

**TO: PLANNING COMMISSION**

**FROM: LISA ANDERSON-OGILVIE, AICP  
DEPUTY COMMUNITY DEVELOPMENT DIRECTOR AND  
PLANNING ADMINISTRATOR**

**SUBJECT: FAIRVIEW REFINEMENT PLAN MAJOR AMENDMENT, DESIGN  
REVIEW, SITE PLAN REVIEW, ADJUSTMENT, AND DRIVEWAY  
APPROACH PERMIT CASE NO. FRPA-DR-SPR-ADJ-DAP21-02; FOR  
PROPERTIES LOCATED AT 2110 STRONG ROAD SE AND THE 4000 TO  
4100 BLOCKS OF REED ROAD SE  
  
(AMANDA APPLICATION NOS. 21-101926-ZO; 21-101951-DR; 21-  
101928-RP; 21-109929-ZO; 21-109932-ZO; 21-101952-ZO)**

**REQUEST**

A consolidated application for a proposed 183-unit multiple family development on two portions of the former Fairview Training Center site totaling approximately 10.74 acres in size. The application includes the following:

- 1) A Class 1 Design Review and Class 3 Site Plan review for the proposed multiple family development.
- 2) A Major Amendment to the Fairview Refinement Plan II refinement plan to:
  - a) Increase the number of dwelling units allowed within the refinement plan from 280 to 457;
  - b) Allow the lot for the East development to exceed the maximum lot depth requirements of the AU (Adaptive Use) and MI (Mixed-Intensity) areas of the refinement plan;
  - c) Allow the minimum required floor-area-ratio (lot coverage) of the West development located within the VC (Village Center) area of the refinement plan to be less than 0.75 FAR;
  - d) Allow the proposed carports within the development to exceed the maximum 1,000 square-foot building footprint required in the VC, AU, and MI areas.
  - e) Allow less than 70 percent of the lot/street frontage of the West development within the VC area of the refinement plan along Lindburg Road and Strong Road to be occupied by buildings placed at the minimum setback line;
  - f) Allow Buildings 20, 23, 25, 26, 27, 28, 29, 30, 31, 32, 33, and 35 to be setback greater than the maximum 20-foot setback abutting a street required in the VC, AU, and MI areas of the refinement plan;
  - g) Allow Buildings 22 and 24 to be setback less than the minimum required 10-foot setback abutting a street required in the AU and MI areas of the refinement plan;
  - h) Allow the proposed off-street parking spaces on the south side of Chapel Drive within the East development and the proposed off-street parking spaces along the

- southern property line of the West development to be setback less than the minimum 20-foot setback abutting a street required in the VC and MI areas of the refinement plan;
- i) Allow the off-street parking spaces located within one of the proposed parking lots within the East development abutting phase 1 of The Grove apartments to be less than the minimum 10-foot setback required abutting an interior side property line in the AU area of the refinement plan;
  - j) Allow three driveway approaches onto the private streets within the East development where a maximum of two driveway approaches per parcel are allowed onto private streets within the AU and MI areas of the refinement plan;
  - k) Allow the driveway approaches onto the proposed private streets within the East and West developments to exceed the maximum driveway approach widths required under the VC, AU, and MI areas of the refinement plan;
- 3) A Class 1 Adjustment to increase the maximum number of parking spaces allowed within the East development from 137 spaces to 146 spaces (SRC 806.015(d)).
- 4) A Class 2 Adjustment to:
- a) Decrease the minimum number of trees required to be planted around the perimeter of buildings (SRC 702.020(b)(4));
  - b) Decrease the minimum number of trees required to be planted around the perimeter of off-street parking areas and allow some of the proposed parking lot planter bays within the development to be less than the minimum required nine feet in width (SRC 702.020(b)(7));
  - c) Allow less than 70 percent of the lot/street frontage of the West development within the VC area of the refinement plan along proposed Village Center Loop to be occupied by buildings placed at the setback line (SRC 702.020(e)(4));
  - d) Allow less than 50 percent of the lot/street frontages of the East development within the AU and MI areas of the refinement plan along streets to be occupied by buildings placed at the setback line (SRC 702.020(e)(4));
  - e) Allow ground floor dwelling units located within 25 feet of a property line abutting a street to be developed without a building entrance facing the street or a direct pedestrian connection to the adjacent sidewalk (SRC 702.020(e)(5));
  - f) Allow proposed buildings located adjacent to interior side property lines to be developed without a 45-degree façade setback plane for those portions of the buildings that are greater than 28 feet in height;
  - g) Allow dead-end off-street parking areas within the East development to be constructed without a turnaround (SRC 806.040(a)); and
  - h) Allow the proposed trash/recycling areas to be developed without on-site turnaround areas for waste collection service vehicles (SRC 800.055(f)(2)).
- 5) A Class 2 Driveway Approach Permit for the proposed driveway approaches within the West development onto Strong Road SE and Lindburg Road SE.

The subject properties total approximately 10.74 acres in size, are zoned FMU (Fairview Mixed-Use) within the Fairview Refinement Plan II refinement plan, and are located at

2110 Strong Road SE and the 4000 to 4100 Blocks of Reed Road SE (Marion County Assessor Map and Tax Lot Numbers: 083W0200100 and 083W11A01002).

**APPLICANT:** Neighborly Ventures, Inc. (Brian Moore, Justin Allen, Jason Tokarski, Elliott Pelfrey)

**OWNER:** Ward Development, LLC (Steve Ward, Marcus Ward, Kyle Ward, Travis Ward, Ryan Ward)

### **RECOMMENDATION**

Based upon the Facts and Findings contained in this staff report, staff recommends that the Planning Commission take the following action for the consolidated Fairview refinement plan major amendment, design review, site plan review, adjustment, and driveway approach permit to develop a proposed 183-unit multiple family development on approximately 10.74 acres of the former Fairview Training Center site located at 2110 Strong Road SE and the 4000 to 4100 Blocks of Reed Road SE:

A. **APPROVE** the Class 3 Site Plan Review for the proposed development, subject to the following conditions of approval:

- Condition 1:** Prior to issuance of building buildings permits for the proposed West development, the final plat for the Legacy Heights Subdivision (Case No. SUB-FRPA20-03) shall be recorded.
- Condition 2:** Obtain all state and federal permits as indicated by the Department of State Lands (DSL).
- Condition 3:** All trash/recycling areas shall conform to the solid waste service area standards of SRC 800.055.
- Condition 4:** Design and construct all City infrastructure, including streets, according to the refinement plan and PWDS.
- Condition 5:** Convey land for dedication to equal a half-width right-of-way of 36 feet along the entire frontage of the subject property measured from the newly established offset centerline of Reed Road SE, and as shown on public construction plans under permit number 21-101403-PC.
- Condition 6:** Construct a full-street improvement along the frontage of Reed Road SE to Minor Arterial street standards from Strong Road SE to approximately 500 feet south of Lindberg Road SE, and as shown on public construction plans under permit number 21-101403-PC.
- Condition 7:** Complete incomplete portions of the boundary street improvements along the property frontages of Strong Road SE and Lindberg Road SE.
- Condition 8:** Construct Village Center Loop SE (private) from Lindburg Road SE to Strong Road SE. The private/public street intersections shall be constructed pursuant to PWDS.

- Condition 9:** Construct an extension of Mountain Street SE (private) from its existing terminus at the northern boundary of the eastern property to Chapel Drive SE (private).
- Condition 10:** Construct incomplete portions of the boundary street improvements along the property frontages of Chapel Drive SE (private).
- Condition 11:** Dedicate public access easements for the proposed private streets within the development.
- Condition 12:** Dedicate a public access easement for the proposed 10-foot-wide multi-use path through the subject property.
- Condition 13:** Construct an 18-inch S-1 water main within Reed Road SE from Strong Road SE to approximately 500 feet south of Lindberg Road SE.
- Condition 14:** Design and construct a storm drainage system at the time of development in compliance with Salem Revised Code (SRC) Chapter 71 and Public Works Design Standards (PWDS).

B. **APPROVE** the Class 3 Design Review for the proposed development subject to the following conditions of approval:

- Condition 1:** A minimum of two plant units shall be provided adjacent to the primary entryway of each dwelling unit or combination of dwelling units.
- Condition 2:** Shrubs shall be distributed around the perimeter of buildings at a minimum density of one plant unit per 15 linear feet of exterior building wall.
- Condition 3:** The private ground floor patios included within the development shall be visually separated from common open space through the use of perimeter landscaping.
- Condition 4:** All parking area planter bays shall be a minimum of 9 feet in width.

