30 feet.*

Proposed

Existing to remain.

- Church St: Existing width is approximately 34'-8" from curb to curb at the Oxford St. intersection. Existing width in areas without angled parking is approximately 30'-0" wide. Refer to file "60-Site-Plan-SSHS".
- Rural St: Existing width including parallel parking, is approximately 40'-0" from curb to curb in areas without angled parking. Refer to file "60-Site-Plan-SSHS".
- Howard St (Church to Winter): Existing width with parallel parking on both sides, is approximately 38'-0" from curb to curb between Church and Winter. Alternate Street Standard used to propose 38'-0" with parallel parking on both sides between Church and Winter, with a traffic control node at the intersection of Winter and Howard to be 29'-0" wide curb to curb. A second traffic control node is located at the intersection of Winter and Howard narrowing the street to 31'-2". This will allow buses to turn into the parking area while preparing them to travel east on Howard which narrows further to 20'-0". Provided in SPLASH site package labeled, "84c-AltStStand-Written Statement-Howard".
- Howard St (Winter to Davidson): Existing width between Winter and Davidson, with no parking, is approximately 20'-0". Alternate Street Standard used to propose maintaining existing 20'-0" width between Winter and Davidson. Provided in SPLASH site package labeled, "84c-AltStStand-Written Statement-Howard".

Sec. 803.035.b Improvements. *All street improvements, including sub-base, base, pavement, curbs, sidewalks, and surface drainage shall conform to all provisions of the Salem Revised Code and the Public Works Design Standards.*

Proposed

Howard St: Half-street improvement will conform to Public Works Design Standards.

Sec. 803.035.k Street trees. *Development adjacent to public streets shall provide street trees that meet the standards and specifications set forth in SRC chapter 86.*

Proposed

- Church St: Nine (9) existing mature trees to remain. In area of new construction, existing street trees will be protected. Alternate Street Standard used to propose existing trees remain in lieu of planting new. Provided in SPLASH site package labeled, "84a-AltStStand-Written Statement-Church".
- Rural St: Six (6) existing mature trees are located between Church and the existing student parking lot. The applicant proposes to add four (4) deciduous trees at the auxiliary gym parking area. Fifteen (15) new street trees are proposed between the auxiliary gym

entrance and the east end of the property. Seven (7) new trees are proposed along the existing retaining wall between the parking and the track. A total of twenty-two (22) coniferous trees planted around the perimeter of the track. The main school project demolishes 11 trees. The applicant would like to plant a coniferous species if allowed by the City. Due to the athletic use of the site the applicant prefers a coniferous species in lieu of deciduous to minimize the accumulation of leaves on multiple fields. This will also provide shade and screen the south face providing an enhanced natural view for neighbors.

Alternate Street Standard used to propose new trees planted in area of portables and auxiliary gym. Provided in SPLASH site package labeled, "84a-AltStStand-Written Statement-Rural".

- Howard St: Twenty-four (24) new street trees. **Alternate Street Standard used to propose twenty-four (24) new street trees and one existing tree to remain.** Provided in SPLASH site package labeled, "84a-AltStStand-Written Statement-Howard".

Sec. 803.035.I Sidewalks

(1) Sidewalk construction required.

Proposed:

- Church St.: Existing 7'-6" sidewalk to remain from the northern Red Oak to the existing ramp leading to the existing north entry. The curb ramp from the corner of Church and Howard street will be replaced along with 8'-0" sidewalk leading south to the northern Red Oak.
- Rural Ave. aux gym parking lot: New 5'-0" wide, striped, paths run through the parking lot at the south side. New 8'-0" wide sidewalk and curb ramps will be added from the auxiliary gym parking area to the east side of the fire and pedestrian access to the track.
- Rural Ave. field/portables: Existing 5'-0" sidewalk parallel to Rural Ave will be demolished. In its place a new 8'-0" wide sidewalk and curb ramps will be added from the auxiliary gym parking area to the east side of the fire and pedestrian access to the track. Provided in SPLASH site package labeled, "84b-AltStStand-Written Statement-Rural".
 - New 10'-0" wide paved paths are included from the main school building to the portables. A new segment of 10'-0" wide walk will connect the existing walk at the south end of the tennis courts to the track. A new segment of 8'-0" walk will be added from Rural Ave. to the track.
- Howard St. (Church to Winter): New 8'-0" wide sidewalks with ADA ramps to street.
- Howard St. (Winter to Davidson): New 8'-0" wide sidewalk and curb ramps will be added from Winter St. parking area to the east side of the new parking area. Provided in SPLASH site package labeled, "84a-AltStStand-Written Statement-Howard".
 - New 5'-0" wide, connected, striped, paved paths are on north, west, south and within parking lot.

Sec. 803.035.p Landscape strips. *Landscape strips for signs, streetlights, and shade trees shall be provided that conform to the Public Works Design Standards.*

Proposed

All improvements will conform to Public Works Design Standards.

Sec. 803.040. - Boundary streets.

(a) General. Except as otherwise provided in this section, dedication of right-of-way for, and construction or improvement of, boundary streets of up to one-half of the right-of-way and

improvement width specified in SRC 803.025 shall be required as a condition of approval for the following:

(5) The construction or enlargement of any building or structure located on property abutting a boundary street and that requires a building permit under SRC chapter 56.

(e) Improvement.

(3)

If development is proposed for only a portion of a development site or complex, the boundary street improvement shall be provided as follows:

(B)

Where the area of development is equal to or less than 25 percent of the total development site or complex area, the street improvement shall be provided in accordance with the following formula:

(i)

Frontage of Required Street Improvement = Proposed Area of Development ÷ Area of Undeveloped Site x Total Street Frontage of Entire Development Site or Complex.

Proposed

Project on-site improvements encompass 16% of site.

Boundary street length is 3,285'-0"

.16x3285 = 525' Frontage of Required Street Improvement

- Church St: No driveway approaches; approximately 78'-0" of sidewalk
- Aux Gym Rural Ave: 177'-0" driveway approach, curbs and sidewalk
- Portables Rural Ave: 36'-0" fire access driveway approach, sidewalk approach and curbs; 280'-0" of sidewalk and curb
- Howard St between Church and Winter: 515'-0" half street improvement
- Howard St between Winter and Davis: 66'-0" driveway approach, sidewalk and curbs; 294'-0" of sidewalk and curb.

Sec. 803.035.q Landscaping. Property owners shall cover at least 75 percent of the unimproved surface area within the right-of-way abutting the property with perennial living plant material which conforms to all other requirements of the UDC, and which is kept free of noxious vegetation.

Proposed

75% of the unimproved surface area within the right-of-way abutting the property will be landscaped.

Driveway Approaches (SRC chapter 804)

Sec. 804.015 Driveway approach permit required- A driveway approach permit shall be obtained prior to constructing, relocating, reconstructing, enlarging, or altering any driveway approach.

Proposed

5 Driveway Approach Permits are required. 4 to be obtained for this site permit. 1 will be obtained for Phase 1/Bid Package 1 work for portables in the track field off Rural Ave. The Land Use Applications are in the SPLASH package labeled "59-DAP-'Location of driveway'-SSHS"

Sec. 804.025 Class 2 driveway approach permit-

Proposed

- Church St: No driveway approaches
- Rural Ave: Three (3) permits for two (2) new approaches and one (1) re-paved approach.
- Howard St: Two (2) permits for two (2) new approaches.

Sec. 804.050.b. Driveway approach width for uses other than single family or two family (table 804-2) Two-way driveway approach min. 22 feet, max. 40'-0"

Proposed

- Rural Ave. driveway approach: 24'-0"
- Rural Ave. driveway approach: 26'-0"
- Rural Ave. "driveway approach"/fire access (1 resurfaced): 20'-0"
- East Howard St. student parking, driveway approach (1 new): 36'-0"
- West Howard St. fine arts parking, driveway approach (1 new): 31'-0"
- All driveways will be constructed and marked per Public Works Design Standards.

Off-Street Parking, Loading, & Driveways (SRC Chapter 806)

Sec.806.010 Proximity of off-street parking to use or activity served- *Off-street parking shall be located on the same development site as the use it serves or in Nonresidential zones within 500 feet of the development site.*

Proposed

Within 500 of the property there are approximately 97 parking spaces.

Sec.806.015.a. Amount off-street parking (table 806-1) *The table requires a minimum of 1 parking space per 6 students for Education Services, Basic education. The number of students shall be calculated based on the total number of students the school is designed to accommodate.*

The school is designed to accommodate 2,248 students. Therefore, the required number of parking spaces is 375.

Proposed

On-street (directly adjacent and in the neighborhood)

	Within 500 of the property:	97 parking spaces
	East end of Rural Ave.:	25 parallel parking spaces
Off-street		
Full Size:	Existing (Church/Rural):	73 angled on property line
	Existing (Interior):	45
	Aux Gym:	71
	Fine Arts:	29
	Student parking:	93
	New spaces:	193
	Full size total:	311
Compact:	Existing:	0
	Aux Gym:	7
	Student parking:	2
	New spaces:	9
	Compact size total:	9 (3% of 330 spaces<75%)
Handicap:	Existing:	4

New:	6	
Handicap Total:		10
Grand Total off-street:		330
Off-street + on-street=		452

Adjustment request to provide 12% fewer parking spaces (45 fewer spaces). Provided in SPLASH site package labeled, "59a-Adjustment-Written Statement-Class1-Parking".

Sec.806.035. Off-street Parking, Vehicle Use, and Driveway Development Standards

Sec.806.035.b. Location. *Shall not be located within required setbacks.*

Perimeter setbacks and landscaping abutting streets- Method A, 10'-0", Type A landscaping, street trees may be included in landscaping.

Proposed

The new off-street parking areas are not located within any required setbacks.

- Church St: Existing angled parking is currently encroaching special setback by 9'-0". Alternate Street Standard used to propose existing 49 parking remain within special setback. Provided in SPLASH site package labeled, "84a-AltStStand-Written Statement-Church".
- Rural Ave. auxiliary gym parking: Existing parking begins 9'-10" to the north of the special setback line. Alternate Street Standard used to propose revised striping begin at 9'-10" north of the special setback line. Provided in SPLASH site package labeled, "84b-AltStStand-Written Statement-Rural".
- Howard St. fine arts parking: Existing parallel parking on south side of Howard St. is proposed for special education bus loading and drop-off. 60'-0" running east/west, parallel to Howard St., will be zero curb with bollards. Parking at fine arts area begins at zone setback line. Alternate Street Standard used to propose new parking begin at 10'-0" zone setback line. 10'-0" street dedication to City will be required. Provided in SPLASH site package labeled, "84c-AltStStand-Written Statement-Howard".
Type A landscaping see heading below "Landscaping and Screening (SRC Chapter 807)"
- Howard St. student parking: Parking begins at zone setback line. Alternate Street Standard used to propose new parking begin at 10'-0" zone setback line. Provided in SPLASH site package labeled, "84c-AltStStand-Written Statement-Howard".
Type A landscaping see heading below "Landscaping and Screening (SRC Chapter 807)"

Sec. 806.035.c.4. Setback adjacent to buildings and structures- *The off-street parking shall be setback from the exterior wall of the building by a minimum 5-foot-wide paved pedestrian walkway.*

Proposed

- Church St: Existing angled parking is 35'-0" at the closest point to the new building. The space between the building and the parking includes an existing sidewalk and landscaping.
- Rural Ave. auxiliary gym parking: Parking on the north side begins 12'-0" from the new building. Parking on the east side begins at 10'-0" from the new building. Parking on the south side begins at 6'-3" from the new building. On the north and east sides, the space is used for sidewalk and bicycle stalls. The space on the south is used as a ramp to the existing building.

- Howard St. performing arts parking: New parking is 27'-11" at the closest point to the new building. The space between the building and the parking includes sidewalks and landscaping.
- Howard St. student parking: New parking is 13'-0" at the closest point to the new Field House. Space between the building and parking includes sidewalks and landscaping. Type A landscaping see heading below "Landscaping and Screening (SRC Chapter 807)"

Sec. 806.035.d.2. Interior landscaping (table 806-5)- *Parking areas less than 50,000 square feet are required to be landscaped at a minimum of five percent.*

Proposed

- Rural Ave. auxiliary gym parking lot: 33,533sf of lot 750sf of infiltration
This lot is existing and will be altered by the addition of the new auxiliary gym. Paving will be removed to provide footings, foundations, slab, utility connection, pedestrian walkways and infiltration basin. Remaining asphalt paving will be restriped to accommodate new traffic patterns. Considering all the lots are tied together the landscape percentage is accounted for in the performing arts and student parking lots. Four (4) street trees will be planted adjacent to the north side of the new sidewalk along Rural Ave at the auxiliary gym parking area. Seven (7) new trees will be placed on the east side of the retaining wall to account for planters that will not be cut into the existing asphalt. **Alternate Street Standard used to propose no landscaping in the parking area. Trees will be planted to the east of the retaining wall.** Provided in SPLASH site package labeled, "84b-AltStStand-WrittenStatement-Rural".
- West Howard St. fine arts parking lot: 11,694sf of lot 1,147sf of landscape (10%)
- East Howard St. student parking lot: 35,534sf of lot 4,390sf of landscape (12%)

Sec. 806.035.d.3. Trees- *A minimum of one deciduous shade tree shall be planted for every 12 parking spaces within an off-street parking area. Trees may be clustered within landscape islands or planter bays and shall be distributed throughout the off-street parking area to create a canopy effect and to break up expanses of paving and long rows of parking spaces.*

Proposed

- Rural Ave. auxiliary gym parking lot: 78 spaces Four (4) trees in the infiltration area
Seven (7) new trees will be placed on the east side of the retaining wall to account for planters that will not be cut into the existing asphalt. The infiltration area will be landscaped and include one street tree. **Alternate Street Standard used to propose trees be planted to the east of the retaining wall.** Provided in SPLASH site package labeled, "84b-AltStStand-WrittenStatement-Rural".
(The main school project demolishes 11 trees. The applicant proposes to provide 22 trees within the outer perimeter of the track. The applicant would like to plant a coniferous species if allowed by the City. Due to the athletic use of the site the applicant prefers a coniferous species in lieu of deciduous to minimize the accumulation of leaves on multiple fields. This will also provide shade and screen the south face providing an enhanced natural view for neighbors. See 807.015.d)
- West Howard St. fine arts parking lot: 31 spaces 14 trees provided
- East Howard St. student parking lot: 95 spaces 30 trees provided

Sec.806.035.d.4. Landscape Islands and planter bays- *Landscape islands and planter bays shall have a minimum planting area of 25 square feet and shall have a minimum width of five feet per Figure 806-7.*

Proposed

All proposed landscape areas are a minimum five feet wide, and over 25 square feet.

Sec.806.035.e. Off-street parking area dimensions. (table 806-6) *Compact 8 feet by 15 feet with 22-foot aisle. Standard 9 feet by 19 feet with 24-foot aisle.*

Proposed

All spaces are sized per the requirements set forth

Sec.806.035.f. Grade- *Off-street parking and vehicle use areas shall not exceed 10 percent grade. Ramps shall not exceed 15 percent grade.*

Proposed

The maximum slope designed is 1:30.

- Rural Ave. auxiliary gym parking lot: 6%-7% max. slope
- West Howard St. fine arts parking lot: 6%-7% max. slope
- Drive/ramp to East Howard St. student parking: 11% max. slope
- East Howard St. student parking lot: 3%-4% max. slope

Sec.806.035.g. Surfacing- *Off-street parking and vehicle use areas shall be paved with a hard surface material meeting the Public Works Design Standards.*

Proposed

All new parking areas will be paved with asphalt. Five-foot type 'A' landscape strips, sidewalks and curbs will be provided in front of spaces.

Sec.806.035.h. Drainage- The new parking lot drainage has been designed and is shown on the attached utility plan.

Sec.806.035.i. Bumper guards or wheel barriers- Curbs around the perimeter of the new parking lot are proposed, with wheel stops.

Sec.806.035.j. Off-street parking area striping- The parking lots will be striped in accordance with Table 806-6.

Sec.806.035.k. Marking and signage

- (1) *Off-street parking and vehicle use area circulation.* Directional arrows will be painted to direct traffic through the new parking lot.
- (2) *Compact parking.* Compact parking spaces will be marked "Compact".

Sec.806.035.l. Lighting- *Off-street parking and vehicle use areas shall not shine or reflect onto adjacent residentially zoned property, or property used for uses or activities falling under household living or cast glare onto the street.*

Proposed

Lighting will be installed in the parking area. These will be shielded fixtures to not shine on adjacent sites. Photometric drawing will be submitted with building permit documents.

- East Howard St. student parking lot: 5 pole mounted fixtures

- Road between Howard St. parking and fine arts parking lot west: 2 pole mounted fixtures
- West Howard St. fine arts parking lot: 2 pole mounted fixtures
- Rural Ave. auxiliary gym parking lot: 2 pole mounted fixtures

Sec.806.035.m. Off-street parking area screening- *Off-street parking areas with more than six spaces shall be screened from abutting residentially zoned property, or property used for uses or activities falling under household living, by a minimum six-foot-tall sight-obscuring fence, wall, or hedge.*

Proposed

- East Howard St. student parking lot: hedge will be planted along the east edge of the parking and the stormwater infiltration area.

Sec.806.040 Driveway development standards for uses or activities other than single family or two family.

Sec.806.040.a. Access.

Proposed

- Rural Ave. auxiliary gym parking lot: 2 full-access driveways and turn-around striping at the north end.
- West Howard St. fine arts parking lot: 1 full-access driveway (There is also an internal, private street connecting the two lots on Howard St.)
- East Howard St. student parking lot: 1 full-access driveway

Sec.806.040.b. Location.

All new parking areas will directly connect to the new driveways and the existing streets.

Sec.806.040.c. Setbacks and Landscaping.

All new parking areas will directly connect to the new driveways and the existing streets.

Sec.806.040.d. Dimensions. (table 806-7) *Two-way driveway 22 feet width; 25 feet inside radius of curves and corners, measured at curb or pavement edge.*

Proposed

- Rural Ave. driveway approaches: 24'-0" and 26'-0"
- East Howard St. student parking, driveway approach: 36'-0"
- West Howard St. fine arts parking, driveway approach: 31'-0"
- All driveways will be constructed and marked per Public Works Design Standards.

Sec.806.040.e. Surfacing- *All driveways shall be paved with hard surface material.*

Proposed

All driveways will be asphalt with concrete curbs meeting Public Works Design Standards.

Sec.806.040.f. Drainage- *Driveways shall be adequately designed, graded, and drained according to the Public Works Design Standards, or to the approval of the Director.*

Proposed

All driveways will be designed, graded and drained to meet Public Works Design Standards.

Sec.806.040.g. “No Parking” signs- *One “No Parking” sign for every 60 feet of driveway length, but no less than 2 signs.*

Proposed

- East Howard St. student parking, driveway approach: 2 signs will be placed, one on each side, of the driveway.
- Private street between Howard St. lots: 4 signs will be placed along the street, 2 on each side.

Sec. 806.055 Amount of bicycle parking (table 806-8) *Basic education min. 2 per classroom. 94 classrooms x 2 bikes= 188 bikes*

Proposed

Church St. main entry:	14 bikes (existing)
Aux Gym:	88
S. Rural St. Entry:	18
S. Music:	38
S. Cafeteria:	24
N. Cafeteria:	6
Bicycle total:	188 bikes

Sec. 806.060. Bicycle parking development standards.

Sec. 806.060.a.1. Location- *Exterior bicycle parking shall be located within a convenient distance of and be clearly visible from the primary building entrance. In no event shall bicycle parking be located more than 50 feet from the primary building entrance as measured along a direct pedestrian access route.*

Proposed

- **Adjustment to distribute bike parking** among the primary entrances for the high school at 6 different locations. Provided in SPLASH site package labeled, “59d-Adjustment-Written Statement-Class2-Bicycles-REV01”.

Sec. 806.060.b. Access- *Bicycle parking areas shall have direct and accessible access to the public right of way and the primary building entrance that is free of obstructions and any barriers, such as curbs or stairs, which would require users to lift their bikes in order to access the bicycle parking area.*

Proposed

During school hours all the locations are free of obstructions, barriers, curbs or stairs.

Sec. 806.060.c. Dimensions- *6 feet long x 2 feet wide; 4 feet access aisle; side by side, ‘U’ shaped racks.*

Proposed

All bike parking spaces will be 2’-0”x6’-0” with 2’-6” between ‘U’ shaped racks mounted parallel to the long edge. The access aisles will all be 4’-0” wide.

Sec. 806.080. Loading Spaces

Sec. 806.080.c. Minimum off-street loading; dimensions (table 806-9) *Education services; >250,000 sf, 2 spaces plus 1 for each 100,000 sf or any portion thereof. Remaining existing building 221,807 sf; new 115,073 sf= 336,880 sf. 336,880 sf – 250,000 sf= 86,880 sf therefore 3 spaces are required.*

Sec. 806.080.c. Dimensions (table 806-9) *Off street loading dimensions 12 feet width x 30 feet length x 14 feet height.*

Proposed

Existing loading areas:	3	
New loading areas (fine arts addition):	1	15'-0" wide x 77'-6" long x Unlimited ht.
Total loading areas:	4	

Landscaping and Screening (SRC Chapter 807)

Sec.807.015.a. Landscaping and screening (table 807.1) *Landscaping type A; min. 1 required planting units (PU) per 20 sf of landscaped area; no required screening.*

Proposed

- Rural Ave. auxiliary gym parking lot: 750sf of infiltration = 37.5 PU
Alternate street standard request to not provide landscaping in this lot. All parking lots are considered part of one general parking area and therefore the landscaping for this lot is provided in the two lots off Howard St. Alternate Street Standard used to propose trees be planted to the east of the retaining wall. Provided in SPLASH site package labeled, "84b-AltStStand-WrittenStatement-Rural".
(The main school project demolishes 11 trees. The applicant proposes to provide 22 trees within the outer perimeter of the track. The applicant would like to plant a coniferous species if allowed by the City. Due to the athletic use of the site the applicant prefers a coniferous species in lieu of deciduous to minimize the accumulation of leaves on multiple fields. This will also provide shade and screen the south face providing an enhanced natural view for neighbors. See 807.015.c)
- West Howard St. fine arts parking lot: 1,147sf of landscape = 58 PU
- East Howard St. student parking lot: 4,390sf of landscape = 220 PU

Sec.807.015.b. Plant materials and minimum plant unit values (table 807-2)

The proposed development will meet the table however the selection has not been determined at this time. Plant material to be reviewed at time of building permitting.

Sec.807.015.c. Preservation of existing trees and vegetation- *The preservation of existing trees and vegetation is encouraged. If preserved, existing trees as defined under SRC chapter 808, existing trees less than ten inches DBH, and existing vegetation may be utilized to satisfy required landscaping if they conform to the minimum plant unit requirements specified in this chapter.*

(d)(1) *Removal of trees within required setbacks. When an existing tree or trees, as defined under SRC chapter 808, within a required setback are proposed for removal, two new trees shall be planted for each tree removed. Replanted trees shall be of either a shade or evergreen variety with a minimum 1.5-inch caliper.*

Proposed

- Howard St: Three (3) trees between the setback and the special setback will be removed. See file 61-Existing-Conditions-Plan-SSHS-REV01 page C4.01 for location of trees scheduled for demolition.
See file 65-Tree-Plan-Site-Survey-SSHS-REV01 page C001. Tree Tables indicate DBH and type. The 2 trees between the setback and special setback are labeled: 12382, CONIFEROUS, 35 DBH and 12383, DECIDUOUS, 22 DBH.

All other demolished trees are within the development site. This site has no “heritage” or “significant” trees as defined by SRC 808. No trees or native vegetation within a riparian corridor will be affected by the proposed development.

(d)(2) *Removal of trees from development site. When more than 75% of the existing trees, as defined under SRC chapter 808, on a development site are proposed for removal, two new trees shall be planted for each tree removed in excess of 75%. Replanted trees shall be of either a shade or evergreen variety with a minimum 1.5-inch caliper. For purposes of this section, existing trees within vision clearance areas, or within areas to be cleared for required roads, utilities, sidewalks, trails, or stormwater facilities, shall not be counted in the total percentage of trees removed from the development site.*

Proposed

Nine (9) trees are planned to be demolished within the development site but not within the setback. These trees are all between Church St. and Winter St. in the area of the new building. See file 61-Existing-Conditions-Plan-SSHS-REV-01 page C4.01 for location of trees scheduled for demolition.

See file 65-Tree-Plan-Site-Survey-SSHS-REV01 page C001. Tree Tables indicate DBH and type. The 9 trees are labeled:

- 10401, DECIDUOUS, 14 DBH
- 10556, CONIFEROUS, 14 DBH
- 13007, CONIFEROUS, 30 DBH
- 24184, CONIFEROUS, 15 DBH
- 24185, CONIFEROUS, 13 DBH
- 24186, CONIFEROUS, 14/16 DBH (This is one tree with a branch that is 14 DBH.)
- 24211, DECIDUOUS, 8 DBH
- 24732, DECIDUOUS, 16 DBH
- 24733, DECIDUOUS, 13 DBH

Proposed

There are 42 trees on the site. Twelve (12) will be demolished with the construction (28%.) Four (4) replacement trees will be planted to meet code requirements. This site has no “heritage” or “significant” trees as defined by SRC 808. No trees or native vegetation within a riparian corridor will be affected by the proposed development.

The applicant proposes to provide 22 trees within the outer perimeter of the track. The applicant would like to plant a coniferous species if allowed by the City. Due to the athletic use of the site the applicant prefers a coniferous species in lieu of deciduous to minimize the accumulation of leaves on multiple fields. This will also provide shade and screen the south face providing an enhanced natural view for neighbors.

Sec.807.015.e. Screening standards- *Height, landscaping shall be of a species that will attain a height of at least six feet within three years after planting. Screening opacity shall be site obscuring, at lease 75% opaque when viewed from an angle at a point 25 feet away. Landscaping shall be of an evergreen species that will attain required opacity within three years after planting.*

Proposed

- Howard St. student parking: Landscaping to east of parking will include screening to meet requirements.

Sec.807.015.g. Street trees- *Development adjacent to public streets shall provide street trees that meet the standards and specifications set forth in SRC Ch. 86.*

See heading above "Sec.86.015.-Administration; rulemaking; responsibilities(e) Planting street trees."

Sec.807.040. Irrigation.

Irrigation will be provided and presented when documents are submitted for building permit.

Natural Resources

Trees (SRC Chapter 808)

No significant trees, heritage trees or trees and native vegetation within a riparian corridor would be affected by the proposed development. Existing trees will be preserved to meet landscaping requirements wherever possible.

Wetlands (SRC Chapter 809)

According to the Salem-Keizer Local Wetland Inventory there are no mapped wetlands or waterways present in the area of the proposed development on the subject property. Hydric soils or hydric inclusions are present on the property. A State permit is required for 50 cubic yards or more of fill removal. Project appears to avoid impacts to jurisdictional wetlands or waters. This is supported by Department of State Lands, Wetland Land Use Notice Response dated 5/14/2019, WN#: WN2019-0216 based on City case file # 19-109682-IQ. (See file 76-Wetlands-Use-Notification-SSHS)

Landslide Hazard Susceptibility (SRC Chapter 810)

According to the City's adopted landslide hazard susceptibility maps, there are no areas of landslide susceptibility on the subject property.

Class 2 – Alternate Street Design – Howard Street

Written Statement for Zone setback, street width, sidewalks and street trees

Site Plan Review – Class 3

South Salem High School 2018 Bond Measure Project

1910 Church Street SE, Salem OR, 97302

Tax Lot # 073W34AC00100

Alternate Street Design (a. Church Street, b. Rural Avenue, c. Howard Street) Attachment Q: 84c

Seeking alternative street standards Sec.803.065 for regulations:

- Sec.542.010.b Setbacks (table 542-3 and 542-4) Abutting street min. 20'
- Sec.86.015/Sec. 803.035.k. Administration; rulemaking; responsibilities(e) Planting street trees/ Street trees
- Sec. 803.025.b Pavement width (table 803-2)
- Sec. 803.035.l Sidewalks
- Sec.806.035.b Off-street Parking, Vehicle Use, and Driveway Development Standards; Location

Use: Basic Education (SRC Sec. 400.070.- Education services)

PE Zone Standards (SRC Chapter 542)

The subject property is currently zoned PE (Public and Private Educational Services). This project will be creating additional classroom space for an increased student body. While 105,603 square feet will be demolished 115,898 square feet will be added. Some existing building components will be voluntarily seismically upgraded. Security upgrades will also trigger remodels within the building.