C. **APPROVE** the Fairview Refinement Plan Major Amendment to:

- a) Increase the number of dwelling units allowed within the refinement plan from 280 to 457;
- b) Allow the lot for the East development to exceed the maximum lot depth requirements of the AU (Adaptive Use) and MI (Mixed-Intensity) areas of the refinement plan;
- c) Allow the minimum required floor-area-ratio (lot coverage) of the West development located within the VC (Village Center) area of the refinement plan to be less than 0.75 FAR;
- d) Allow less than 70 percent of the lot/street frontage of the West development within the VC area of the refinement plan along Lindburg Road and Strong Road to be occupied by buildings placed at the minimum setback line;

- e) Allow Buildings 20, 23, 24, 25, 26, 27, 28, 29, 30, 31, 32, 33, 34, and 35 to be setback greater than the maximum 20-foot setback abutting a street required in the VC, AU, and MI areas of the refinement plan;
  - f) Allow Building 24 to be setback less than the minimum required 10-foot setback abutting a street required in the AU and MI areas of the refinement plan;
  - g) Allow the proposed off-street parking spaces along the southern property line of the West development to be setback less than the minimum 20-foot setback abutting a street required in the VC and MI areas of the refinement plan;
  - h) Allow the off-street parking spaces located within one of the proposed parking lots within the East development abutting phase 1 of The Grove apartments to be less than the minimum 10-foot setback required abutting an interior side property line in the AU area of the refinement plan;
  - i) Allow three driveway approaches onto the private streets within the East development where a maximum of two driveway approaches per parcel are allowed onto private streets within the AU and MI areas of the refinement plan; and
  - j) Allow the driveway approaches onto the proposed private streets within the East and West developments to exceed the maximum driveway approach widths required under the VC, AU, and MI areas of the refinement plan.
- D. **APPROVE** the Class 1 Adjustment to increase the maximum number of parking spaces allowed within the East development from 137 spaces to 139 spaces.
- E. **DENY** the Class 2 Adjustment to decrease the minimum number of trees required to be planted around the perimeter of buildings and place the following condition on the design review approval to ensure conformance with the requirements of SRC 702.020(b)(4):
- Condition 5:** The landscaping plans shall be revised to include trees at a minimum density of 10 plant units per 60 linear feet of exterior building wall. Such trees shall be located not more than 25 feet from the edge of the building footprint.
- F. **DENY** the Class 2 Adjustment to allow ground floor dwelling units located within 25 feet of a property line abutting a street to be developed without a building entrance facing the street or a direct pedestrian connection to the adjacent sidewalk.
- G. **APPROVE** the Class 2 Adjustment to:
- a) Decrease the minimum number of trees required to be planted around the perimeter of off-street parking areas;
  - b) Allow less than 70 percent of the lot/street frontage of the West development within the VC area of the refinement plan along proposed Village Center Loop to be occupied by buildings placed at the setback line;
  - c) Allow less than 50 percent of the lot/street frontages of the East development within the AU and MI areas of the refinement plan along streets to be occupied by buildings placed at the setback line; and

- d) Allow proposed buildings located adjacent to interior side property lines to be developed without a 45-degree façade setback plane for those portions of the buildings that are greater than 28 feet in height.

H. **APPROVE** the Class 2 Driveway Approach Permit for the proposed driveway approaches within the West development onto Strong Road SE and Lindburg Road SE.

### **PROCEDURAL FINDINGS**

1. On January 19, 2021, Multi/Tech Engineering submitted, on behalf of the applicant, Neighborly Ventures, Inc., a consolidated Fairview Refinement Plan Major Amendment, Class 1 Design Review, Class 3 Site Plan Review, and Class 2 Driveway Approach Permit application for a proposed 183-unit multiple family development.

During the course of reviewing the application for completeness, the need for a Class 1 and Class 2 Adjustment to certain development standards of the Salem Revised Code (SRC) were also identified.

2. Because multiple land use applications are required in connection with the proposed development, the applicant, pursuant to SRC 300.120(c), chose to consolidate the applications and process them together as one. When multiple applications are consolidated, the review process for the application shall follow the highest numbered procedure type required for the land use applications involved, and the Review Authority for the application shall be the highest applicable Review Authority under the highest numbered procedure type.

Based on these requirements, the proposed consolidated application is required to be reviewed by the Planning Commission and processed as a Type III procedure.

3. The application was deemed complete for processing on May 25, 2021.
4. Notice of the public hearing for the proposed development was subsequently provided, pursuant to SRC requirements, on May 26, 2021. Public notice was posted on the properties by the City Case Manager for the application on June 4, 2021.
5. The public hearing on the proposed Fairview Refinement Plan Major Amendment, Class 1 Design Review, Class 3 Site Plan Review, Class 1 Adjustment, Class 2 Adjustment, and Class 2 Driveway Approach Permit application is scheduled for June 15, 2021.
6. 120-Day Rule. The state-mandated 120-day local decision deadline for the application is September 22, 2021.

### **BACKGROUND / PROPOSAL**

On September 24, 2003, the City Council adopted amendments to the Salem Area Comprehensive Plan (SACP) and Salem Revised Code (SRC) establishing a "Mixed Use" comprehensive plan designation and a "Fairview Mixed Use" (FMU) zone district. The adopted amendments were applied to the former Fairview Training Center site to promote the sustainable mixed-use development of the 275-acre property.

Development within the Fairview Mixed-Use zone requires a master planning process, the first step of which began with the adoption of the Fairview Plan in 2005. The Fairview Plan is the master plan for the entire Fairview site that establishes the overall goals and policies to guide future development of the property.

The second step in the master planning process is the adoption of more detailed refinement plans. Refinement plans are intended to apply to areas of the site not less than 40 acres in size. Refinement plans serve as detailed regulatory plans that implement the overall goals, policies, and development objectives of the Fairview Plan and establish the specific standards for development within the Refinement Plan area.

Since the approval of the Fairview Plan in 2005 the following four refinement plans have been approved for Fairview site:

- Pringle Creek Community (2005)
- Lindburg Green/Fairview Refinement Plan II (2009; Amended 2016)
- Simpson Hills (2012; Expired)
- Fairview Addition West (2014)

The application under review by the Planning Commission is a consolidated Fairview Refinement Plan Major Amendment, Class 1 Design Review, Class 3 Site Plan Review, Class 1 Adjustment, Class 2 Adjustment, and Class 2 Driveway Approach Permit for a proposed multiple family development on two portions of the Fairview Training Center site located at 2110 Strong Road SE and the 4000 to 4100 Blocks of Reed Road SE **(Attachment A)** and covered by the Fairview Refinement Plan II refinement plan. The Fairview Refinement Plan II refinement plan was originally approved in 2009 and subsequently amended in 2016. The 2016 amendment:

- 1) Expanded the boundaries of the refinement plan to include property for the City's Fairview community park, as well as land to the west and northwest of the park;
- 2) Reconfigured the layout of the refinement plan to accommodate the addition of the proposed park and the land to the west and northwest of the proposed park; and
- 3) Modified the development standards applicable within the refinement plan area.

Because the subject properties are located within the boundaries of the Fairview Refinement Plan II refinement plan, development of the properties is subject to the development standards included within that refinement plan. The Fairview Refinement Plan II refinement plan can be found on the City's website at the following location:

<https://www.cityofsalem.net/CityDocuments/fairview-refinement-plan-ii.pdf>

The applicant's proposal includes a total of 183 multiple-family dwelling units intended to be developed as a second phase of The Grove apartments at Fairview. A total of 105 of the proposed units are located on the western portion of the development at 2110 Strong Road SE (*southwest corner of the intersection of Lindburg Road SE and Strong Road SE*). The remaining 78 proposed units are located on the eastern portion of the development within the 4000 to 4100 Blocks of Reed Road SE (*between Strong Road SE and Lindburg Road SE*).

The proposed multiple family dwelling units are located within three-story buildings served by on-street and off-street parking, common open space, and internal private streets.

The western portion of the proposed development has frontage on Strong Road SE and Lindburg Road SE, and the eastern portion of the proposed development has frontage on Strong Road SE, Lindburg Road SE, and Reed Road SE. Strong Road and Lindburg Road are both designed as Collector streets under the City's Transportation System Plan (TSP) and Reed Road is designated as a Minor Arterial street under the TSP.

In addition to the above public streets, the proposed development will also be served by private streets. Within the western portion of the proposed development, Village Center Loop, a private street, extends east-west through the site between Lindburg Road and Strong Road to provide access to the 105 units proposed on that portion of the site. Within the eastern portion of the proposed development an existing private street, Chapel Lane, extends east-west through the site between Reed Road and Heritage Street to provide access to the northern and southern portions of that site. In addition, Mountain Street, a private street that was created as part of the first phase of The Grove apartments, extends north-south through the eastern portion of the proposed development between Strong Road and Chapel Drive.

Vehicular access to the development is proposed to be provided by existing and proposed streets. The western portion of the development will be served by two driveway approaches onto proposed Village Center Loop and the eastern portion of the development will be served by two driveway approaches onto Mountain Street and one driveway approach onto Chapel Drive. No new driveway approaches are proposed onto Reed Road SE.

Bicycle and pedestrian access to, within, and through the development will be provided via sidewalks and multi-use paths along existing and proposed streets, as well as an internal network of pedestrian connections, including a multi-use path, designed to provide connectivity between Lindburg Road and Chapel Lane.

The proposed development requires the following land use approvals:

- 1) Class 3 Site Plan Review.** Class 3 Site Plan Review is required for the proposed development because it meets the triggers for requiring Class 3 Site Plan Review under SRC 220.005(b)(3). Because the standards of the Fairview Refinement Plan II refinement plan supersede many of the standards of the Salem Revised Code (SRC), the purpose of the site plan review application is to review the development for conformance with the applicable standards of the refinement plan and those applicable standards of the SRC.

Findings evaluating the proposed development for conformance with the applicable Class 3 Site Plan Review approval criteria included under SRC 220.005(f)(3) are included under Section 7 of this report

- 2) Class 1 Design Review.** Design review is required for the proposed development because, as with multiple family developments located outside the Fairview Mixed-Use (FMU) zone, multiple family development within the Fairview Refinement Plan



II refinement plan is subject to design review according to the multiple family design review standards contained under SRC Chapter 702 (*with specific exceptions to certain design review standards identified under Section 7 – Table 2 (Fairview Alternative Multi-Family Design Standards) of the refinement plan on page 24*).