Sec. 542.010.b. Setbacks (table 542-3 and 542-4)

Abutting Street

Buildings and accessory structures, minimum 20 feet plus one foot for each one foot of height over thirty-five feet but need not exceed 50 feet. Theater fly is 52'-6" to tallest point – 35' = 22'-6" x 1' = 22' + 20' = 42'-0" abutting street setback for theater fly.

Proposed

10'-0" dedication of property to City is required along Howard St. from Church to Davis.

Howard St: 2'-3" between the zone setback line and the proposed structure (Classroom bldg. ht.= 30'-4");

44'-00" between the zone setback line and the theater fly wall (Theater fly bldg. ht.=52'-6")

Utilizing an alternate street standard to propose 10'-0" zone setback in lieu of 20'-0" from the special setback.

Benefits (supporting Sec.250.005(d)2)A)ii) and Sec.250.005(d)2)B))

- Special education buses are adjacent to the entrances leading to their classrooms.
- There is a 12'-0" landscape buffer prior to the sidewalk to help reduce distractions for the Special Education classrooms.
- Utilizing this alternate street standard for Howard St, provides compliance on Church St for the intersection zone setback.
- The site is maximized for interior teaching space.
- The building occupants will have full 8'-0" wide sidewalks.
- The intersection is safer by providing the node at Winter St bringing pedestrians to the edge of the street beyond parked vehicles prior to entering the street.
- There will be more traffic control due to narrow intersections and streets.
- The neighborhood will be enhanced by street trees on Howard. Yet the building is not negatively impacted by the proximity of the trees.
- The fire department will have good access to the building if necessary.

- The existing building sits 16'-4" within the current 20'-0" zone setback. By reducing the zone setback, the existing non-conformance is reduced to 6'-4".



Standing at intersection of Church Street and Howard Street looking east down Howard Street.

Sec.86.015.-Administration; rulemaking; responsibilities(e) Planting street trees. Pursuant to SRC 803.035(k), any person undertaking development adjacent to public streets shall provide street trees to the maximum extent feasible in accordance with the standards and specifications set forth in this chapter and applicable administrative rules.

AND

Sec. 803.035.k Street trees. Development adjacent to public streets shall provide street trees that meet the standards and specifications set forth in SRC chapter 86.

Proposed

Howard St: Twenty-four (24) new street trees.

The applicant proposes to use an alternate street standard to provide twenty-four (24) new street trees. Thirteen (13) of the trees are planned between Church and Winter. They have been placed to allow for fire department access and allow for proper building maintenance. Eleven (11) trees are planned to provide a canopy near Howard St edge. The trees will be planted within the zone setback.

Benefits (supporting Sec.250.005(d)2)A)ii) and Sec.250.005(d)2)B)

- The neighborhood will be enhanced by street trees on Howard. Yet the building is not negatively impacted by the proximity of the trees.
- The fire department will have good access to the building if necessary.



Standing on sidewalk at Winter Street looking west up Howard Street.

Sec. 803.025.b Pavement width (table 803-2) *Collector, min. 34 feet, Type A and C; min. 40 feet, Type B; Local Street, min. 30 feet.*

Proposed

Howard St (Church to Winter): Existing width with parallel parking on both sides, is approximately 38'-0" from curb to curb between Church and Winter. Alternate Street Standard used to propose 38'-0" with parallel parking on both sides between Church and Winter, with a traffic control node at the intersection of Cottage and Howard to be 29'-0" wide curb to curb. A second traffic control node is located at the intersection of Winter and Howard narrowing the street to 31'-2". This will allow buses to turn onto the driveway apron into the parking area while preparing them to travel east on Howard which narrows further to 20'-0".

Howard St (Winter to Davidson): Existing width between Winter and Davidson, with no parking, is approximately 20'-0". Alternate Street Standard used to propose maintaining existing 20'-0" width between Winter and Davidson.

Benefits (supporting Sec.250.005(d)2)A)ii) and Sec.250.005(d)2)B))

- Narrow streets are considered a traffic control mechanism. The applicant would like to maintain safety for the building occupants and the neighborhood.
- The street appears to function properly as currently constructed.
- The proposed driveway approach to the new eastern parking area adjacent to Howard Street has been designed with curb radius returns per City Detail 315. These returns allow vehicles entering and leaving the parking lot to do so without encroaching into other vehicle travel lanes. The 15' radius returns are preliminary and can be adjusted/enlarged as necessary to alleviate any City concerns about the safety of vehicles at this driveway connection. Please see attached graphic.
- Budget is better spent directly enhancing the learning experience.



Standing on sidewalk looking north across Howard Street; down Winter Street. Location for first node.



Standing at retaining wall looking north west toward the intersection of Howard Street and Winter Street. Location for second node.

Sec. 803.035.I Sidewalks

(1) Sidewalk construction required.

Proposed:

- West Howard St. (Church to Winter): New 8’-0” wide sidewalks with ADA ramps to street.
- Howard St. (Winter to Davidson): New 8’-0” wide sidewalk and curb ramps will be added from Winter St. parking area to the east side of the new parking area. Existing 5’-0” sidewalk will remain from east side of new parking area to property line.

New 5’-0” wide, connected, striped, paved paths are on north, west, south and within parking lot. Three (3) of the four (4) pedestrian crossing points are paved with concrete in lieu of asphalt. One (1) of the crossing points consists of a speed bump and striping.

Benefits (supporting Sec.250.005(d)2)B)

- New 8’-0” sidewalks meet current requirements.
- New ADA ramps will be added at all street crosswalk locations.

- Existing sidewalks to remain from east of new parking area to property line appear to be functioning properly.
- Budget is better spent directly enhancing the learning experience.



Standing at intersection of Winter Street and Howard Street looking east down Howard Street.

Sec.806.035. Off-street Parking, Vehicle Use, and Driveway Development Standards

Sec.806.035.b. Location. *Shall not be located within required setbacks.*

Perimeter setbacks and landscaping abutting streets- Method A, 10'-0", Type A landscaping, street trees may be included in landscaping.

Proposed

Howard St. performing arts parking: Existing parallel parking on south side of Howard St. is proposed for special education bus loading and drop-off. 60'-0" running east/west, parallel to Howard St., will be zero curb with bollards.

Fine arts parking begins at zone setback line. Alternate Street Standard is used to propose new parking begin at the new, 10'-0" zone setback line.

Type A landscaping see heading below "Landscaping and Screening (SRC Chapter 807)"

Howard St. student parking: Parking begins at zone setback line. Alternate Street Standard used to propose new parking begin at the 10'-0" zone setback line.

Type A landscaping see heading below "Landscaping and Screening (SRC Chapter 807)"

Benefits (supporting Sec.250.005(d)2)B)

- Curb side drop-off and pick-up for disabled occupants is safe and efficient due to proximity to building.
- The student parking spaces will be counted in the total off-street parking count.
- The spaces can be used for students, staff and visitors.
- The site is maximized for interior teaching space.

Summary

With sound reasoning the applicant seeks the Director's approval to authorize the use of alternative street standards citing Sec.803.065(a)2). "Where the development site is served by fully developed streets that meet the standards in effect at the time the streets were originally constructed." Howard Street between Winter and Davidson functions as it is currently built. It is an established street with beautiful residential character. As proposed the aesthetics of the existing street can be enhanced by the addition of street trees.

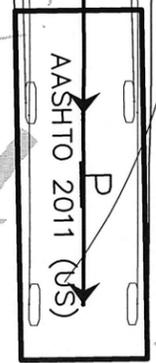
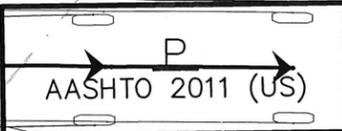
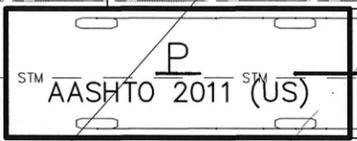
If adjustment criteria are applied to Alternate Street Standards the criteria can be met. There are three criteria for a Class 2 Adjustment each of which have been met by the proposal. These are identified by the parenthetic information listed at the end of each issue and summarized here.

The improvements to Howard Street from Church to Winter provide safety measures while maximizing the developable site area and maintaining a residential street scape. In turn the proposed development supports criteria established by Sec.250.005(d)2)A)ii). The proposal to maintain the existing street scape from Winter to Davidson supports Sec.250.005(d)2)B). Maintaining the current width from Winter to Davidson is a measure that will help provide steady paced traffic from the new parking area. Per the Pre-Application meeting held 10-21-19, the Planning Division indicates the property is not within a residential zone. However, the PE zone where the project is located is surrounded by residential zones. The applicant recognizes the importance of neighborhood and suggests the current street scape is essential to and will not detract from the livability or appearance of the surrounding residential area. Finally, the cumulative effect of the requested adjustments for parking, and landscaping, and all three (3) alternate street standard remains consistent with the overall purpose of the PE zone. Each adjustment and alternate allow for the design and construction of an up-to-date high school facility that will accommodate an expanded student enrollment, improved education facilities that will enhance the learning experience, and creates a wider range of educational opportunities for current and future generations of students. These reasons are offered in support of Sec.250.005(d)2)C).

The Salem-Keizer School District serves over 42,000 students across 2 counties through 65 schools. On May 15, 2018, Salem-Keizer voters approved a bond to fund school renovations and expansions to address crowding and safety across the district. In total this project serves the long-term educational needs of the community.

HOWARD ST SE

19+00

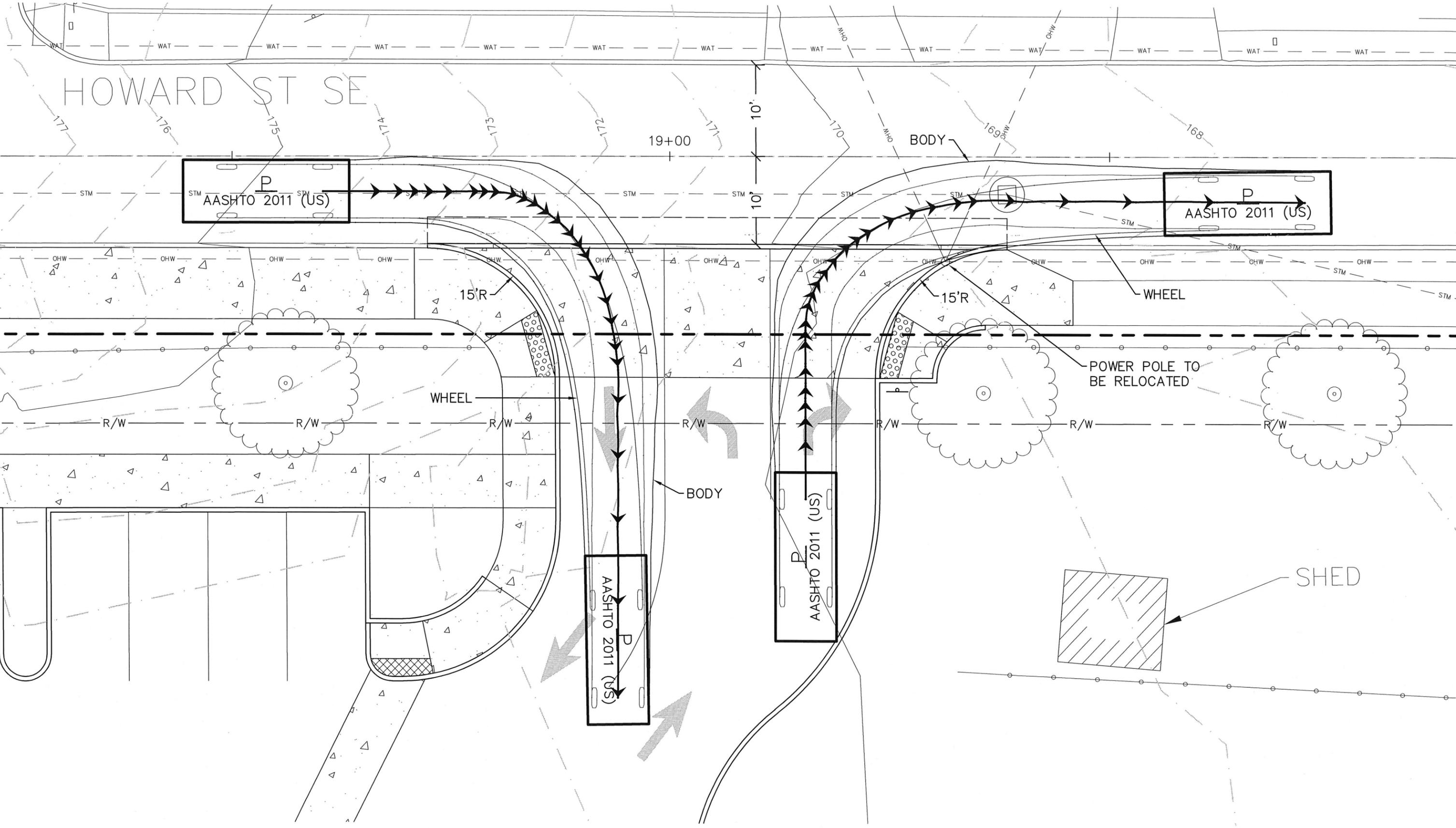


BODY

WHEEL

POWER POLE TO BE RELOCATED

SHED



Class 2 – Alternate Street Design – Rural Avenue
Written Statement for Parking within setback and street trees
Site Plan Review – Class 3
South Salem High School 2018 Bond Measure Project
1910 Church Street SE, Salem OR, 97302

Tax Lot # 073W34AC00100

Alternate Street Design (a. Church Street, b. Rural Avenue, c. Howard Street) Attachment Q: 84b

Seeking alternative street standards Sec.803.065 for regulations:

- Sec.86.015./803.035.k. Administration; rulemaking; responsibilities(e) Planting street trees/Street Trees
- Sec.803.035.l. Sidewalks
- Sec.806.035.b. Off-street Parking, Vehicle Use, and Driveway Development Standards; Location
- Sec. 806.035.d.2. Off-street Parking, Vehicle Use, and Driveway Development Standards; Interior landscaping (table 806-5)
- Sec. 806.035.d.3. Off-street Parking, Vehicle Use, and Driveway Development Standards; Trees

Use: Basic Education (SRC Sec. 400.070.- Education services)

PE Zone Standards (SRC Chapter 542)

The subject property is currently zoned PE (Public and Private Educational Services). This project will be creating additional classroom space for an increased student body. While 105,603 square feet will be demolished 115,898 square feet will be added. Some existing building components will be voluntarily seismically upgraded. Security upgrades will also trigger remodels within the building.

Sec.86.015.-Administration; rulemaking; responsibilities(e) Planting street trees. Pursuant to SRC 803.035(k), *any person undertaking development adjacent to public streets shall provide street trees to the maximum extent feasible in accordance with the standards and specifications set forth in this chapter and applicable administrative rules.*

Proposed

Rural St: There are two (2) trees in parking islands along Rural. One is a Pin Oak measuring 5 DBH. The second is a Red Oak measuring 25 DBH. The remaining four (4) existing trees are between the sidewalk and the building measuring between 13 and 28 DBH. These trees will remain and are not in the area of construction. Four (4) new deciduous street trees are proposed between the new driveway access points to the auxiliary gym. Fifteen (15) new coniferous street trees are proposed between the auxiliary gym entrance and the east end of the property. Seven (7) new coniferous trees are proposed along the existing retaining wall between the parking and the track. A total of twenty-two (22) coniferous trees planted around the perimeter of the track. The main school project demolishes 11 trees. The applicant would like to plant a coniferous species if allowed by the City. Due to the athletic use of the site the applicant prefers a coniferous species in lieu of deciduous to minimize the accumulation of leaves on multiple fields.

Benefits (support of Sec.250.005(d)2)B)

- The existing trees between the school and Rural Ave will continue to enhance the street experience.
- Adding trees at the entrance to the auxiliary gym parking area will help enhance the neighborhood.

- Adding trees at the auxiliary gym will help bring the frontage closer to current codes for landscaping.
- Adding trees along the track will provide shade for the local area.
- The coniferous trees will screen the south face of the track providing an enhanced natural view for neighbors.



Rural Avenue looking east



Rural Avenue looking west

Sec. 803.035.k Street trees. *Development adjacent to public streets shall provide street trees that meet the standards and specifications set forth in SRC chapter 86.*

Proposed

- Rural St: Six (6) existing mature trees are located between Church and the existing student parking lot. The applicant proposes to add four (4) deciduous trees at the auxiliary gym parking area. Fifteen (15) new street trees are proposed between the auxiliary gym entrance and the east end of the property. Seven (7) new trees are proposed along the existing retaining wall between the parking and the track. A total of twenty-two (22) coniferous trees planted around the perimeter of the track. The main school project demolishes 11 trees. The applicant would like to plant a coniferous species if allowed by the City. Due to the athletic use of the site the applicant prefers a coniferous species in lieu of deciduous to minimize the accumulation of leaves on multiple fields. This will also provide shade and screen the south face providing an enhanced natural view for neighbors. **Alternate Street Standard used to propose new trees planted in area of portables and auxiliary gym.** Provided in SPLASH site package labeled, "84a-AltStStand-Written Statement-Rural".

Benefits (support of Sec.250.005(d)2)B)

- The existing trees between the school and Rural Ave will continue to enhance the street experience.
- Adding trees at the entrance to the auxiliary gym parking area will help enhance the neighborhood.
- Adding trees at the auxiliary gym will help bring the frontage closer to current codes for landscaping.
- Adding trees along the track will provide shade for the local area.

- The coniferous trees will screen the south face of the track providing an enhanced natural view for neighbors.

Sec. 803.035.I Sidewalks

(1) Sidewalk construction required.

Proposed:

- Rural Ave. aux gym parking lot: New 5'-0" wide, striped, paths run through the parking lot at the south side. New 8'-0" wide sidewalk and curb ramps will be added from the auxiliary gym parking area to the east side of the fire and pedestrian access to the track.
- Rural Ave. field/portables: Existing 5'-0" sidewalk parallel to Rural Ave will be demolished. In its place a new 8'-0" wide sidewalk and curb ramps will be added from the auxiliary gym parking area to the east side of the fire and pedestrian access to the track. Provided in SPLASH site package labeled, "84b-AltStStand-Written Statement-Rural".
 - New 10'-0" wide paved paths are included from the main school building to the portables. A new segment of 10'-0" wide walk will connect the existing walk at the south end of the tennis courts to the track. A new segment of 8'-0" walk will be added from Rural Ave. to the track.

Benefits (supporting Sec.250.005(d)2)B)

- New ADA ramps will be added at all street crosswalk locations.
- New 8'-0" wide sidewalks will meet current requirements.
- Existing 5'-0" wide sidewalks east of the new pedestrian walk adjacent to the new fire access appear to be functioning properly.
- Budget is better spent directly enhancing the learning experience.

Sec.806.035. Off-street Parking, Vehicle Use, and Driveway Development Standards

Sec.806.035.b. Location. *Shall not be located within required setbacks.*

Perimeter setbacks and landscaping abutting streets- Method A, 10'-0", Type A landscaping, street trees may be included in landscaping.

Proposed

Rural Ave. auxiliary gym parking: Existing parking for students, staff and visitors begins 9'-10" to the north of the special setback line within the zone setback. Alternate proposal to maintain existing start of parking. To maximize on-site parking and manage budget, the existing asphalt will only be demolished in the area of the new building, for new sidewalks, new utilities to the street, and for an infiltration basin. The area open for water runoff will be landscaped and include a street tree. No other asphalt will be demolished in this parking area. Drive isles and parking will be restriped.

Benefit (supporting Sec.250.005(d)2)B)

- Existing non-compliant situation will be enhanced by the addition of ADA pedestrian access along Rural Ave and pedestrian connectivity within the parking area.
- Traffic will be controlled better using properly constructed driveway access points.
- The eastern side of this parking area is the exit location for buses and provides a path for fire engine access to the site.
- One street tree will be added to continue the pattern of trees marching down Rural Ave.
- Budget is best spent directly enhancing the learning experience.



Existing parking lot on Rural Avenue looking west

Sec. 806.035.d.2. Interior landscaping (table 806-5)- *Parking areas less than 50,000 square feet are required to be landscaped at a minimum of five percent.*

Proposed

- Rural Ave. auxiliary gym parking lot: 33,533sf of lot 750sf of infiltration
- Alternate street standard is proposed to not provide landscaping within the existing parking area. As an alternate the applicant would like to locate new trees adjacent to the east side of the retaining wall. This lot is existing and will be altered by the addition of the new auxiliary gym. Paving will be removed to provide footings, foundations, slab, utility connection, pedestrian walkways and infiltration basin. Remaining asphalt paving will be restriped to accommodate new traffic patterns. Considering all the lots are tied together, the landscape percentage is accounted for in the performing arts and student parking lots. Seven (7) new trees will be placed on the east side of the retaining wall to account for planters that will not be cut into the existing asphalt. The infiltration area will be landscaped and include one street tree. Landscaping in other parking areas exceed 5% for their area. The area used to plant seven (7) new coniferous trees is approximately 11,200 square feet (40'-0"x40'-0"=1,600 sf/tree.)
- West Howard St. performing arts parking lot: 11,694sf of lot 1,147sf of landscape (10%)
 - East Howard St. student parking lot: 35,534sf of lot 4,390sf of landscape (12%)

Benefit (supporting Sec.250.005(d)2)A)ii)

- An existing non-compliant situation will be enhanced by the addition of trees adjacent to the parking stalls planned on the east side of the retaining wall. This area plus the area of landscaping in the other two (2) lots is 17,487 sf. Which exceeds the total requirement for

landscaped area by 5,373 sf. (750+1147+4390+11200=17487) Requirement for all 3 lots is 12,114 sf.

- The existing retaining wall will not be burdened by the addition of trees.
- Irrigation can be accommodated easier on the east side of the retaining wall providing for a better long-term life expectancy for the trees.

Sec. 806.035.d.3. Trees- *A minimum of one deciduous shade tree shall be planted for every 12 parking spaces within an off-street parking area. Trees may be clustered within landscape islands or planter bays and shall be distributed throughout the off-street parking area to create a canopy effect and to break up expanses of paving and long rows of parking spaces.*

Proposed

Rural Ave. auxiliary gym parking lot: 78 spaces Four (4) trees in the infiltration area
Seven (7) new trees will be placed on the east side of the retaining wall to account for planters that will not be cut into the existing asphalt. The infiltration area will be landscaped and include one street tree. (The main school project demolishes 11 trees. The applicant proposes to provide 22 trees within the outer perimeter of the track. The applicant would like to plant a coniferous species if allowed by the City. Due to the athletic use of the site the applicant prefers a coniferous species in lieu of deciduous to minimize the accumulation of leaves on multiple fields. This will also provide shade and screen the south face providing an enhanced natural view for neighbors. See 807.015.d)

West Howard St. fine arts parking lot:	31 spaces	14 trees provided
East Howard St. student parking lot:	95 spaces	30 trees provided

Benefit (supporting Sec.250.005(d)2)A)ii) and Sec.250.005(d)2)B)

- The number of trees that can be planted on the east side of the retaining wall exceeds the amount that would be planted in the parking lot by one (1). There are seventy-eight (78) parking spaces proposed therefore seven (7) trees would be required. The proposal is for seven (7) new trees on the lower side of the retaining wall plus one tree in the infiltration basin.
- There are thirty-four (34) extra trees planted within the site.
- Adding trees at the entrance to the auxiliary gym parking area will help enhance the neighborhood.

Adding trees at the auxiliary gym will help bring the frontage closer to current codes for landscaping.

Summary

With sound reasoning the applicant seeks the Director’s approval to authorize the use of alternative street standards citing Sec.803.065(a)2). “Where the development site is served by fully developed streets that meet the standards in effect at the time the streets were originally constructed.” Rural Avenue functions as it is built. It is an established street with residential character which also supports the neighborhood as a parking resource. As proposed the safety and function of the existing street can be enhanced through the addition of newly constructed driveway access and street trees. If adjustment criteria are applied to Alternate Street Standards the criteria can be met.

There are three criteria for a Class 2 Adjustment each of which have been met by the proposal. These are identified by the parenthetic information listed at the end of each issue and summarized here.

The proposal to maintain the existing street scape with enhancements supports Sec.250.005(d)2)A)ii). The addition of driveway access control will be a positive direction regarding

safety. It will slow traffic and provide clear pedestrian access. Per the Pre-Application meeting held 10-21-19, the Planning Division indicates the property is not within a residential zone. However, the PE zone where the project is located is surrounded by residential zones. The applicant recognizes the importance of neighborhood and suggests the current street scape is essential to and will not detract from the livability or appearance of the surrounding residential area. These reasons are offered in support of Sec.250.005(d)2)B). Finally, the cumulative effect of the requested adjustments for parking, and landscaping, and this alternate street standard remains consistent with the overall purpose of the PE zone. Each adjustment and alternate allow for the design and construction of an up-to-date high school facility that will accommodate an expanded student enrollment, improved education facilities that will enhance the learning experience, and creates a wider range of educational opportunities for current and future generations of students. These reasons are offered in support of Sec.250.005(d)2)C).

The Salem-Keizer School District serves over 42,000 students across 2 counties through 65 schools. On May 15, 2018, Salem-Keizer voters approved a bond to fund school renovations and expansions to address crowding and safety across the district. In total this project serves the long-term educational needs of the community.

Class 2 – Alternate Street Design – Church Street
Written Statement for Setbacks on NW curve and Traffic Control
Site Plan Review – Class 3
South Salem High School 2018 Bond Measure Project
1910 Church Street SE, Salem OR, 97302

Tax Lot # 073W34AC00100

Alternate Street Design (a. Church Street, b. Rural Avenue, c. Howard Street) Attachment Q: 84a

Seeking alternative street standards Sec.803.065. for regulations:

- Sec.542.010.b. Setbacks (table 542-3 and 542-4) Abutting street min. 20'
- Sec.86.015./803.035.k. Administration; rulemaking; responsibilities(e) Planting street trees/Street Trees
- Sec.803.015 Traffic impact analysis
- Sec.806.035.b. Off-street Parking, Vehicle Use, and Driveway Development Standards; Location
- Sec. 806.035.d.3. Off-street Parking, Vehicle Use, and Driveway Development Standards; Trees

Use: Basic Education (SRC Sec. 400.070.- Education services)

PE Zone Standards (SRC Chapter 542)

The subject property is currently zoned PE (Public and Private Educational Services). This project will be creating additional classroom space for an increased student body. While 105,603 square feet will be demolished 115,898 square feet will be added. Some existing building components will be voluntarily seismically upgraded. Security upgrades will also trigger remodels within the building.

Sec. 542.010.b. Setbacks (table 542-3 and 542-4)

Abutting Street

Buildings and accessory structures, minimum 20 feet plus one foot for each one foot of height over thirty-five feet but need not exceed 50 feet. Theater fly is 52'-6" to tallest point – 35' = 22'-6" x 1' = 22' + 20' = 42'-0" abutting street setback for theater fly.

Proposed

Church St: Classroom bldg.- 15'-0" from closest point to 30' radius of zone setback line.
Bldg. ht= 30'-4" to top of new structure adjacent to Church St.
Theater fly parapet- 364'-2" from closest point to 30'-0" radius of zone setback line.
Bldg. ht= 52'-6" to top of theater fly parapet

Benefits (supporting Sec.250.005(d)2)A)ii)

Utilizing an alternate street standard for Howard St zone setback of 10'-0", the radius (30'-0") for the zone setback on Church St (20') provides for compliance with the regulation.

- The street and the building are not parallel therefore the proposed building sits 11'-6" from the curved portion of the zone setback at the closest point. It is 24'-1" from the zone setback at the furthest point.

Sec.86.015.-Administration; rulemaking; responsibilities(e) Planting street trees. Pursuant to SRC 803.035(k), *any person undertaking development adjacent to public streets shall provide street trees to the maximum extent feasible in accordance with the standards and specifications set forth in this chapter and applicable administrative rules.*

Proposed

Church St: Existing mature trees to remain. In area of new construction, existing street trees will be protected.

Benefits (support of Sec.250.005(d)2)B)

- There are two (2) mature Red Oak trees measuring 36 and 49 DBH. The drip line of these healthy, mature trees is touching the existing building. It would not be prudent to plant a new tree that might compete with the existing.
- The existing trees will be protected during construction.