Findings evaluating the proposed development for conformance with the applicable multiple family design review standards of SRC Chapter 702 and Section 7 – Table 2 of the refinement plan, as required under SRC 225.005(e)(1), are included under Section 8 of this report.

- 3) Fairview Refinement Plan Major Amendment.** The applicant has requested a number of deviations from the provisions/development standards of the Fairview Refinement Plan II refinement plan in conjunction with the proposed development.

Pursuant to SRC 530.035, a major amendment to a refinement plan is required for any amendment to a refinement plan that results in a substantial change to the refinement plan. A substantial change to a refinement plan, includes, but is not limited to, one that:

- Increases or decreases the number of proposed residential units per acre by more than 20 percent or exceeds the maximum allowed number of dwelling units permitted within the FMU zone; or
- Varies building height, floor-area-ratio (FAR), lot coverage, building setbacks, or other development standards by more than 20 percent of that delineated in the refinement plan.

As identified in this staff report, a major amendment to eleven provisions of the refinement plan were requested by the applicant. The first major amendment requested seeks approval to increase the maximum number of residential units proposed within the refinement plan from 280 dwelling units to 457 dwelling units in order to allow the proposed development.

The other major amendments requested pertain to development standards of the refinement plan that the proposed development does not meet (*such as setback requirements, minimum building street frontage requirements, maximum number of allowed driveway approaches, etc...*), and which exceed the 20 percent threshold to require a major amendment to the standards. These major amendments are the equivalent to what would otherwise be reviewed as a zoning adjustment, but because the development standards are part of the refinement plan, a major amendment is required instead.

Findings evaluating the proposed Fairview Refinement Plan Major Amendment for conformance with the applicable approval criteria included under SRC 530.035(e)(2) are included under Section 9 of this report.

- 4) Class 1 & 2 Adjustments.** The applicant has also requested a number of adjustments to various development standards of the SRC in conjunction with the proposed development. As identified in this staff report, a Class 1 Adjustment has

been requested to allow the eastern portion of the proposed development to exceed its maximum allowed vehicle parking requirement. The remaining eight adjustments requested with the application are Class 2 Adjustments and primarily seek deviation from specific multiple family design review standards included under SRC Chapter 702.

Findings evaluating the proposed Class 1 Adjustment for conformance with the applicable approval criteria included under SRC 250.005(d)(1) are included under Section 10 of this report. Findings evaluating the proposed Class 2 Adjustment for conformance with the applicable approval criteria included under SRC 250.005(d)(2) are included under Section 11 of this report.

- 5) Class 2 Driveway Approach Permit.** A Class 2 Driveway Approach Permit is required for the proposed development in order to approve the proposed vehicular access to serve the development. Pursuant to SRC 804.001 and the driveway approach standards included under Section 7, Table 1, of the refinement plan, a driveway approach permit is only required for driveway approaches onto public streets. Driveways onto private streets are governed by the standards of the refinement plan and therefore reviewed through site plan review.

Because the proposed development includes only two driveway approaches onto a public street (*where proposed Village Center Loop connects to Lindburg Road and Strong Road within the western portion of the development*), the required driveway approach permit is for those two driveway approaches only.

Findings evaluating the proposed Class 2 Driveway Approach Permit for conformance with the applicable approval criteria included under SRC 804.025(d) are included under Section 12 of this report.

## **APPLICANT'S PLANS AND STATEMENT**

Land use applications must include a statement addressing the applicable approval criteria and be supported by proof they conform to all applicable standards and criteria of the Salem Revised Code. The written statement provided by the applicant summarizing the request and addressing the applicable approval criteria, as well as the site/development plans for the project, are attached to this report as identified below. Subsequent to the public notice being provided for the proposed development, revised site plans, open space plans, landscape plans, grading plans, and utility plans were provided by the applicant on June 1, 2021, in order to bring the development into greater conformance with some of the development standards that either a major amendment or adjustment had been requested for. The plans attached to this report are the applicant's most current revised versions of the plans.

- Existing Conditions Plan: **Attachment B**
- Revised Site, Open Space, Landscaping, Grading, and Utility Plans: **Attachment C**
- Building Elevations: **Attachment D**
- Applicant's Written Statement: **Attachment E**

Staff utilized the information provided by the applicant to evaluate the proposal and to establish the facts and findings included within the staff report.

## **SUMMARY OF RECORD**

The following items are submitted to the record and are available upon request: All materials submitted by the applicant, including any applicable professional studies such as traffic impact analysis, geologic assessments, and stormwater reports; any materials and comments from public agencies, City departments, neighborhood associations, and the public; and all documents referenced in this report.

## **FACTS AND FINDINGS**

### **1. Salem Area Comprehensive Plan (SACP)**

Comprehensive Plan Map: The subject property is designated Mixed-Use on the Salem Area Comprehensive Plan map.

The Comprehensive Plan designations of surrounding properties include:

<b>Comprehensive Plan Designation of Surrounding Properties</b>	
North	Mixed-Use
South	Mixed-Use
East	Mixed-Use
	Across Reed Road SE, Community Service Hospital
West	Mixed-Use

Relationship to Urban Service Area: The subject property lies outside the City's Urban Service Area. The Urban Service Area is that territory within City where all required public facilities (streets, water, sewer, storm water, and parks) necessary to serve development are already in place or fully committed to be extended.

Pursuant to the urban growth management requirements contained under SRC Chapter 200 (Urban Growth Management), properties located outside the Urban Service Area are required to obtain an Urban Growth Preliminary Declaration if development will proceed prior to the necessary public facilities being extended to the property and the Urban Service Area being expanded to incorporate the property.

Two UGA preliminary declarations have been approved for the Fairview property identifying required linking public facilities for streets, water, sewer, storm water, and parks that are required to be extended to or provided on the site in order to adequately serve the property.

The first UGA preliminary declaration (Case No. UGA04-10) was approved on September 30, 2004, and applied to the 32.5 acres of the Fairview property included within the Pringle Creek Community Refinement Plan. The second UGA preliminary declaration (Case No. UGA04-08) was approved on November 15, 2004, and applied to the remainder of the Fairview property. On August 4, 2011, an amendment to UGA04-08 was approved modifying the public facility requirements.

## 2. Zoning

The subject property is zoned Fairview Mixed Use (FMU). Zoning of surrounding properties is as follows:

Zoning of Surrounding Properties	
North	FMU (Fairview Mixed-Use)
South	FMU (Fairview Mixed-Use)
East	FMU (Fairview Mixed-Use)
	Across Reed Road SE, PH (Public and Private Health Services)
West	FMU (Fairview Mixed-Use)

## 3. Existing Conditions

There are two properties included with the proposed development. The western property is located at 2110 Strong Road SE and is approximately 4.3 acres in size. This property is currently part of a larger 25.27-acre property (Tax Lot No. 083W0200100) that received tentative approval for a subdivision in 2020 (*Subdivision Case No. SUB-FRPA20-03 – Legacy Heights subdivision*). When the final plat for the approved subdivision is recorded the subject 4.3-acre western property will become Lot 78 of the Legacy Heights subdivision plat.

The eastern property is located in the 4000 to 4100 Blocks of Reed Road SE and consists of one lot (*Parcel 1 of Partition Plat 2019-67*) that is approximately 6.44 acres in size.

In regard to the natural features of both properties, the topography of the western property slopes upward to the south whereas the topography of the eastern property is comparatively flat. There are no existing wetlands/waterways located on the western property, but there are trees, including significant Oregon White Oaks, and two mapped wetland areas located on the eastern property.

## 4. City Department Comments

- A. The City of Salem Building and Safety Division reviewed the proposal and identified no objections.
- B. The Salem Fire Department reviewed the proposal and indicated no objections.
- C. The Public Works Department reviewed the proposal and provided comments regarding street and City utility improvements required to serve the development and recommended conditions of approval to ensure conformance with the applicable requirements of the SRC. Comments from the Public Works Department are included as **Attachment F**.

## 5. Public Agency and Private Service Provider Comments

Notice of the proposal was provided to public agencies and to public & private service providers. As of the date of completion of this staff report no comments have been received.

## 6. Neighborhood Association and Public Comments

The subject property is located within the boundaries of the Morningside Neighborhood Association.

Open House / Neighborhood Association Meeting. SRC 300.320 requires an applicant to hold an open house or, as an alternative, attend a regularly scheduled neighborhood association meeting in-lieu of hosting an open house prior to submitting an application for certain types of land use applications. Pursuant to SRC 300.320(b)(1), land use applications included in this proposed consolidated land use application request require an open house/neighborhood association meeting.

On January 13, 2021, the applicant attended a regularly scheduled meeting of the Morningside Neighborhood Association to present the proposed development; thereby satisfying the requirements of SRC 300.320(f).

### Neighborhood Association Comments

Notice of the application was provided to the neighborhood association pursuant to SRC 300.620(b)(2)(B)(v), which requires notice to be sent to any City-recognized neighborhood association whose boundaries include, or are adjacent to, the subject property. As of the date of completion of this staff report no comments have been received from the neighborhood association.

### Public Comments

In addition to providing notice to the neighborhood association, notice was also provided, pursuant to SRC 300.620(b)(2)(B)(iii), (vi), & (vii), to all property owners and tenants within 250 feet of the subject property. As of the date of completion of this staff report no comments have been received from surrounding property owners or interested individuals.

### Homeowners Association

The subject property is not located within a Homeowners Association.