Church Street looking north



Church Street looking south

Sec.803.015 Traffic impact analysis-

(b) Applicability. An applicant shall provide a traffic impact analysis if one of the following conditions exists:

(1) The development will generate 200 or more daily vehicle trips onto a local street or alley, or 1,000 daily vehicle trips onto a collector, minor arterial, major arterial, or parkway. Trips shall be calculated using the adopted Institute of Transportation Engineer's Trip Generation Manual. In developments involving a land division, the trips shall be calculated based on the proposed development that will occur on all lots that will be created by the land division.

(2) The increased traffic resulting from the development will contribute to documented traffic problems, based on current accident rates, traffic volumes or speeds, and identified locations where pedestrian and/or bicyclist safety is a concern.

(3) The City has performed or reviewed traffic engineering analyses that indicate approval of the development will result in levels of service of the street system that do not meet adopted level of service standards.

(c) Improvements may be required. On-site and off-site public or private improvements necessary to address the impacts identified in the traffic impact analysis may be required as conditions of development approval. Improvements include, but are not limited to, street and intersection improvements, sidewalks, bike lanes, traffic control signs and signals, parking regulation, access controls, driveway approach location and design, and street lighting.

Proposed

A traffic impact analysis is not required based on the expected net change in traffic as a result of the demolition of the 1926 building and the site's new use. In good faith the applicant has engaged DKS Associates to provide an "Expansion Transportation Evaluation" to address concerns related to student drop off. The report dated 09/10/2019 is included, reference file "76-Transportation-Evaluation-SSHS-REV01." Based on the recommendations of the evaluation the applicant is proposing to provide improvements to traffic control at the intersection of Church St and Oxford St.

Church St./Oxford St. intersection: The improvement will consist of a raised concrete median running parallel with Church St. The median will contain flexible barricades from beginning to end. This barrier is intended to prohibit left hand turns from north bound traffic on Church St and for east bound traffic on Oxford St. Please reference the attachment for a graphic representation of the proposal.

Benefits (supporting Sec.250.005(d)2)A)ii)

- Provide safety for pedestrians crossing Church St. at peak traffic times (morning drop off and afternoon pick up.)
- Control traffic to decrease congestion.



Church Street entrance looking west to Oxford Street



Oxford Street looking east

Sec. 803.035.I Sidewalks

(1) Sidewalk construction required.

Proposed:

- Church St.: Existing 7'-6" sidewalk to remain from the northern Red Oak to the existing ramp leading to the existing north entry. The curb ramp from the corner of Church and Howard street will be replaced along with 8'-0" wide sidewalk leading south to the northern Red Oak.

Benefits (supporting Sec.250.005(d)2)B)

- The new curb ramp will meet current cross slope requirements.
- New portion of sidewalk will increase width by 6" to meet current requirements.
- The remaining sidewalks south of the replaced appear to be functioning properly although they are between 4 and six inches too narrow for current code compliance. (If curb thickness is taken into consideration the current sidewalks are 8'-0" wide.)
- Budget is best spent directly enhancing the learning experience.

Sec.806.035. Off-street Parking, Vehicle Use, and Driveway Development Standards

Sec.806.035.b. Location. *Shall not be located within required setbacks.*

Perimeter setbacks and landscaping abutting streets- Method A, 10'-0", Type A landscaping, street trees may be included in landscaping.

Proposed

The new off-street parking areas are not located within any required setbacks.

Church St: Existing angled parking is currently encroaching special setback by 9'-0". The project proposes to utilize the existing 49 parking spaces within special setback.

Benefits (supporting Sec.250.005(d)2)B)

- The spaces are currently used for staff and visitors.
- Alteration of the existing spaces could decrease the quantity if islands are added. Mature trees in the vicinity support the landscaping requirements.
- They appear to be functioning properly.

- Budget is better spent directly enhancing the learning experience.

Sec. 806.035.d.3. Trees- *A minimum of one deciduous shade tree shall be planted for every 12 parking spaces within an off-street parking area. Trees may be clustered within landscape islands or planter bays and shall be distributed throughout the off-street parking area to create a canopy effect and to break up expanses of paving and long rows of parking spaces.*

Proposed

Church St: Existing parking is grouped starting at the north end of the street as: a mature Red Oak (36 DBH), 9 angled parking spots, a mature Red Oak (49 DBH), sidewalk, 19 angled parking spots, building entrance, 9 angled parking spots, a mature Sequoia (75 DBH), 12 angled parking spots, a mature Red Oak (43 DBH), a sidewalk, ends at Rural Ave. There are five (5) mature trees in the landscaped area between the building and the sidewalk at the head of the parking. The DBH of these trees ranges from 11-35.

Benefits (supporting Sec.250.005(d)2)B)

- Three of the four parking groupings meet the requirement.
- Alteration of the existing spaces could decrease the quantity if islands are added. Mature trees in the vicinity support the landscaping requirements.
- They appear to be functioning properly.
- They meet the intent of the code for providing shade and beauty.
- Budget is better spent directly enhancing the learning experience.

Summary

With sound reasoning the applicant seeks the Director’s approval to authorize the use of alternative street standards citing Sec.803.065(a)2). “Where the development site is served by fully developed streets that meet the standards in effect at the time the streets were originally constructed.” Church Street functions as it is built. It is an established street with beautiful residential character. As proposed the safety and function of the existing street can be enhanced. If adjustment criteria are applied to Alternate Street Standards the criteria can be met.

There are three criteria for a Class 2 Adjustment each of which have been met by the proposal. These are identified by the parenthetic information listed at the end of each issue and summarized here.

The proposal to maintain the existing street scape with enhancements supports Sec.250.005(d)2)A)ii. The addition of traffic control will be a positive direction regarding safety. It will slow traffic and provide clear pedestrian access. Per the Pre-Application meeting held 10-21-19, the Planning Division indicates the property is not within a residential zone. However, the PE zone where the project is located is surrounded by residential zones. The applicant recognizes the importance of neighborhood and suggests the current street scape is essential to and will not detract from the livability or appearance of the surrounding residential area. These reasons are offered in support of Sec.250.005(d)2)B). Finally, the cumulative effect of the requested adjustments for parking, and landscaping, and this alternate street standard remains consistent with the overall purpose of the PE zone. Each adjustment and alternate allow for the design and construction of an up-to-date high school facility that will accommodate an expanded student enrollment, improved education facilities that will enhance the learning experience, and creates a wider range of educational opportunities for current and future generations of students. These reasons are offered in support of Sec.250.005(d)2)C).

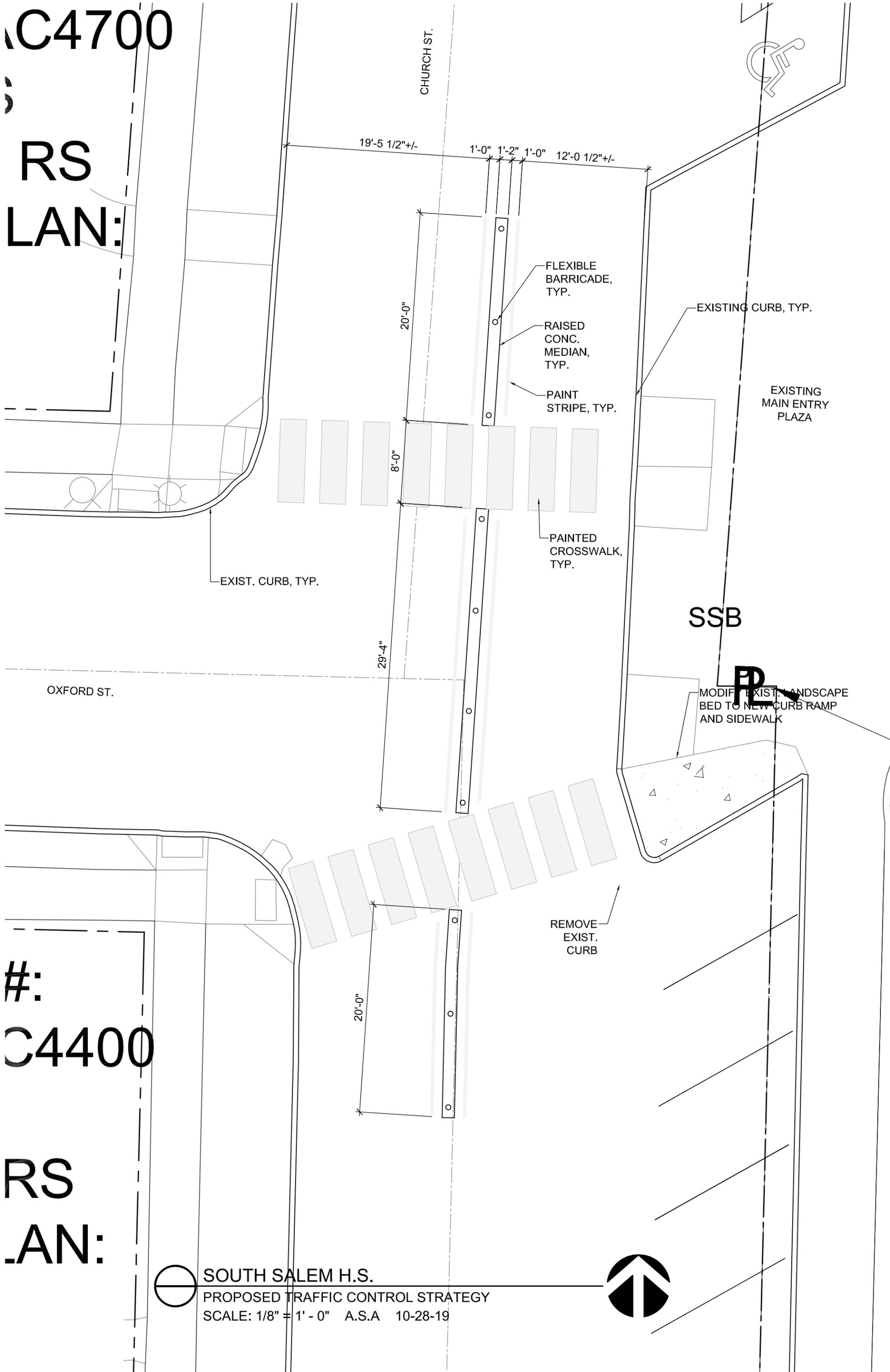
The Salem-Keizer School District serves over 42,000 students across 2 counties through 65 schools. On May 15, 2018, Salem-Keizer voters approved a bond to fund school renovations and expansions to address crowding and safety across the district. In total this project serves the long-term educational needs of the community.

C4700

RS
LAN:

#:
C4400

RS
LAN:



SOUTH SALEM H.S.
 PROPOSED TRAFFIC CONTROL STRATEGY
 SCALE: 1/8" = 1' - 0" A.S.A 10-28-19





ANDERSON SHIRLEY
ARCHITECTS

South Salem High School Additions and Renovations
1910 Church Street SE
Salem, OR 97302

Site Plan Review Public Response

Site Plan Review – Class 3 – Responses to Public Comments

SPR-ADJ-20-03

SPR-ADJ-DAP20-03

AMANDA Application No.: 19-123324-RP/ 19-121325-ZO/ 19-123268-ZO

06 February 2020

Case Manager: *Olivia Dias, Planner II*
City of Salem

Ms. Dias:

The following are responses to Public Input regarding Site Plan Review-Class 3-#SPR-ADJ20-03:

Date	Name/Title	Concern	Applicant Response
02-05-20	Victor Dodier/Chair, SCAN Transportation Committee	Sec 803.015 Traffic impact analysis: The school district asserts that there will not be a sufficient change in traffic on Church and Howard due to the demolition/construction to warrant a traffic impact analysis. Rather, the district cites the work done by DKS Associates as a good faith effort. SCAN members expressed their concerns about the effect of increased traffic on Howard Street and within the neighborhood to the north and east of South Salem High at the first (July 2019) meeting with school district staff.	DKS and Associates has been providing transportation advise and services to clients for over 40 years in California, Oregon, Washington, and Texas. The document provided to SKSD includes an evaluation of transportation circulation, safety needs, trip generation, existing intersection studies, projected capacity of the school, site distance evaluations, internal circulation and parking.

	<p>The SCAN Transportation Committee believes that traffic impact analysis of Church Street and Howard Street is needed. One, while the student population may not be increasing significantly due to the remodel/ new construction from current levels, its composition is changing. Middle school students that attended Howard Street Charter School did not drive. High School students have the potential to drive.</p> <p>Two, the location of student parking is changing. The new parking lot located off Howard Street is described in the application as parking for students, staff and visitors. The main lot located off Rural Street is reduced in size. More students, staff and visitors will be traveling Church Street and Howard Street as well as the narrow residential streets. (Cottage, Winter, Summer, Davidson, etc.) to the north of the school to reach and leave the new parking lot.</p> <p>Three, the proposal to install a median barrier at Church and Oxford Street was first mentioned to SCAN members by DKS at a lightly attended Dec. 18 SCAN Transportation meeting. The district's proposal may a sound one, improving both the safety and function of the intersection during prime AM and PM school drop off/ pick up hours. It has support within the SCAN Transportation Committee.</p>	<p>DKS and Associates has provided Existing Traffic Volumes and Operations for the following intersections: Howard Street/Church Street; Howard Street/Winter Street; Rural Avenue/Church Street; and Rural Avenue/Winter Street. The data collected begins on page 2 and is compiled on page 7 of the document labeled 76-Transportation-Evaluation-SSHS-REV02 dated 10/30/19. This analysis includes traffic counts for am and midday peak periods, two-hour traffic counts, and turns. While counts were recorded, they were less than the national trip generation rates. Therefore, the most conservative numbers (ITE 10th Edition) were utilized in the analysis. The study indicates the intersections currently operate with desired delays and capacity.</p> <p>Page 5 Parking Survey indicates the lot adjacent to Howard Street primarily served the staff for the middle school.</p> <p>The student parking is being more evenly distributed throughout the neighborhood. Nineteen parking spaces will be relocated from the Rural lot to the Howard Street lot. The project proposes to add 44 parking spaces within the site, which reduces the vehicles parked within the neighborhood streets. The Howard Street lot has a total capacity of 97 spaces. Page 8 summarizes that the intersections would meet desired operating standards for both peak hours. The current street widths are considered a benefit to maintaining slower speeds, better vehicle control and awareness.</p> <p>The project proposes to include 8 signs indicating "Bus Loading Only School Days 7AM-4PM" and "Student Drop Off/Pick Up Only School Days 7AM-4PM" on Howard St. between Church and Winter.</p>
	<p>Sec. 806.035.b Off-street Parking, etc. Alternative street standards are used to propose marking 60 feet of existing parallel parking on south side of Howard Street for special education loading and unloading. This proposal takes a significant amount of on-street parking out of use. If this use allowed, the times when the loading zone is in effect should be posted.</p>	

	<p>Alternative street standards are also cited to allow the minimal internal landscaping with the proposed new 97-space parking lot locate off Howard Street. There do not appear to be pedestrian walkways within the parking lot. In addition, the sidewalks connecting the parking lot and school buildings appear to be 5-foot walkways.</p> <p>We are concerned about this use of an alternative street standard. The parking lot should have internal landscaping and pedestrian walkways for safety. It is anticipated that internal landscaping and walkways will require a larger footprint in order to achieve the 97-space capacity but should not add significantly to the expense associated with the parking lot.</p> <p>In addition, the sidewalks connecting the parking lot to the buildings should be wider, perhaps 8 feet wide, to accommodate groups.</p>	<p>Per sheet A0.04 New Site Plan Area 3 shows pedestrian walkways around the north, west and south perimeter as well as crossing the drive lane between the fine arts lot and the lower Howard St. lot. On the west side of the entry drive, the pedestrian walk cuts through the parking lot to the eastern most parking island. The pedestrian path runs between the head end of parking stalls traveling south to cross the southern drive lane and connect with the southern sidewalk via an island.</p> <p>The proposed new sidewalk width adjacent to Howard St. is 8'-0". The remaining sidewalks at the perimeter of the lower Howard St. lot range between 6'-0" and 9'-0" depending on adjacent grading and soil purposes. The narrowest, at 6'-0" wide along the west parking stalls is sized to allow for percolation of water in the west rain garden. As students and visitors traverse up to the school the sidewalks become wider up to 8'-0". As people maneuver from vehicles through the site they will naturally gather and collect people becoming the largest as they approach their destination.</p>
	<p>SEC 803.035.1 Sidewalks. The school district invokes alternative street standards to retain the existing 5-foot wide sidewalk on Howard Street from Cottage to Davidson to its east property line and on Rural Avenue from the main parking lot east past the track to its east property line. There are two concerns. First, the 5-foot width may be too narrow to handle the amount of foot-traffic. This is especially true on Howard Street from the new parking lot to the building and on Rural Avenue from the main parking lot to the track field entrance at Rural and Summer. The Committee supports 8-foot wide sidewalks along the entire property line down Howard St. and Rural Ave. The second concern is that, if the existing 5-foot sidewalks remain, they have been in place for many years. They are cracked and are uneven in places. Either the district or the City (it is not clear which jurisdiction is responsible) should take advantage of the construction project and replace the sidewalks now. Replacement in conjunction with the construction project may</p>	<p>Per file labeled 84c-AltStStand-WrittenStatement-Howard-REV02 page 4, the sidewalks adjacent to the street will be replaced with 8'-0" wide walks from Church St. to the west side of the lower lot driveway access. Since group foot traffic wanes as pedestrians walk farther from the building, existing sidewalks will remain from east of the new drive access to Davis St.</p> <p>Per file labeled 84b-AltStStand-WrittenStatement-Rural-REV02 page 4, the sidewalk from the existing main school building to the west side of the fire access lane on the lower field, adjacent to Rural Ave., will be replaced with an 8'-0" wide walk. Since group foot traffic wanes as pedestrians walk farther from the building existing sidewalks will remain from east of the new</p>

		cost less than a stand-alone project as some point in the future.	pedestrian access to the eastern property line.
		New covered bike parking along Church St. is welcomed, but we wish to avoid large stands of racks in any one location. The SCAN Transportation Committee supports the detailed bicycle-parking plan submitted by Gary and Angela Obery.	The proposed bicycle parking layout has been revised to eliminate 68 of the bike stalls located under the existing canopy which have been determined to conflict with bus traffic. As recommended by public comment the 68 stalls will be relocated to: 24 bicycle parking spaces on the north end of Church St. directly to the west of the new construction; 24 bicycle parking spaces will be incorporated on Howard St. west of the driveway access to the fine arts parking lot; and 20 bicycle parking spaces will be incorporated on the west side of the fine arts parking lot to the south of the loading drive. All these locations are within 500'-0" of a building entry. After issuance of site plan approval, SKSD plans to request modifications to re-distribute bike parking spaces on the site and to cover many bike parking spots. The proposal includes relocating 8 bike parking spaces near the Howard St. lower parking, 4 bike parking near the batting cages and 8 bike parking near the track/field off Rural Ave. The proposal will also include 10 skateboard storage locations. Please reference the attached letter in support of active healthy transportation revisions from SKSD dated 02/05/2020.
		Crosswalks at intersections along Rural Avenue, Church Street and Howard Street should be marked on both or all sides of the intersections. Currently, some intersections (Rural and Summer or Winter and Howard, for example) are marked on only side; the crosswalks should be marked on both sides. In addition, the City should consider in-street pedestrian crossing signs.	The proposal includes pedestrian sidewalk ramp cuts, textured notification, cross street striping and new concrete at most intersections. The scope is limited to the applicant's side of the improvement.
02-05-20	Chris French/Sr.	Cherriots has reviewed the site plan and request that the sidewalk and driveway entrance be required to be brought	Sidewalks will be improved from the intersection of Church and Howard along the

	Planner Cherriots	to a state of good repair in order to improve pedestrian safety and access.	south side of Howard Street to Summer Street. Sidewalks will be improved on Rural Ave. from the east side of the existing school to Summer St. along the north side of Rural Ave.
02-05-20	Jon Christenson/ Friends of Historic Salem/ Howard Hall	Please encourage or recommend the applicant include an interpretative historical plaque in its development program at this location. To have an on-going dialogue with City Historic Preservation Officer/Archaeologist and individuals in the community to standards and content.	The State Historic Preservation Office and SKSD are in negotiations regarding a Memorandum of Agreement to record the significance of the historic above ground elements. Salem Historic Landmarks Commission will receive a copy of the agreement when it is available.
		What we have seen, observed, is a thoughtlessness to watering and access to water for specific types of plantings on school properties. As I mentioned, Clark Creek on the east side of the SSHS campus, Howard Street to Rural- has not been treated well. In the 1970's, the bed- riparian habitat, became concrete, the walls are concrete, the water temperature – flowing into Pringle Creek (Willamette River is too high, water quality is low. So, an ethic – standard of quality landscaping nearby is important to progress. As we move forward.	The area mentioned is outside the scope of the proposed project.
		One clear deficiency is an absence of detailed TIA or Traffic Impact Analysis and analysis in change in circulation patterns, vehicular infiltration into nearby residential streets. Also, potential need for safety of another STOP sign on Howard, bottom of the hill.	DKS Associates has been retained to provide a transportation evaluation. The document was provided as support to the proposal. The file name is 76-Transportation-Evaluation-SSHS-REV02 dated 10/30/19. This document addresses multi-way stop intersections via evaluation based on the Manual on Uniform Traffic Control Devices Multi-Way Stop and City of Salem all-way stop guidelines. (reference page 11)
		Proposed reduction of minimum off-street parking requirements from 375 spaces to 253 spaces.	With the addition of 77 angled parking spots around the perimeter of the school which are partially on the school property, there are 326 spaces, 44 more that currently on site. According to the traffic study the current spaces were occupied on average, 61% of the time.
		Proposed reduction to the minimum distance from primary entrance for bicycle parking from 50-feet to 500-feet.	The proposed design will be altered to accommodate the intent of suggestions

		<p>Comment: Bike utilization studies, one most recent by Cornell University, find distance and location are critical factors. As proposed, this is not a well-designed element.</p> <p>Proposed reduction to the requirement for interior landscaping for off-street parking areas.</p> <p>Comment: City landscaping standards should be upheld. Plantings should consider change in climate, increasing drought in the summer, and access to water.</p> <p>Landscaping requirements should not be reduced for parking areas near Howard Street. This area- east side – has a historical record of poor maintenance, destruction of the natural waterway, e.g. Clark Creek, an absence of tree canopy, inadequate drainage and water temperatures. One of the worst, identified as such, in the Pringle Creek watershed.</p>	<p>made by public comment. See specifics noted previously.</p> <p>Howard Street parking lot will meet the requirements for landscaping. The Rural Ave. parking lot will defer the landscaping components to adjacent areas of the site due to construction scope within the existing parking lot. This area is only being disturbed in the area of the new construction. The remaining asphalt will remain with new striping for altered parking and drive lanes. Trees will be planted along the east retaining wall perpendicular to Rural Ave and parallel to Rural Ave at the lower field. There will be landscaping incorporated in the auxiliary gym parking lot where existing asphalt is being removed to accommodate building infrastructure.</p>
02/10/4/20	<p>Angela and Gary Obery/SSHS Neighbors and Parents/Leaders of Salem Bike Boulevard Advocates</p>	<p>We agree to the acceptance of the reduction to the minimum distance, and propose an expanded reduction with conditions that increase the number and location of covered parking, as well as providing bike parking access to athletic facilities and locking parking racks for students traveling by skateboard.</p>	<p>Please reference the attached letter from Joel Smallwood, Director of Maintenance and Construction Services regarding bicycle parking at South Salem High School. The proposal will be adjusted to remove the 68 bike parking stalls perceived to conflict with bus traffic. In turn the revision will increase access to bike stalls throughout the campus and neighborhood by adding 24 bike parking spots on Church St.; 24 bike parking stalls on Howard St.; and 20 bike parking stalls off the fine arts parking to the west. See attached site plan.</p>
		<p>Quality over Quantity: School staff note that approximately 20 bikes are currently parked at the school each day. With a growing enrollment, encouragement, and improved parking and access this number could increase. However, it is not likely that the school will need the proposed 188 biking spots within the next 15 years. We</p>	<p>In order to follow City protocol, the current proposal will be altered by moving stalls from the interior of the site to locations on Church and Howard Street as noted above. Via modification to the approved, conditioned site plan, the district plans to request modifications to relocate some bike parking spaces and to provide cover for many bike parking spots. Bike stalls will be added near the field house, batting cages and the track.</p>

		<p>support a reduction in the current plan to 100 spots if changes in the plan focus on the quality of parking rather than mere quantity. More covered bike parking encourages year-round cycling in our rainy climate.</p> <p>Location:</p> <p>As currently designed, the bike parking has unbalanced distribution of bike parking as related to neighborhood and campus facility access areas. Distance from the primary entrance for some bicycle parking needs to be increased beyond 500-Feet, so as help balance access. Parking that encourages active transportation during the school day, as well as to evening theater performances, summer sports practices, and neighborhood meetings is worthy of attention and meets a wider variety of community needs.</p> <p>Safety & Inclusion:</p> <p>The current plan for bike parking locations create a large number of conflicts within the interior campus for students on bikes and buses. This daily path of traffic is an issue of concern for students, parents, and school staff.</p> <p>Several students are currently riding their skateboards to school. These students leave their boards with the school secretary or a trusted teacher for safe keeping during the day. The proposed plan offers enhanced options for these students choosing active transportation and provides access outside of traditional school hours.</p>	
01-27-20	Clyde Leon Owen/ Neighbor	Does this construction have any impacted on our home and what will be the construction hours and where will they park?	The construction site is limited to SKSD property. Permitting processes established through the City for new construction, erosion control measures as required by the State of Oregon and all mandatory City ordinances will be enforced throughout the construction process.
01-23-20	Mark Young/ Project	We are coordinating the removal of our facilities on Howard ST SE with the school district and new streetlight installations with the City of Salem and the electrical contractor. The	The design team including architects and electrical engineers have been in contact with Mark Young and Matt McHill of PGE

	Manager/ PGE	attached Civil drawing has the utilities shown not shown on the submitted comment plans.	regarding design and construction of revised power supply. SKSD has established multiple work orders and contracts to facilitate future construction at South.
01-27-20	Margaret B Saxe/ Neighbor	I have a question regarding the map where it says OPEN CHANNEL EASEMENT FROM TOP OF CLARK CREEK CHANNEL. Does that mean there will be some kind of connection, like a bridge ,from the school property over to our neighborhood?	There is no scope of work adjacent to Clark Creek channel. There are no connections in the form of bridges from the school property neighboring properties.
01-27-20	Carolyn Timberlake/ Neighbor	Please have workers take a day off and end by evening time.	The construction activity will be required to follow current City ordinance and regulation as monitored by the City.
01-27-20	J. Linfoot	I hope there can be some quiet time – Sun. off or aft 5 PM or something some of us are home a lot. Thanks	See comment above.
02-05-20	James and Christine Heyen	In addition to our attached responses, we would also like to request a meeting between the applicant (Salem Keizer School District), the Planning Division, and neighbors listed in our response. We believe that our collective knowledge of traffic and pedestrian patterns in the neighborhood will be invaluable as the school district and City plan how to handle the challenges of the new construction. We also feel that we are the ones who will experience the greatest impacts from the proposed changes. As such, it is only appropriate that our concerns be heard. a. Waiving standards that require a greater setback from the street for the proposed higher building façade. Comment: As we are unsure how high the proposed new construction will be, we're concerned that resulting shadows will impact the quantity and quality of easterly sunlight that is cast on homes along Church and Oxford Street.	The proposed design is 30'-4" tall. At the closest point the building face will be 65'-3" off the centerline of Church Street. The building will be 42'-3" off the centerline of Howard Street. The new building front would be 8'-6" to the south of the existing, non-conforming, building corner. The building is 4'-8" lower than code maximum at the two-story portion. The tallest part of the theater will be 52'-6" tall and sits 84'-0" from the center line of Howard Street. This building face, parallel to Howard Street is 34'-0" long.