## 7. CLASS 3 SITE PLAN REVIEW APPROVAL CRITERIA

Site plan review is required for any development that requires a building permit, unless the development is identified as being exempt from site plan review under SRC 220.005(a)(2). Class 3 Site Plan Review is required for development proposals that involve a land use decision or limited land use decision as defined under ORS 197.015. Because the proposed development involves a Fairview Refinement Plan Major

Amendment, Class 1 Adjustment, Class 2 Adjustment, and Class 2 Driveway Approach Permit, the proposed site plan review must be processed as a Class 3 Site Plan Review.

Salem Revised Code (SRC) 220.005(f)(3) sets forth the following criteria that must be met before approval can be granted to an application for Class 3 Site Plan Review. The following subsections are organized with approval criteria shown in ***bold italic***, followed by findings evaluating the proposed development's conformance with the criteria. Lack of compliance with the following criteria is grounds for denial of the Class 3 Site Plan Review application, or for the issuance of certain conditions to ensure the criteria are met.

***SRC 220.005(f)(3)(A): The application meets all applicable standards of the UDC.***

**Finding:** The proposed development is a 183-unit multiple-family development.

The subject property is designated "Mixed-Use" on the Salem Area Comprehensive Plan Map and zoned FMU (Fairview Mixed-Use). Pursuant to SRC 530.015, development within the FMU zone is required to be undertaken pursuant to the Fairview plan and subsequent refinement plans. The Fairview Plan is the master plan that identifies the overall goals and policies for development of the Fairview site and refinement plans are detailed regulatory plans that implement the Fairview Plan.

Pursuant to SRC 530.030, standards and processes stipulated in an approved refinement plan supersede the standards and processes of the Unified Development Code (UDC) and shall be used as review criteria for any specific development proposal within the area covered by the approved refinement plan.

Because the subject property is located within an area of the Fairview site which has an approved refinement plan, the standards of the Fairview Refinement Plan II refinement plan supersede many of those of the Salem Revised Code (SRC) and are the standards applicable to the proposed development.

The proposed development conforms to the applicable standards of the Fairview Refinement Plan II refinement plan and the applicable standards of the Unified Development Code (UDC) as described below:

**Fairview Refinement Plan II**

Allowed uses and development standards within Fairview Refinement Plan II are differentiated based on specific overlay areas established by the FMU zone.

Pursuant to Section 3 of the refinement plan (*General Allocation and Identification of Major Proposed Land Uses*), there are three zones/overlay areas defined in the FMU zone and Fairview Plan which are present in the refinement plan. These zones/overlay areas include the MI (Mixed-Intensity), AU (Adaptive Use), and VC (Village Center) areas.

The subject properties proposed for the 183-unit multiple family development are located in each of the three zones/overlay areas. As shown on the FMU area zoning map submitted by the applicant (**Attachment G**), the approximate 4.3-acre West

development is located within the VC area, the approximate 3.36-acre northern portion of the East development is located within the AU area, and the approximate 2.61-acre southern portion of the East development is located within the MI area.

### ***Refinement Plan Section 2 (Permitted Land Uses)***

Allowed uses within the refinement plan are identified under Section 2 (Permitted Land Uses) of the refinement plan.

Pursuant to this section, the allowed uses within the refinement plan are, with a few limited exceptions, the uses identified in the Permitted Uses Table of the FMU zone, specifically Table 530-1.

The proposed development includes multiple family uses in VC, AU, and MI areas of the subject properties. As identified under Table 530-1 of the FMU zone, Multiple Family is a permitted use in all three of the areas and is therefore a permitted use on the subject properties within the refinement plan.

### ***Refinement Plan Section 7 (Table 1) (Development Standards)***

- ***Density:***

Allowed residential densities within the VC, AU, and MI areas of the refinement plan are summarized in the following table:

<b>VC, AU, &amp; MI Residential Density</b>		
<b>Overlay Area</b>	<b>Min. Density</b>	<b>Max. Density</b>
VC Area	13 du/acre	35 du/acre
AU Area	6 du/acre	35 du/acre
MI Area	7 du/acre	30 du/acre

The West development, located within the VC area, is approximately 4.3 acres in size and includes a total of 105 proposed dwelling units. The 105 units proposed results in a density of 24.4 dwelling units per acre which exceeds the minimum density of the VC area but does not exceed the maximum density.

The northern portion of the East development, located within the AU area, is approximately 3.36 acres in size and includes a total of 51 proposed dwelling units. The 51 dwelling units proposed results in a density of 15.2 dwelling units per acre which exceed the minimum density of the AU area but does not exceed the maximum density.

The southern portion of the East development, located within the VC area, is approximately 2.61 acres in size and includes a total of 27 dwelling units. The 27 dwelling units proposed results in a density of 10.3 dwelling units per acre which exceeds the minimum density of the AU area but does not exceed the maximum density.

Because the proposed number of dwelling units within each area fall within the allowed density ranges of the refinement plan, this standard is met. However, the

refinement plan further limits the number of dwelling units allowed regardless of the proposed individual densities. Under Section 3 of the refinement plan (General Allocation and Identification of Major Proposed Land Uses), it is indicated that:

*“There is no minimal amount of residential development in any area. However, residential development projects must meet the minimum and maximum density requirements for the area. The FMU zoning does not set a minimum number of residential units, but sets the maximum at 2000 units.”*

The refinement plan further states that:

*“It is anticipated that this refinement area will have units for about 280 families. Combined with those expected in the other approved refinement plans the total is about 1200. This leaves a wide range for the 14 acres of “Woods” and the 60+ acres owned by Simpson Hills that have not yet been planned.”*

Based on the above provisions of refinement plan, the maximum total number of dwelling units allowed within the Fairview Refinement II refinement plan is 280. Though the 183 units included with this proposed development fall within the allowed density ranges of the VC, AU, and MI areas, a major amendment to the refinement plan is required because the additional 183 units result in the maximum allowed 280 total units being exceeded.

Because the proposed development exceeds the total maximum number of units allowed within the refinement plan, a major amendment to the refinement plan has been requested with the application.

Analysis of the major amendment request and findings evaluating its conformance with the applicable approval criteria are included in Section 9 of this report.

- **Lot Standards:**

Lot size and dimensions requirements applicable within the VC, AU, and MI areas of the refinement plan are summarized in the following table:

VC, AU, & MI Area Lot Standards		
Lot Area	VC, AU, & MI	Min. 1,000 sq. ft.
Lot Width	AU & MI	Min. 6 ft. per dwelling unit <i>(applicable to multiple family)</i>
	VC	Min. 20 ft.
		Max. 30 ft. per dwelling unit <i>(applicable to multiple family)</i>
Lot Depth	AU & VC	Min. 40 ft.
		Max. 200 ft. <i>(applicable to mixed-use/commercial and multiple family)</i>
	MI	Min. 40 ft.
		Max. 150 ft. <i>(applicable to mixed-use/commercial and multiple family)</i>



As previously explained in this report, the proposed development consists of two separate properties. The eastern property is located in the 4000 to 4100 Blocks of Reed Road SE and consists of one lot (*Parcel 1 of Partition Plat 2019-67*) that is approximately 6.44 acres in size.

The western property, located at 2110 Strong Road SE, is currently part of a larger 25.27-acre property (Tax Lot No. 083W0200100) that received tentative approval for a subdivision in 2020 (*Subdivision Case No. SUB-FRPA20-03 – Legacy Heights subdivision*). When the final plat for the approved subdivision is recorded the subject 4.3-acre western property will become Lot 78 of the Legacy Heights subdivision plat. In order to ensure that the West development will be located on legally established unit of land, the following condition of approval is recommended:

**Condition 1:** Prior to issuance of building buildings permits for the proposed West development, the final plat for the Legacy Heights Subdivision (Case No. SUB-FRPA20-03) shall be recorded.

Both the western property and the eastern properties exceed the minimum lot area and width requirements of the refinement plan. The western property also conforms to the minimum and maximum lot depth requirement of the refinement plan. The eastern property, however, does not conform to the maximum allowed 200-foot lot depth for multiple family uses in the AU area or the maximum allowed 150-foot lot depth for multiple family uses in the MI area. Because the eastern property does not meet maximum lot depth standards for the proposed use, a major amendment to this refinement plan standard has been requested with the application.

Analysis of the major amendment request and findings evaluating its conformance with the applicable approval criteria are included in Section 9 of this report.

▪ **Lot Coverage**

Lot coverage requirements applicable within the AU and VC areas of the refinement plan are summarized in the following table:

VC, AU, & MI Area Lot Coverage Standards		
Building Coverage	VC, AU, & MI	Max. 70%
Building Footprint	VC, AU, & MI	Max. 10,000 sq. ft.
		Max. 1,000 sq. ft. ( <i>applicable to accessory structures</i> )
Floor-Area-Ratio	VC	Min. 0.75

Building Coverage. The proposed development conforms to the maximum building coverage standard of the refinement plan. The West development is approximately 187,308 square feet in size and covered by approximately 42,013 square feet of building area. The resulting 22.4 percent lot coverage falls below the maximum coverage requirement.

The East development is approximately 259,980 square feet in size and covered by approximately 29,716 square feet of building area. The resulting 11.4 percent lot coverage for the East development also falls below the maximum lot coverage requirement.

**Building Footprint.** The proposed development conforms to the maximum building footprint requirements for building and accessory structures established under the refinement plan. None of the building footprints of the apartment buildings within the development exceed 10,000 square feet in size and the maintenance building within the West development does not exceed 1,000 square feet in size.

In the public hearing notice provided for the proposed development, a major amendment was identified as being included with the application to allow several of the carports included within the development to exceed the maximum allowed 1,000 square-foot accessory structure footprint. However, subsequent to the public hearing notice being provided, the applicant submitted the revised plans which are attached to this report that show the proposed carports with reduced footprints that now conform to the maximum 1,000 square-foot accessory structure footprint standard. As such, a major amendment to this standard is no longer needed and all buildings and accessory structures within the development conform to this standard.

**Floor-Area-Ratio.** Within the VC area of the refinement a minimum floor-area-ratio (FAR) of 0.75 is established. Because the West development is located within the VC area, the minimum 0.75 FAR applies to the buildings within that portion of the proposed development. The West development includes a total of nine buildings with a total floor area of approximately 99,283 square feet. The resulting FAR of 0.53 falls below the minimum 0.75 FAR required within the VC area. Because the proposed West development does not meet the minimum required FAR, a major amendment to this refinement plan standard has been requested with the application.

Analysis of the major amendment request and findings evaluating its conformance with the applicable approval criteria are included in Section 9 of this report.

▪ **Setbacks:**

The setbacks for buildings, accessory structures, and parking areas within the VC, AU, and MI areas of the refinement plan are summarized in the table below:

Because multiple family development within the refinement plan is also subject to multiple family design review, the proposed development must also conform to the additional building, accessory structure, and parking area setbacks established under SRC Chapter 702 (Multiple Family Design Guidelines and Standards). Analysis of the proposed development for conformance with the applicable multiple family design standards is included under Section 8 of this staff report.

<b>Summary of Setbacks</b>		
<b>Building &amp; Accessory Structures</b>		
FMU Zone Boundary Setback	VC, AU, & MI	Min. 20 ft. <i>(applicable to all buildings and accessory structures)</i>
Strong & Lindburg Roads	VC	Min. 10 ft. / Max. 20 ft.
		Min. 70% of lot frontage shall be occupied by buildings placed at the minimum setback line.
Front Abutting Street	VC, AU, & MI	Min. 10 ft. / Max. 20 ft.

Side Street	VC, AU, & MI	Min. 10 ft. / Max. 20 ft.
Side Interior	AU & MI	Min. 8 ft. <i>(applicable to multiple family abutting single family)</i>
	VC	Min. 0 ft. <i>(applicable to all other multiple family)</i>
Rear Principal Building	AU & MI	Min. 30 ft.
	VC	Min. 5 ft.
Rear Other Buildings	VC, AU, & MI	Min. 2 ft. <i>(applicable to residential accessory buildings)</i>
<b>Parking Areas</b>		
Front/Street Setback	VC, AU, & MI	Min. 20 ft.
Side Setback Adjacent to Residential	VC, AU, & MI	Min. 10 ft.
Side Setback Adjacent to Non-Residential	VC, AU, & MI	Min. 5 ft.

As shown on the site plan, the proposed development conforms to the minimum required setbacks within the refinement plan with the exception of the following buildings which do not meet setback requirements:

<b>Buildings Not Meeting Setback Requirements</b>		
	<b>Min. Setback Abutting Street Not Met</b>	<b>Max. Setback Abutting Street Not Met</b>
West Development	N/A	Buildings 27, 28, 29, 30, 31, 32, 33, 34, & 35
East Development	Building 24	Buildings 20, 23, 24, 25, & 26

In addition to the above identified buildings that do not meet minimum setback requirements, two off-street parking areas included with the proposed development do not meet minimum setback requirements. The first is the parking area within the West development located along the southern property line where a minimum 20-foot setback is required but only a 16-foot setback is provided. The second is the small stand-alone parking area within the East development located north of Building 23 where a minimum 10-foot setback is required but only a 7.5-foot setback is provided.

Because not all of the proposed buildings and off-street parking areas within the development conform to setback requirements, a major amendment to the refinement plan has been requested with the application. Analysis of the major amendment request and findings evaluating its conformance with the applicable approval criteria are included in Section 9 of this report.

Building Frontage. In addition to building setback requirements, the refinement plan also establishes a minimum building frontage standard within the VC area requiring

a minimum of 70 percent of the lot/street frontage of lots fronting on Strong Road and Lindburg Road to be occupied by buildings placed at the minimum setback line. As indicated in the applicant’s written statement and shown on the proposed site plan, the street frontages of the West development which front on Strong Road and Lindburg Road do not conform to the minimum 70 percent building frontage standard. A major amendment to this standard has therefore been requested with the application. Analysis of the major amendment request and findings evaluating its conformance with the applicable approval criteria are included in Section 9 of this report.

▪ **Building Dimensions:**

Building dimension requirements applicable within the VC, AU, and MI areas of the refinement plan are summarized in the following table:

Building Dimension Standards		
Height	VC, AU, & MI	Max. 45 ft. <i>(applicable to buildings)</i>
		Max. 18 ft. <i>(applicable to accessory structures)</i>
Exterior Wall Length	VC, AU, & MI	Max. 100 ft. without 4 ft. change of plane <i>(applicable to mixed-use/commercial and multiple family)</i>

Height. As illustrated by the elevation drawings for the proposed apartment buildings and the maintenance building (**Attachment D**), the heights of these buildings fall below the maximum 45-foot and 18-foot height limits established in the refinement plan.

Exterior Wall Length. As shown on the proposed site plans (**Attachment C**), the buildings within the development are designed to include building off-sets four feet or greater in depth. These building off-sets ensure that exterior building wall length does not exceed 100 feet without a change of plane as required under this refinement plan standard.

▪ **Parking & Loading:**

Parking and loading requirements applicable within the VC, AU and MI areas of the refinement plan are summarized in the following table:

Parking & Loading Standards		
Parking Stalls	VC, AU, & MI	Min. 1 space per dwelling unit <i>(applicable to residential uses)</i>
Eligible On-Street Parking	VC, AU, & MI	50% of required parking may be located on street within 100 feet of the lot it serves <i>(applicable to multiple family)</i>
Surface Parking Coverage	VC, AU, & MI	Max. 40% of parcel
Bicycle Parking	VC, AU, & MI	Min. 1 space per dwelling unit <i>(applicable to residential multi-family and mixed-use)</i>

Parking Stalls & Eligible On-Street Parking. As identified in the above table, the refinement plan requires a minimum of one parking space per dwelling unit and a maximum of 50 percent of the required parking may be located on-street within 100 feet of the lot it serves. The refinement plan also specifies that parking spaces provided within the development are required to meet the minimum stall dimensions established under the SRC.

In addition to the minimum off-street parking requirements included in the refinement plan, the City's off-street parking, loading, and driveways chapter (SRC Chapter 806) establishes a maximum limit on the number of parking space that may be provided based on the minimum number of spaces required. Pursuant to SRC Chapter 806, Table 806-2, If the minimum number of spaces required equals more than 20 spaces, the maximum allowed parking is 1.75 times the minimum number of spaces required.

Based on the above identified minimum and maximum off-street parking requirements, the proposed 183 unit multiple family development requires the following vehicle parking:

<b>Vehicle Parking Summary</b>				
<b>Use</b>	<b>Dwelling Units</b>	<b>Minimum Spaces Req.</b>	<b>Maximum Spaces</b>	<b>Spaces Provided</b>
West Development	105	105	184	<b>163 (Off-Street)</b>
				<b>10 (On-Street)</b>
<b>Total:</b>				<b>173</b>
East Development	78	78	137	<b>106 (Off-Street)</b>
				<b>33 (On-Street)</b>
<b>Total:</b>				<b>139</b>

As shown on the site plans and summarized in the above table, the number of parking spaces provided for the West development conform to the minimum and maximum parking requirements and the number of on-street parking spaces do not exceed 50 percent of the minimum required spaces.

The East development, however, exceeds the maximum allowed number of parking spaces by two, but the number of on-street spaces does not exceed the allowed maximum. Because maximum off-street parking is exceeded for the East development, a Class 1 Adjustment to this standard has been requested with the application. Analysis of the Class 1 Adjustment and findings evaluating its conformance with the applicable approval criteria are included in Section 10 of this report.

Off-Street Parking Area Dimensions. SRC 806.035(e), Table 806-6, establishes minimum dimension requirements for off-street parking stalls and the drive aisles serving them. Based on the layout of the parking spaces within the development, the proposed parking stalls and access aisles must meet the following standards:

<b>Minimum Parking Stall &amp; Drive Aisle Dimensions</b>		
<b>Stall Type</b>	<b>Parking Stall Dimension</b>	<b>Drive Aisle Width <sup>(1)</sup></b>
90° Standard Stall	9 ft. x 19 ft.	24 ft.
90° Compact Stall <sup>(2)</sup>	8 ft. x 15 ft.	22 ft.
	8 ft.- 6 in. x 15 ft.	
<b>Notes</b>		
<p>(1) <u>Drive Aisle Width Serving Standard and Compact Stalls:</u> Pursuant to SRC 806, Table 806-6, when a parking lot drive aisle serves both standard and compact size parking stalls of 80 degrees or more, the drive aisle shall be a minimum of 24 feet.</p> <p>(2) <u>Compact Stall Dimension Next to Wall or Post:</u> Pursuant to SRC 806, Table 806-6, compact sized parking stalls next to a wall or post must be a minimum of 8-foot 6-inches in width.</p>		

As shown on the site plans for the proposed development, both standard size and compact size parking stalls are provided. The standard size stalls conform to the minimum required 9-foot width and 19-foot depth and the compact size stalls conform to the minimum required 8-foot width and 15-foot depth.