		<p>b. Retention of the existing mature trees along Church, Rural, and Howard Streets.</p> <p>Comment: We have no objections to the retention of the mature trees along any of these streets. In fact, we would request that additional trees be added along the west edge of the school facing Church Street.</p>	<p>As a maintenance concern, new trees are planned for areas not directly adjacent to buildings. Where possible, mature trees will be maintained and protected throughout the construction process.</p>
		<p>c. The requirement of a traffic impact analysis.</p> <p>Comment: We strongly disagree with the assertion that a traffic analysis is not warranted. The forthcoming renovation and expansion to SSHS (which we support) will bring with it significant impacts to traffic patterns in our neighborhood – patterns which in turn significantly impact our quality of life and the safety of our streets. We insist that the school district conduct a thorough traffic analysis which will enable planners to make wise decisions, supported by data, regarding the flow of traffic through our neighborhood. At present, we feel that this issue has been given insufficient thought and that there are more creative strategies that should be explored.</p> <p>Suggestions:</p> <ol style="list-style-type: none"> i. One-way grid pattern on Church Street and/or Oxford Street. ii. Implementation of speed-calming traffic controls along the streets abutting the school. iii. Addition of signage indicating “local access only” at entrances to the alley ways between Church and High Street. iv. Addition of a three-way stop at the intersection of Church and Oxford Streets. v. Improved cross walks with increased signage and flashing lights. vi. Addition of flashing 20 mph school zone signs throughout the neighborhood. vii. Speed bumps on Church and Howard Streets. <p>Reduction of off-street parking from 375 spaces to 253 spaces.</p> <p>Comment: We have concerns about this recommendation. While this won't necessarily have an impact during the construction period, we forecast that it will have negative impact on the neighborhood and student body population continue to increase. With the projected increase in the SSHS student</p>	<p>DKS Associates has been retained to provide a traffic analysis/transportation evaluation. The document was provided as support to the proposal. The file name is 76-Transportation-Evaluation-SSHS-REV02 dated 10/30/19. This document addresses Parent Loading Areas beginning on page 15. The one-way grid was studied. The conclusion resulted in speed-calming traffic control items and improved cross walks. Alley signage would be outside the scope of this project and fall to City regulations and enforcement.</p>
		<p>Reduction of off-street parking from 375 spaces to 253 spaces.</p> <p>Comment: We have concerns about this recommendation. While this won't necessarily have an impact during the construction period, we forecast that it will have negative impact on the neighborhood and student body population continue to increase. With the projected increase in the SSHS student</p>	<p>See comments above. The proposed number of off-street parking was evaluated in the traffic analysis as being enough to provide the needed additional parking (per City code) for the anticipated increase in students/staff. The campus will have a net increase of 44 parking spaces on site through the proposed design. Therefore, there is a result of no additional impact to the</p>

		<p>population of approximately 500 students, a 30% reduction in parking is counterintuitive.</p>	<p>adjacent neighborhood beyond today's conditions. Additionally, a parking survey of the existing HS showed that only 61% of on-site parking was being utilized by the high school.</p>
		<p>Change in the distance to the primary entrance for bicycle parking from 50-feet to 500-feet. Comment: We are interested in encouraging bicycle use as a transportation option for students and faculty. Our concern is that locating bicycle racks further away from the main entrance would only discourage bicycle use. Further, our neighborhood has experienced repeated cycles of bicycle theft in recent years and keeping bicycles in prominent view could deter bicycle thieves.</p>	<p>See comments above.</p>
		<p>Reduction of interior landscaping for off-street parking areas. Comment: This proposal seems reasonable for the parking areas. However, and as noted previously, we would like additional trees planted in the front west side) of the school on the open grass area.</p>	<p>See comments above.</p>
		<p>Proposed changes to the intersection of Church and Oxford Streets. Specifically, the addition of a barrier with pedestrian refuge to prevent left-hand turns from Oxford onto Church and from Church onto Oxford. Comment: We do not believe a concrete median preventing left-hand turns will resolve the existing traffic flow issues at this intersection. Our observation is that congestion consistently results from parents stopping to drop students off directly in front of the school, not from cars making left turns onto Oxford or Church Streets. We feel the addition of a concrete barrier will have no meaningful impact on this behavior. Instead, we fear that the most likely outcome will be increase traffic using alley ways between High and Church Streets as people attempt to navigate their exit after dropping off their students.</p>	<p>See comment above.</p>
		<p>We do not support the proposed intersection change to Church and Oxford Streets. Instead, we recommend the ideas previously offered in item 1, above. We feel that the desired outcomes of efficient traffic flow and student safety will be better achieved with the addition of a three-way stop and improved crosswalks.</p>	

	<p>Impact of proposed new student parking lot on traffic on Howard Street.</p> <p>Comment: We feel it is essential that a traffic analysis be conducted to assess the impact of the new student parking lot on Howard Street. Relocation the primary student parking lot to this end of campus will place a substantially higher traffic demand on this already narrow street. We are very interested to know what improvements the city proposes for Howard Street in order to accommodate the increased traffic load, while ensuring safety in this portion of our neighborhood.</p>	<p>DKS Associates has been retained to provide a transportation evaluation. The document was provided as support to the proposal. The file name is 76-Transportation-Evaluation-SSHS-REV02 dated 10/30/19. The study accounted for future growth of the student population, impacts to Howard Street/Church Street intersection based on redistribution of student parking to the lot off Howard Street, and multiple ideas to improve the efficiency of the parent drop-off/pickup activity at Oxford St/Church St.</p>
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MEMO

TO: Olivia Dias, Planner III
Community Development Department

FROM: Glenn J. Davis, PE, CFM, Chief Development Engineer
Public Works Department

DATE: January 31, 2020

**SUBJECT: PUBLIC WORKS RECOMMENDATIONS
SPR-ADJ-DAP20-03 (19-121324)
1910 CHURCH STREET SE
SOUTH SALEM HIGH SCHOOL ADDITIONS**

PROPOSAL

Class 3 Site Plan Review for the addition of classrooms, performing arts classrooms and theater, new auxiliary gym, field house and parking area of the existing Salem High School. An alternative street standard is requested for Howard Street, Church Street and Rural Street abutting the subject property. Driveway Approach Permits for five driveways to be constructed or reconstructed. Two Class 2 Adjustments are requested for:

- 1) A reduction to minimum off-street parking requirements from 375 spaces to 253 spaces.
- 2) A reduction to minimum 20-foot setback abutting a street in order to construct an addition two feet from Howard Street and 10 feet abutting Church Street.
- 3) A reduction to the minimum distance from primary entrance for bicycle parking from 50-feet to 500-Feet.
- 4) A reduction to the requirement for interior landscaping for off-street parking areas.

For property approximately 22.09 acres in size, zoned PE (Public/Private Education) and located at 1910 Church Street SE - 97301 (Marion County Assessor Map and Tax Lot number: 073W34AC / 100, 073W34AD / 15500 AND 073W34AD / 15400).

RECOMMENDED CONDITIONS OF APPROVAL

1. Design and construct a storm drainage system at the time of development in compliance with *Salem Revised Code* (SRC) Chapter 71 and *Public Works Design Standards* (PWDS).
2. Provide a restoration plan and performance security pursuant to SRC 77.122 or

Code authority references are abbreviated in this document as follows: *Salem Revised Code* (SRC); *Public Works Design Standards* (PWDS); *Salem Transportation System Plan* (Salem TSP); and *Stormwater Management Plan* (SMP).

SRC 110.100 for the temporary modular unit gravel pad proposed on Rural Street SE.

3. Convey land for dedication to equal a half-width right-of-way of 30 feet on the development side of Howard Street SE, including sufficient right-of-way to accommodate public infrastructure at the property corners.
4. Provide a public sidewalk easement along the development frontage of Howard Street SE and Church Street SE for portions of sidewalk that are not located within the right-of-way.
5. Reconstruct the curb ramp at the intersection of Church Street SE and Howard Street SE, including AC pavement restoration as needed.
6. Construct a 15-foot half-street improvement on Howard Street SE from Winter Street SE to Davidson Street SE to Local street standards as specified in the City Street Design Standards and consistent with the provisions of SRC Chapter 803.
7. The proposed driveway at Winter Street SE and Howard Street SE shall be redesigned to meet PWDS.
8. Construct sidewalk as shown on the applicant's site plan along the frontage of Howard Street SE and Rural Street SE. In addition, sidewalks not in conformance with current ADA cross slope standards shall be replaced along the development frontage of Church Street SE.
9. Provide easements for public utility mains within the subject property pursuant to PWDS.
10. Install street trees to the maximum extent feasible along all development frontages of Howard Street SE, Church St SE and Rural Street SE.
11. Provide a performance guarantee pursuant to SRC 77.122 or SRC 110.100 in an amount not to exceed \$15,000 for construction of traffic calming devices according to a traffic analysis performed within 1 year of occupancy of the proposed school additions. The performance guarantee shall be released a maximum of three (3) years after occupancy for the proposed use.

FACTS

Streets

1. Howard Street SE

- a. Standard—This street is designated as a Local street in the Salem TSP. The standard for this street classification is a 30-foot-wide improvement within a 60-foot-wide right-of-way.
- b. Existing Conditions—This street has a varied 21-foot to 40-foot improvement within a 40-foot-wide right-of-way abutting the subject property.

2. Church Street SE

- a. Standard—This street is designated as a Local street in the Salem TSP. The standard for this street classification is a 30-foot-wide improvement within a 60-foot-wide right-of-way.
- b. Existing Conditions—This street has an approximate 30-foot improvement within a 60-foot-wide right-of-way abutting the subject property.

3. Rural Avenue SE

- a. Standard—This street is designated as a Collector street in the Salem TSP. The standard for this street classification is a 40-foot-wide improvement within a 60-foot-wide right-of-way.
- b. Existing Conditions—This street has an approximate 40-foot improvement within a 60-foot-wide right-of-way abutting the subject property.

Storm Drainage

1. Existing Conditions

- a. A 42-inch storm main is located Howard Street SE.
- b. A 24-inch storm main is located in Church Street SE.
- c. An 18-inch storm main is located in Rural Avenue SE.
- d. Two 30-inch private storm lines are located on the subject property.

Water

1. Existing Conditions

- a. The subject property is located in the G-0 water service level.
- b. A 6-inch water main is located in Howard Street SE. Mains of this size generally convey flows of 500 to 1,200 gallons per minute.
- c. A 6-inch water main is located in Church Street SE. Mains of this size generally convey flows of 500 to 1,200 gallons per minute.
- d. A 30-inch water main is located in Rural Avenue SE. Mains of this size generally convey flows of 13,200 to 30,800 gallons per minute.
- e. A 6-inch water main is located in Rural Avenue SE. Mains of this size generally convey flows of 500 to 1,200 gallons per minute.

Sanitary Sewer

1. Existing Conditions

- a. There are 8-inch and 10-inch sewer mains located on the subject property, which do not appear to be within existing public easements.
- b. A 10-inch sewer main is located on the subject property.
- c. An 8-inch sewer main is located in Church Street SE.
- d. An 8-inch sewer main is located in Rural Avenue SE.
- e. A 10-inch sewer main is located in Rural Avenue SE.

CRITERIA AND FINDINGS

Analysis of the development based on relevant criteria in SRC 220.005(f)(3) is as follows:

Criteria: SRC 220.005(f)(3)(A) The application meets all applicable standards of the UDC (*Unified Development Code*)

Finding—With completion of the conditions above, the subject property meets all applicable standards of the following chapters of the UDC: 601 – Floodplain, 802 –

Public Improvements, 803 – Streets and Right-of-Way Improvements, 804 – Driveway Approaches, 805 – Vision Clearance, 809 – Wetlands, and 810 - Landslides.

Public Works staff has reviewed the Flood Insurance Study and Flood Insurance Rate Maps and has determined that no floodplain or floodway areas exist on the subject property.

The Salem-Keizer Local Wetland Inventory shows that there are wetland channels and/or hydric soils mapped on the property. The applicant should contact the Oregon Department of State Lands to verify if any permits are required for development or construction in the vicinity of the mapped wetland area(s). Wetland notice was sent to the Oregon Department of State Lands pursuant to SRC 809.025.

According to the City's adopted landslide hazard susceptibility maps and SRC Chapter 810 (Landslide Hazards), there are mapped 2-point landslide hazard areas on the subject property. The proposed activity of a school building addition adds 3 activity points to the proposal, which results in a total of 5 points. Therefore, the proposed development is classified as a moderate landslide risk and requires a geological assessment. A Geotechnical Engineering Report, prepared by Professional Service Industries, Inc. and dated April 25, 2019, was submitted to the City of Salem. A supplemental Geotechnical Report Addendum dated June 26, 2019, was also submitted to the City of Salem. This assessment and subsequent addendum demonstrate the subject property could be developed without increasing the potential for slope hazard on the site or adjacent properties.

Criteria: SRC 220.005(f)(3)(B) The transportation system provides for the safe, orderly, and efficient circulation of traffic into and out of the proposed development, and negative impacts to the transportation system are mitigated adequately

Finding—Rural Avenue SE meets the right-of-way width and pavement width standards per the Salem TSP. The applicant has requested an alternative sidewalk standard to allow for portion of replaced sidewalk to be built as curb line sidewalk to accommodate the existing right-of-way configuration. The sidewalk may be located along the curb line as shown on the proposed plan pursuant to SRC 803.035(l)(2)(B). The existing right-of-way configuration does not allow for street trees to be planted pursuant to SRC 86.015(e), therefore, staff supports an alternative street standard to allow trees to be planted on private property.

Church Street SE meets the right-of-way width and pavement width standards per the Salem TSP. The applicant has requested an alternative street standard to allow for the existing streetscape to remain with the exception of the curb ramp at Church Street SE and Howard Street SE. Staff has reviewed the proposed request and recommend sidewalks along the development frontage of Church Street SE not in conformance with

current ADA cross slope standards be replaced. The proposed curb ramp replacement at the corner of Church Street SE and Howard Street SE shall be replaced in conformance with PWDS, including AC pavement restoration as needed to correct excessive running slope in the existing crosswalk. Public Works staff recommends approving the alternative street standard request on Church Street SE to maintain the existing sidewalk location and width pursuant to SRC 803.065(a)(1).

The existing configuration of Howard Street SE does not meet current standards for its classification of street per the Salem TSP. The applicant has requested an alternative street standard to maintain the existing 20-foot width on Howard Street SE from Winter Street SE to Davidson Street SE. The City Traffic Engineer has reviewed this proposal and does not support an alternative street standard. The existing pavement width is not sufficient to accommodate for safe turning movements into and out of the proposed parking lot improvements on Howard Street SE. As specified in the conditions of approval, the applicant shall construct a half-street improvement to Local street standards as specified in the Public Works Design Standards and consistent with the provisions of SRC Chapter 803, including an 8-foot-wide sidewalk and street trees. Public Works staff recommends approving the alternative street standard request to maintain the existing improvement width and sidewalk location on Howard Street SE from Church Street SE to Winter Street SE. The alternative street standard is approved based pursuant to SRC 803.065(a)(1) and the need for an accessible bus drop off location.