The parking stalls within the development are served by 24-foot-wide and 26-foot-wide drive aisles in conformance with the requirements of SRC 806.035(e), Table 806-6.

Surface Parking Coverage. The proposed development conforms to the maximum surface parking coverage standard of the refinement plan. The West development is approximately 187,308 square feet in size and covered by approximately 61,716 square feet of parking and driveway area. The resulting 32.9 percent surface parking coverage falls below the maximum 40 percent coverage requirement.

The East development is approximately 259,980 square feet in size and covered by approximately 53,939 square feet of parking and driveway area. The resulting 20.7 percent surface parking coverage for the East development also falls below the maximum coverage requirement.

Bicycle Parking. Bicycle parking for the proposed development is required to be provided in the minimum amount of one space per dwelling unit. Based on the 183 dwelling units proposed within the development, a minimum of 183 bike parking spaces are required.

As shown on the proposed site plans, a total of 106 bike parking spaces are provided for the 105 dwelling units located within the West development and a total of 78 bike parking spaces are provided for the 78 dwelling units located within the East development. The bike spaces are distributed throughout the site in proximity to the proposed buildings. This standard is met.

Loading: Minimum loading requirements are established under SRC Chapter 806, Table 806-9. The minimum loading requirement for Multiple Family is as follows:

Minimum Loading		
Multiple Family	50 to 99 dwelling units	1 space (12'W x 19'L x 12'H)
	100 to 199 dwelling units	2 spaces (12'W x 19'L x 12'H)
<b>Note:</b> If a recreational or service building is provided, at least one of the required loading spaces shall be located in conjunction with the recreational or service building.		

The 105 dwelling units proposed within the West development result in a minimum loading requirement of two spaces. As shown on the site plan, the West development includes two loading areas in conformance with this standard, one of which is located in conjunction with the proposed maintenance building.

The 78 dwelling units proposed within the East development result in a minimum loading requirement of 1 space. As shown on the site plan, the East development includes one loading area in conformance with this standard.

The proposed development meets the loading requirements of SRC Chapter 806.

▪ **Driveway/Curb Cuts:**

Driveway and curb cut requirements applicable within the VC, AU, and MI areas of the refinement plan are summarized in the following table. The driveway and curb cut standards identified in the refinement plan apply only to private streets. Driveways and curb cuts on public streets are subject to the applicable provisions of the SRC.

Driveway & Curb Cut Standards		
Maximum Per Parcel	VC, AU, & MI	Max. 2
Access	VC, AU, & MI	Only from lesser class right-of-way (ROW)/Easement
Width (Driveway serving 15+ stalls)	AU & MI	Min. 12 ft. / Max. 16 ft.
	VC	Min. 10 ft. / Max. 18 ft.
Width (Driveway serving 4-14 stalls)	VC, AU, & MI	Min. 10 ft. / Max. 14 ft.
Width (Driveway serving 1-4 stalls)	VC, AU, & MI	Min. 12 ft.
Curb Cuts	VC, AU, & MI	Max. 2 per parcel

The West development includes a total of two curb cuts/driveway approaches onto a private street (Village Center Loop) and two driveway approaches onto public streets – one for the connection of Village Center Loop onto Lindburg Road and the second for the connection of Village Center Loop onto Strong Road.

The East development includes a total of three driveway approaches onto private streets – two onto Mountain Street and the third onto Chapel Drive. No new driveway approaches from the East development onto public streets are proposed.

Pursuant to the requirements of the refinement plan, the two private street driveway approaches onto Lindburg Road and Strong Road within the West development are subject to the requirements of the SRC and therefore must be developed in conformance with the requirements of SRC Chapter 804. Analysis of the conformance of this driveway approach with the applicable standards of the SRC Chapter 804 is included in Section 12 of this report.

The five proposed driveway approaches onto private streets are subject to the above identified requirements of the refinement plan. Pursuant to the refinement plan requirements, the number of driveway approaches are limited to a maximum of two per parcel, the maximum widths of driveway approaches are limited based on the number of parking stalls served, and access is required to be taken from the street with the lower classification.

The two driveway approaches onto the private street within the West development conform to the standards of the refinement plan with the exception of their proposed widths, which exceed maximum width requirements.

The three driveway approaches onto the private streets within the East development exceed the maximum allowed two driveway approaches per parcel. The widths of the driveways also exceed maximum width requirements.

Because not all of the driveways within the development conform to the standards of the refinement plan, a major amendment to the refinement plan standards has been requested with the application. Analysis of the major amendment request and findings evaluating its conformance with the applicable approval criteria are included in Section 9 of this report.

### **SRC 530.065 (NATURAL RESOURCE GUIDELINES)**

The FMU Zone recognizes the importance of the presence of natural resources on the site and how those resources help to define the special character of the property. As such, SRC Chapter 530.065 establishes natural resource guidelines to address their preservation. The Fairview Plan, any subsequent refinement plan, and any development within the FMU Zone shall identify how existing natural resources will be protected and how natural hazards will be mitigated through compliance with the following:

- (1) SRC Chapter 808 (Preservation of Trees and Vegetation)
- (2) SRC Chapter 809 (Wetlands)
- (3) SRC Chapter 810 (Landslide Hazards)

### **SRC CHAPTER 808 (PRESERVATION OF TREES & VEGETATION)**

Pursuant to the FMU zone, specifically SRC 530.065, and the provisions of the Fairview Refinement Plan II refinement plan included under Section 10 (Tree Inventory and Preservation Plan), compliance with the City's tree ordinance is the means by which tree preservation and removal is regulated within the refinement plan.

The City's tree preservation ordinance (SRC Chapter 808) protects Heritage Trees, Significant Trees (*including Oregon White Oaks with diameter-at-breast-height of 24*



*inches or greater*), trees and native vegetation in riparian corridors, and trees on lots and parcels greater than 20,000 square feet. The tree preservation ordinance defines “tree” as, “any living woody plant that grows to 15 feet or more in height, typically with one main stem called a trunk, which is 10 inches or more dbh, and possesses an upright arrangement of branches and leaves.”

Under the City’s tree preservation ordinance, pursuant to SRC 808.035(a), tree conservation plans are required in conjunction with development proposals involving the creation of lots or parcels to be used for the construction of single family or duplex dwelling units, if the development proposal will result in the removal of trees.

Tree conservation plans are required to preserve all heritage trees, significant trees, trees and native vegetation within riparian corridors, and a minimum of 25 percent of the remaining trees on the property. If less than 25 percent of the existing trees on the property are proposed for preservation, the applicant must show that only those trees reasonably necessary to accommodate the development are designated for removal. If significant trees and trees within a riparian corridor are proposed for removal, the applicant must show that there are no reasonable design alternatives to enable preservation of those trees.

Because the proposal is for a multiple family development, a tree conservation plan is not required, but the development must maintain compliance with the provisions of SRC Chapter 808.

As shown on the tree conservation map provided by the applicant (**Attachment H**), a total of 61 trees of 10 inches or more dbh are located on the property. Of the 61 trees, 41 are proposed for preservation and 20 are proposed for removal. Of the 41 trees identified for preservation, 14 are significant Oregon white oaks. Of the 20 trees proposed for removal, four are significant Oregon white oaks that, along with nine other trees, are necessary to be removed based on their proximity to Reed Road and the associated improvements required with the widening of the street.

The trees proposed for removal are consistent with the trees proposed for removal as shown on Plate 8 (Tree Inventory and Preservation Plan) of the refinement plan and the number of trees preserved meets the requirements of SRC Chapter 808.

In addition to preserving existing trees, new trees are required to be planted within the proposed development as provided under the refinement plan. As shown on the proposed landscaping plans (**Attachment C**), new trees will be planted throughout the development within the common open space areas, adjacent to buildings, and along the perimeter of proposed parking areas.

#### **SRC CHAPTER 809 (WETLANDS):**

According to the Salem-Keizer Local Wetland Inventory (LWI), the Natural Resources Inventory included as part of the Fairview Plan, and as explained in Section 9 (Wetland and Riparian Resources) of the refinement plan, there are mapped wetland areas within the refinement plan area. As shown on the proposed site plan there are wetland areas located within the East development on the portion of the site within the MI area north of Lindburg Road and south of Chapel Drive. There is also an additional wetland

area located within the East development on the portion of the site within the AU area north of Chapel Drive near Reed Road. As shown on the site plan, these wetland areas are proposed to remain undeveloped within the common open space areas within the development.

Removal-fill activities within waters of the state are regulated by the Oregon Department of State Lands (DSL) and the U.S. Army Corps of Engineers. Due to the presence of these wetland areas within the development, notice of the proposal was provided to DSL. In order to ensure that development of the property is conducted in compliance with the requirements of SRC Chapter 809 and the DSL, the following condition of approval is recommended:

**Condition 2:** Obtain all state and federal permits as indicated by the Department of State Lands (DSL).

### **SRC CHAPTER 810 (LANDSLIDE HAZARDS)**

The City's landslide hazard ordinance (SRC Chapter 810) establishes standards and requirements for the development of land within areas of identified landslide hazard susceptibility.

According to the City's adopted landslide hazard susceptibility maps, portions of the West development include areas of two to three mapped landslide hazard susceptibility points. Based on the requirements of SRC Chapter 810, the cumulative total of 4 to 5 points between those associated with the land and those associated with the proposed development activity indicates a moderate landslide susceptibility risk. As such a geologic assessment is required in conjunction with the proposed development.

A geotechnical investigation, prepared by Multi-Tech Engineering Services, Inc. dated November 19, 2020, was submitted with the application. The assessment demonstrates the subject property can be developed with the recommendations contained in the report.

### **SRC 800.055 (SOLID WASTE SERVICE AREAS)**

SRC 800.055 establishes design standards that apply to all new solid waste, recycling, and compostable service areas, where use of a solid waste, recycling, and compostable receptacle of 1 cubic yard or larger is proposed.

A solid waste service area is defined under SRC 800.010(e) as, "An area designed and established for the purpose of satisfying the local collection franchisee service requirements for servicing receptacles, drop boxes, and compactors singularly or collectively."

As shown on the site plans for the proposed development, three trash/recycling areas accommodating trash receptacles of 4 cubic yards in size are included within the West development and two trash/recycling areas accommodating receptacles of 4 cubic yards in size are included within the East development.

The trash/recycling areas are enclosed by a 6.5-foot-tall CMU block wall, have an interior dimension within the enclosure of approximately 16 feet in width by 12 feet in depth, and are free of vertical obstructions above the receptacles.

SRC 800.055(f)(1)(A) requires a vehicle operation area to serve each proposed trash/recycling area within a development that is free of obstructions and no less than 45 feet in length and 15 feet in width. For trash receptacles greater than two cubic yards in size, the vehicle operation area must be located perpendicular to the front of the enclosure.

In the public hearing notice provided for the proposed development, a Class 2 Adjustment was identified as being included with the application to allow the proposed trash/recycling areas within the development to be located without on-site turnarounds areas for the waste collection vehicles. However, subsequent to the public hearing notice being provided, the applicant submitted revised plans which are attached to this report that modified the location and positioning of the proposed trash/recycling areas. As such, the Class 2 Adjustment for the turnaround area is no longer needed, but two of the revised locations for the trash/recycling areas within the West development (*one adjacent to Building 28 and the other adjacent to Building 31*) have physical obstructions within the required 15-foot by 45-foot vehicle operation area and appear to be at an angle that will be difficult for the trucks to access for servicing; and one of the revised locations for the trash/recycling areas within the East development (the one located closest to Building 20) is located in a location where it does not appear possible for a truck to have direct front access to the enclosure.

In order to ensure the proposed trash/recycling areas within the development conform to the applicable standards of SRC 800.055, the following condition of approval is recommended:

**Condition 3:** All trash/recycling areas shall conform to the solid waste service area standards of SRC 800.055.

***SRC 220.005(f)(3)(B): The transportation system provides for the safe, orderly, and efficient circulation of traffic into and out of the proposed development, and negative impacts to the transportation system are mitigated adequately;***

**Finding:** The subject properties are located adjacent to three existing public streets, Strong Road SE, Lindburg Road SE, and Reed Road SE; and one existing private street, Chapel Drive SE. The proposed development will also include two additional private streets, Village Center Loop SE within the West development and Mountain Street SE within the East Development.

In order to ensure that the existing and proposed streets serving the development will meet the applicable requirements of the SRC, the Public Works Design Standards (PWDS), and the refinement plan, the following conditions of approval are recommended:

**Condition 4:** Design and construct all City infrastructure, including streets, according to the refinement plan and PWDS.

**Condition 5:** Convey land for dedication to equal a half-width right-of-way of 36 feet along the entire frontage of the subject property measured from the newly established offset centerline of Reed Road SE, and as shown on public construction plans under permit number 21-101403-PC.

- Condition 6:** Construct a full-street improvement along the frontage of Reed Road SE to Minor Arterial street standards from Strong Road SE to approximately 500 feet south of Lindberg Road SE, and as shown on public construction plans under permit number 21-101403-PC.
- Condition 7:** Complete incomplete portions of the boundary street improvements along the property frontages of Strong Road SE and Lindberg Road SE.
- Condition 8:** Construct Village Center Loop SE (private) from Lindburg Road SE to Strong Road SE. The private/public street intersections shall be constructed pursuant to PWDS.
- Condition 9:** Construct an extension of Mountain Street SE (private) from its existing terminus at the northern boundary of the eastern property to Chapel Drive SE (private).
- Condition 10:** Construct incomplete portions of the boundary street improvements along the property frontages of Chapel Drive SE (private).
- Condition 11:** Dedicate public access easements for the proposed private streets within the development.

The development, as recommended to be conditioned, provides for the safe, orderly, and efficient circulation of traffic into and out of the proposed development. This criterion is met.

***SRC 220.005(f)(3)(C): Parking areas and driveways are designed to facilitate safe and efficient movement of vehicles, bicycles, and pedestrians; and***

**Finding:** Vehicular access to the proposed development is provided via five driveway approaches onto the various streets which serve the development.

Pedestrian access to and throughout the interior of the development is provided by a separate network of pedestrian pathways/sidewalks distributed throughout both sites. The proposed pedestrian pathways/sidewalks are raised above the surface of vehicle travel lanes and striped crosswalks are provided where pedestrian pathways/sidewalks must cross driveways. These features provide a clear separation between vehicles and pedestrians and ensure that the proposed parking areas within the development minimize vehicle and pedestrian conflicts throughout the site.

In addition to the proposed pathways/sidewalks throughout both sites, a 10-foot-wide multi-use path is proposed to be provided within the East Development to provide additional bicycle and pedestrian connectivity through the site between Lindburg Road and Chapel Drive. Provision of this multi-use path satisfies bicycle and pedestrian connectivity requirements of the refinement by providing an alternative north-south connection through the Oak Block where a street connection between Lindburg Road and Chapel Drive is not otherwise possible due to the existing wetland area on the site.

In order to ensure public pedestrian access through the site to meet connectivity requirements, the following condition of approval is recommended:

**Condition 12:** Dedicate a public access easement for the proposed 10-foot-wide multi-use path through the subject property.

The development, as recommended to be conditioned, ensures safe and efficient movement of vehicles, bicycles, and pedestrians. This criterion is met.

***SRC 220.005(f)(3)(D): The proposed development will be adequately served with City water, sewer, stormwater facilities, and other utilities appropriate to the nature of the development.***

**Finding:** The Public Works Department has reviewed the applicant's preliminary plan for this site. Water, sewer, and storm infrastructure are available within surrounding streets/areas and are adequate to serve the proposed development. In order to serve upstream parcels, the applicant shall construct an 18-inch S-1 water main in Reed Road SE from Strong Road SE to approximately 500 feet south of Lindberg Road SE.

The applicant's engineer submitted a statement demonstrating compliance with Stormwater PWDS Appendix 004-E(4)(b) and SRC Chapter 71. The preliminary stormwater design demonstrates the use of green stormwater infrastructure to the maximum extent feasible. At the time of development, the applicant's engineer shall design and construct a storm drainage system at the time of development in compliance with SRC Chapter 71 and PWDS.

The applicant shall design and construct all utilities (sewer, water, and storm drainage) according to the PWDS and to the satisfaction of the Public Works Director.

In order to ensure the proposed development will be adequately served with City water, sewer, stormwater facilities, and other utilities appropriate to the nature of the development, the following conditions of approval are recommended:

**Condition 13:** Construct an 18-inch S-1 water main within Reed Road SE from Strong Road SE to approximately 500 feet south of Lindberg Road SE.

**Condition 14:** Design and construct a storm drainage system at the time of development in compliance with Salem Revised Code (SRC) Chapter 71 and Public Works Design Standards (PWDS).

## **8. CLASS 1 DESIGN REVIEW APPROVAL CRITERIA**

Salem Revised Code (SRC) 225.005(e)(1) sets forth the criteria that must be met before approval can be granted to an application for Class 1 Design Review. Pursuant to SRC 225.005(e)(1) an application for a Class 1 Design Review shall be approved if all of the applicable design review standards are met.

Pursuant to SRC 530.015, because the subject property is located within the Fairview Mixed-Use (FMU) zone, development of the property is subject to the standards included within an approved refinement plan. The approved refinement plan for this area of the Fairview site is the *Fairview Refinement Plan II*.

Under Section 7, Table 2 (Fairview Alternative Multi-Family Design Standards), of the refinement plan it is explained that:

*“Except where such standards/guidelines are specifically modified by the Fairview Alternative Multi-Family Design Standards outlined below, the City of Salem will review plans for multifamily buildings (projects > 5 units) for conformance with the City’s multiple family design review guidelines/standards of SRC Chapter 702.”*

Because the proposed development includes more than five dwelling units it is subject to both the alternative multiple family design standards identified in the refinement plan as well as the multiple family design review standards of SRC Chapter 702.

The following subsections are organized with the multiple family design review standard shown in ***bold italic***, followed by findings evaluating the proposal for conformance with the design review standard. Lack of compliance with the following design review standards is grounds for denial of the Class 1 Design Review application, or for the issuance of certain conditions to ensure the multiple family design review standards are met.

**A. Open Space Standards (Per Fairview Alternative Multi-Family Design Standards – Section 7 / Table 2 (page 24 of refinement plan)):**

**(1) Common Open Space: None required in VC or within ~600 ft. of Public Open Space.**

**Finding:** Though the West development is located within the VC Area of the refinement plan and the northern portion of the East development is located with 600 feet of Fairview community park, the proposed development still provides open space exceeding that which would otherwise be required if the property were not subject to the reduced open space requirements of the refinement plan due to the location of the two properties.