The applicant is proposing improvements to traffic control at the intersection of Church Street SE and Oxford Street SE based on a voluntary Transportation Evaluation provided by DKS Associates. These improvements are not required as a condition of approval for the proposed development. The improvements consist of a raised concrete median and barricades intended to prohibit left turns from northbound traffic on Church Street SE and for eastbound traffic on Oxford Street SE. Staff supports the proposed improvements but recommend reviewing if the median is warranted after construction and occupancy of the school addition.

As shown on the proposed site plan, the applicant shall convey land for dedication to equal a half-width right-of-way of 30 feet on the development side of Howard Street SE, including sufficient right-of-way to accommodate public infrastructure at the property corners. The applicant shall provide a public sidewalk easement along the development frontage of Howard Street SE and Church Street SE for portions of sidewalk that are not located within the right-of-way.

Street standards require that sidewalks shall be located parallel to and one foot from the adjacent right-of-way (SRC 803.035(I)(2)(A)); however, findings provided by the School District demonstrate the need for a curb line sidewalk along Howard Street SE between Church St SE and Winter Street SE to accommodate accessible school bus drop off areas. Therefore, the sidewalk may be located along the curb line as shown on the

proposed plan pursuant to SRC 803.035(l)(2)(B). New and replaced sidewalks shall be constructed a minimum of 8-feet in width pursuant to SRC 803.035(l)(2)(D).

Pursuant to SRC 86.015(e), the applicant shall install street trees to the maximum extent feasible along all development frontages.

Criteria: SRC 220.005(f)(3)(C) Parking areas and driveways are designed to facilitate safe and efficient movement of vehicles, bicycles, and pedestrians

Finding— The proposed driveways on Rural Ave SE provide for safe turning movements into and out of the property. As specified in the conditions of approval, the proposed driveway approaches to Howard Street SE shall be modified to meet current PWDS and accommodate the required boundary street improvement along the eastern portion of Howard Street SE.

Staff have reviewed the relocation of the main parking area from Rural Street SE to Howard Street SE and recommends the applicant provide a security for general traffic calming measures to address the potential impacts and change of traffic patterns that may result. The applicant shall provide a performance guarantee pursuant to SRC 77.122 or SRC 110.100 in an amount not to exceed \$15,000. A traffic analysis shall be performed within 1 year of occupancy of the proposed school additions. The performance guarantee shall be released a maximum of three (3) years after occupancy for the proposed use.

Criteria: SRC 220.005(f)(3)(D) The proposed development will be adequately served with City water, sewer, storm drainage, and other utilities appropriate to the nature of the development

Finding—The Public Works Department has reviewed the applicant's preliminary plan for this site. The water, sewer, and storm infrastructure are available within surrounding streets/areas and are adequate to serve the proposed development, with the exception of public sewer mains that bisect the subject property. The applicant shall provide easements for public utility mains within the subject property pursuant to PWDS.

The applicant's engineer submitted a statement demonstrating compliance with Stormwater PWDS Appendix 004-E(4)(b) and SRC Chapter 71. The preliminary stormwater design demonstrates the use of green stormwater infrastructure to the maximum extent feasible for all areas of new or replaced impervious surface, but requests an exception for the proposed temporary modular unit site on Rural Ave SE. This area is proposed to be a temporary pad for placement of modular units during construction. Due to the temporary nature of this impervious surface, staff supports alternative storm design standards under the condition the applicant provides a restoration plan and performance security pursuant to SRC 77.122 or SRC 110.100. The performance security shall include an approved amount that ensures this area is

amended to restore infiltration abilities of the soil and include contingencies for discovery after the modular units are removed.

The applicant shall design and construct all utilities (sewer, water, and storm drainage) according to the PWDS and to the satisfaction of the Public Works Director.

Howard Street Eastern Driveway

Criteria—A Class 2 Driveway Approach Permit shall be granted if:

- (1) The proposed driveway approach meets the standards of this Chapter and the Public Works Design Standards;**

Finding- The proposed driveway meets the standards of SRC Chapter 804 and Public Works Design Standards (PWDS).

- (2) No site conditions prevent placing the driveway approach in the required location;**

Finding—There are no site conditions prohibiting the location of the proposed driveway.

- (3) The number of driveway approaches onto an arterial are minimized;**

Finding—The proposed driveway is not accessing onto an arterial street.

- (4) The proposed driveway approach, where possible:**

- i. Is shared with an adjacent property; or**
- ii. Takes access from the lowest classification of street abutting the property;**

Finding—The proposed driveway is currently located with access to the lowest classification of street abutting the subject property.

- (5) Proposed driveway approach meets vision clearance standards;**

Finding—The proposed driveway meet the PWDS vision clearance standards set forth in SRC Chapter 805.

- (6) The proposed driveway approach does not create traffic hazards and provides for safe turning movements and access;**

Finding—The driveway will be constructed in coordination with the required half-street improvement on Howard Street SE and is not expected to create traffic hazards.

(7) The proposed driveway approach does not result in significant adverse impacts to the vicinity;

Finding—Staff analysis of the proposed driveway and the evidence that has been submitted indicate that the location of the proposed driveway will not have any adverse impacts to the adjacent properties or streets if constructed as part of the required half-street improvement on Howard Street SE.

(8) The proposed driveway approach minimizes impact to the functionality of adjacent streets and intersections; and

Finding—The eastern driveway access onto Howard Street SE is located on a Local street and does not create a significant impact to adjacent streets and intersections.

(9) The proposed driveway approach balances the adverse impacts to residentially zoned property and the functionality of adjacent streets.

Finding—

The proposed development is surrounded by residentially zoned property. The proposed driveway is taken from the lowest classification street abutting the subject property. The driveway balances the adverse impacts to residentially zoned property and will not have an effect on the functionality of the adjacent streets if constructed as part of the required half-street improvement on Howard Street SE.

Howard Street Western Driveway

Criteria—A Class 2 Driveway Approach Permit shall be granted if:

(1) The proposed driveway approach meets the standards of this Chapter and the Public Works Design Standards;

Finding- As specified in the conditions of approval, the proposed driveway must be redesigned to meet the standards of SRC Chapter 804 and Public Works Design Standards (PWDS).

(2) No site conditions prevent placing the driveway approach in the required location;

Finding—There are no site conditions prohibiting the location of the proposed driveway.

(3) The number of driveway approaches onto an arterial are minimized;

Finding—The proposed driveway is not accessing onto an arterial street.

(4) The proposed driveway approach, where possible:

- i. **Is shared with an adjacent property; or**
- ii. **Takes access from the lowest classification of street abutting the property;**

Finding—The proposed driveway is currently located with access to the lowest classification of street abutting the subject property.

(5) Proposed driveway approach meets vision clearance standards;

Finding—The proposed driveway meet the PWDS vision clearance standards set forth in SRC Chapter 805.

(6) The proposed driveway approach does not create traffic hazards and provides for safe turning movements and access;

Finding— No evidence has been submitted to indicate that the proposed driveway will create traffic hazards or unsafe turning movements if constructed pursuant to PWDS.

(7) The proposed driveway approach does not result in significant adverse impacts to the vicinity;

Finding—Staff analysis of the proposed driveway and the evidence that has been submitted indicate that the location of the proposed driveway will not have any adverse impacts to the adjacent properties or streets if constructed as part of the required half-street improvement on Howard Street SE.

(8) The proposed driveway approach minimizes impact to the functionality of adjacent streets and intersections; and

Finding—The eastern driveway access onto Howard Street SE is located on a Local street and does not create a significant impact to adjacent streets and intersections.

- (9) The proposed driveway approach balances the adverse impacts to residentially zoned property and the functionality of adjacent streets.**

Finding—

The proposed development is surrounded by residentially zoned property. The proposed driveway is taken from the lowest classification street abutting the subject property. The driveway balances the adverse impacts to residentially zoned property and will not have an effect on the functionality of the adjacent streets if constructed as part of the required half-street improvement on Howard Street SE.

Rural Avenue Auxiliary Gym Driveways

Criteria—A Class 2 Driveway Approach Permit shall be granted if:

- (1) The proposed driveway approach meets the standards of this Chapter and the Public Works Design Standards;**

Finding- The proposed driveways meets the standards of SRC Chapter 804 and Public Works Design Standards (PWDS) for two-way driveway approaches to a Collector street.

- (2) No site conditions prevent placing the driveway approach in the required location;**

Finding—There are no site conditions prohibiting the location of the proposed driveways.

- (3) The number of driveway approaches onto an arterial are minimized;**

Finding—The proposed driveways are not accessing onto an arterial street.

- (4) The proposed driveway approach, where possible:**

- i. Is shared with an adjacent property; or**
- ii. Takes access from the lowest classification of street abutting the property;**

Finding—The proposed driveways are currently located with access to the lowest classification of street abutting the proposed parking lot renovation.

(5) Proposed driveway approach meets vision clearance standards;

Finding—The proposed driveways meet the PWDS vision clearance standards set forth in SRC Chapter 805.

(6) The proposed driveway approach does not create traffic hazards and provides for safe turning movements and access;

Finding—The proposed driveways will not create a known traffic hazard and will provide for safe turning movements for access to the subject property.

(7) The proposed driveway approach does not result in significant adverse impacts to the vicinity;

Finding—Staff analysis of the proposed driveway and the evidence that has been submitted indicate that the location of the proposed driveways will not have any adverse impacts to the adjacent properties or streets.

(8) The proposed driveway approach minimizes impact to the functionality of adjacent streets and intersections; and

Finding—The proposed driveways do not appear to create a significant impact to adjacent streets and intersections and reduce the number of existing driveways in the vicinity.

(9) The proposed driveway approach balances the adverse impacts to residentially zoned property and the functionality of adjacent streets.

Finding—

The proposed development is surrounded by residentially zoned property. The proposed driveways are replacing three existing one-way driveways and provide improved traffic circulation, balancing the adverse impacts to residentially zoned property and functionality of the adjacent streets.

Rural Avenue Fire Access Driveway

Criteria—A Class 2 Driveway Approach Permit shall be granted if:

(1) The proposed driveway approach meets the standards of this Chapter and the Public Works Design Standards;

Finding- The proposed driveway meets the standards of SRC Chapter 804 and Public Works Design Standards (PWDS) for a one-way driveway approach to a Collector street.

(2) No site conditions prevent placing the driveway approach in the required location;

Finding—There are no site conditions prohibiting the location of the proposed driveways. The new driveway is reconstructing an existing driveway approach.

(3) The number of driveway approaches onto an arterial are minimized;

Finding—The proposed driveway is not accessing onto an arterial street.

(4) The proposed driveway approach, where possible:

- i. **Is shared with an adjacent property; or**
- ii. **Takes access from the lowest classification of street abutting the property;**

Finding—The proposed driveway is currently located with access to the lowest classification of street abutting the proposed development frontage.

(5) Proposed driveway approach meets vision clearance standards;

Finding—The proposed driveway meets the PWDS vision clearance standards set forth in SRC Chapter 805.

(6) The proposed driveway approach does not create traffic hazards and provides for safe turning movements and access;

Finding—The proposed driveway will not create a known traffic hazard and will provide for safe turning movements for access to the subject property.

(7) The proposed driveway approach does not result in significant adverse impacts to the vicinity;

Finding—Staff analysis of the proposed driveway indicate that the location of the proposed driveway will not have any adverse impacts to the adjacent properties or streets.

(8) The proposed driveway approach minimizes impact to the functionality of adjacent streets and intersections; and

Finding—The proposed driveway does not appear to create a significant impact to adjacent streets and intersections.

(9) The proposed driveway approach balances the adverse impacts to residentially zoned property and the functionality of adjacent streets.

Finding—The proposed development is surrounded by residentially zoned property. The proposed driveway is replacing an existing driveway in the same location.

RESPONSE TO CITIZEN COMMENTS

Traffic Impact Analysis

Citizen testimony suggests that a TIA should be required.

Staff response: SRC 803.015(b)(1) states that TIAs are required for developments that generate over 1,000 daily vehicle trips on a Collector street. The Assistant City Traffic Engineer has determined that the proposed school additions generate less than 1,000 daily trips. Rural Avenue S is a Collector street as shown in the *Salem Transportation System Plan*. Street classification is based on the *Salem Transportation System Plan*, not the existing street condition. No TIA is required.

Staff have reviewed the citizen testimony and will recommend security for general traffic calming measures to address the potential impacts of the proposed development. Other improvements suggested by testimony will need to be evaluated under separate review and are not included as part of this application.

Sidewalk and Driveway Improvements

Testimony suggests that driveway entrances should be brought into compliance to improve pedestrian safety and access.

Staff response:

Sidewalks along the development frontages are being replaced to an 8-foot width as required by Public Works Design Standards and summarized in the conditions of approval. New and replaced driveway approaches must be replaced to meet current Public Works Design Standards as specified in the conditions of approval.

Crosswalk Markings

Citizen testimony suggests that crosswalks at intersections along Rural Avenue, Church

Street and Howards Street should marked on both sides of the intersections.

Staff response:

Current markings are intentional to create an orderly crossing pattern around the school. Students are encouraged to cross at a single point rather than multiple locations for safety reasons. Multiple crossing in the area may create unnecessary traffic conflicts with pedestrians and vehicles. Staff does not support additional crossings at this time but will reevaluate after occupancy of the proposed use.

Open Channel Easement on Clark Creek

Citizen testimony requests information regarding reference to the Open Channel Easement referenced on the site plan and a connection, such as a bridge, across Clark Creek.

Staff response:

The Open Channel Drainage Easement on Clark Creek is existing and not proposed to be modified with this project. A bridge connection is not proposed.

Proposed Median at Church and Oxford Street

Citizen testimony objects to the proposed traffic mitigation measures at the intersection of Church Street SE and Oxford Street SE.

Staff response:

The proposed traffic mitigation measures are based on a voluntary traffic analysis and recommended by the applicants traffic engineer. The proposal will be further reviewed as part of the plan approval process pursuant to SRC 77.091. Staff is recommending the school district reevaluate one year after the expansion is complete to determine if the mitigation measures are still necessary.

Prepared by: Robin Dalke, Development Services Operations Manager
cc: File