As shown on the open space plan (**Attachment C**), approximately 48 percent of the total site area of the West development is reserved as open space. Within the East development, approximately 76.6 percent of the portion of the site located within the MI area is reserved as open space and approximately 73.4 percent of the portion of the site within the AU area is reserved as open space.

The open space provided includes a variety of common open space areas of sufficient size distributed throughout the site for use by all residents of the development as required by the design standards. Open space is provided around and between buildings, along the perimeter of the development within required setbacks, and throughout the interior of the development with identified recreation areas as well as areas that will be preserved as undisturbed open space to preserve existing trees and wetlands within the East Development.

The subject properties are also located in proximity to Fairview Park, which will eventually, upon build-out, provide recreational and open space amenities beyond those provided within the development.

As indicated above and illustrated on the site plan and open space plan, the proposed development includes a variety of active and passive open space areas for use by residents of the development. The common open space provided within the development conforms to this design standard.

**(2) *Play and Recreation Areas: None required in VC or within ~600 ft. of Public Open Space.***

**Finding:** Though the West development is located within the VC Area of the refinement plan and the northern portion of the East development is located with 600 feet of Fairview community park, the proposed development still provides recreational areas that are otherwise encouraged for multiple family developments that are not located within the VC area of the refinement plan or within 600 of public open space.

As shown on the proposed site plan (**Attachment C**), two recreation areas are provided within the West development and one recreation area is provided within the East Development. The proposed development conforms to this design standard.

**(3) *Private Open Space:***

- ***Minimum 48 square feet per dwelling unit in VC;***
- ***Minimum 96 square feet per dwelling unit in all other areas.***

**Finding:** As indicated in the applicant's written statement and illustrated on the proposed site plans, each dwelling unit within the proposed development includes private open space exceeding both the multiple family design standards of SRC Chapter 702 and the alternative multiple family design review standards included in the refinement plan.

All proposed ground floor dwelling units have patio areas that exceed minimum size and dimension requirements. All second and third floor units have balconies/decks no less than a minimum of 48 square feet in size, with no dimension less than 6 feet. All of the private open space areas within the development are also directly accessible from the dwelling units. The private open space provided within the proposed development conforms to this design standard.

**B. *Landscaping Standards (702.020(b)):***

**(1) *To encourage the preservation of trees and maintain or increase tree canopy, a minimum of one tree shall be planted or preserved for every 2,000 square feet of gross site area.***

**Finding:** As shown on the landscape plans (**Attachment C**) for the proposed development, existing trees are proposed to be preserved and new trees are proposed to be planted in conformance with this design standard.

The site area of the West development is approximately 187,308 square feet.

Based on the minimum requirement of one tree preserved or planted for every 2,000 square feet of gross site area, the minimum number of trees required for the West development is 94 trees. As shown on the landscape plan a total of 132 trees are proposed to be planted within the West development, therefore exceeding this design standard.

The site area of the East development is approximately 259,980 square feet. Based on the minimum requirement of one tree preserved or planted for every 2,000 square feet of gross site area, the minimum number of trees required for the East development is 130 trees. As shown on the landscape plan a total of 184 trees are proposed within the East development, therefore exceed this design standard. Of the total 184 trees provided for the East development, 41 are existing trees that are proposed to be preserved.

The existing trees proposed to be preserved and the new trees proposed to be planted are distributed throughout the site around the perimeter of the proposed buildings and within the interior and around the perimeter of the proposed parking areas. The proposed development conforms to this design standard.

**(2) *Multiple family buffer abutting single family (Fairview Alternative Multi-Family Design Standard)***

- ***Buffer multiple family from single family uses with minimum 6 ft. fence and trees greater than 1.5-inch caliber every 20 ft.***

**Finding:** The East development is bordered by Reed Road to the east, Strong Road to the north, and Lindburg Road to the south. To west, the development abuts the Heritage School and the first phase of The Grove apartments. Because the East development does not abut any single family uses, this design review standard is not applicable to that portion of the proposed development.

The West development does, however, abut future potential single-family uses within the Fairview Addition West refinement plan to the west of the subject property. The potential future residential uses are located adjacent to the northwest corner of the subject property. As shown on the proposed landscape plans (**Attachment C**), a combination of trees (*Bowhall Red Maples*) planted 20 feet on center and a 6-foot sight-obscuring fence are provided to buffer the proposed development from the potential future single family uses to the west. The proposed development conforms to this design standard.

**(3) *To define and accentuate primary entryways, a minimum of two plant units, shall be provided adjacent to the primary entryway of each dwelling unit, or combination of dwelling units.***

**Finding:** As identified on the landscape plans, planting beds are shown around the perimeter of the proposed buildings, but the location of the specific



plantings to be provided within the proposed beds are not shown. Because the location of the specific plantings is not identified, staff is unable to determine whether the proposed landscaping design meets this standard. In order to ensure each primary entryway is defined and accentuated as required by this design standard, the following condition of approval is recommended:

**Condition 1:** A minimum of two plant units shall be provided adjacent to the primary entryway of each dwelling unit or combination of dwelling units.

The proposed development, as recommended to be conditioned, conforms to this design standard.

- (4) *To soften the visual impact of buildings and create residential character, new trees shall be planted, or existing trees shall be preserved, at a minimum density of ten plant units per 60 linear feet of exterior building wall. Such trees shall be located not more than 25 feet from the edge of the building footprint.***

**Finding:** As shown on the proposed landscape plans, trees are provided around the perimeter of existing buildings, but not all of the proposed buildings include the minimum number of required trees.

A summary of the buildings that do and do not currently meet the perimeter tree planting requirement is provided in the following table:

<b>Building Perimeter Tree Planting</b>		
	<b>Requirement Met</b>	<b>Requirement Not Met</b>
West Development	Buildings 29, 33, 34, & 35	Buildings 27, 28, 30, 31, & 32
East Development	Buildings 21, 23, 24, 25, & 26	Buildings 20 & 22

Because the proposed development does not meet the minimum tree planting requirement around the perimeter of all buildings, a Class 2 Adjustment to this design standard has been requested as part of the application.

Analysis of the Class 2 Adjustment request and findings evaluating its conformance with the Class 2 Adjustment approval criteria are included in Section 11 of this report.

- (5) *Shrubs shall be distributed around the perimeter of buildings at a minimum density of one plant unit per 15 linear feet of exterior building wall.***

**Finding:** As identified on the landscape plans, planting beds are shown around the perimeter of the proposed buildings, but the location of the specific plantings to be provided within the proposed beds are not shown. Because

the location of the specific plantings is not identified, staff is unable to determine whether the proposed landscaping design meets this standard. In order to ensure that shrubs are distributed around the perimeter of the proposed buildings as required by this design standard, the following condition of approval is recommended:

**Condition 2:** Shrubs shall be distributed around the perimeter of buildings at a minimum density of one plant unit per 15 linear feet of exterior building wall.

The proposed development, as recommended to be conditioned, conforms to this design standard.

**(6) *To ensure the privacy of dwelling units, ground level private open space shall be physically and visually separated from common open space with perimeter landscaping or perimeter fencing.***

**Finding:** All of the proposed buildings within the development include ground floor private open space which abuts common open space. As such, the proposed ground floor patios are required to include a buffer to separate them from the common open space. The applicant's written statement indicates that all ground level private open space areas will be screened with landscaping, but the landscaping plans do not show the location of the proposed vegetative screening.

In order to ensure that all of the proposed ground floor private open space areas within the development are buffered from the common open space areas as required by this design standard, the following condition of approval is recommended:

**Condition 3:** The private ground floor patios included within the development shall be visually separated from common open space through the use of perimeter landscaping.

The proposed development, as recommended to be conditioned, conforms to this design standard.

**(7) *Parking are perimeter landscaping and canopy tree planter bays (Fairview Alternative Multi-Family Design Standard)***

- ***Minimum one canopy tree per 30 ft. of parking area perimeter. Trees must be planted within 15 ft. of paving.***
- ***Plant canopy trees in greater than 9-foot wide planter bays.***

**Finding:** As shown on the proposed landscape plans, trees are not currently planted at the minimum required 30-foot spacing around the perimeter of all of the proposed parking areas within the development and some of the proposed parking lot planter bays are less than the minimum required 9-foot width. Because the proposed development does not meet the minimum parking area

tree planting requirements, a Class 2 Adjustment to this design standard has been requested as part of the application.

Analysis of the Class 2 Adjustment request and findings evaluating its conformance with the Class 2 Adjustment approval criteria are included in Section 11 of this report.

**(8) *Multiple family developments with 13 or more units are exempt from the landscaping requirements in SRC chapter 806.***

**Finding:** The proposed development includes more than 13 dwelling units. Therefore, the parking lot landscaping requirements of SRC Chapter 806 are not applicable to the proposed development.

**C. Site Safety and Security (702.020(c)):**

**(1) *Windows shall be provided in all habitable rooms, other than bathrooms, on each wall that faces common open space, parking areas, and pedestrian paths to encourage visual surveillance of such areas and minimize the appearance of building bulk.***

**Finding:** As indicated in the applicant's written statement and shown on the proposed building floor plans and elevation drawings (**Attachment E**), windows are included in all habitable, other than bathrooms, on each wall that faces common open space, parking areas, and pedestrian paths. The proposed development conforms to this design standard.

**(2) *Lighting shall be provided that illuminates all exterior dwelling unit entrances, parking areas, and pedestrian paths within the development to enhance visibility and resident safety.***

**Finding:** As indicated in the applicant's written statement and shown on the proposed site plans, the proposed development includes exterior lighting along sidewalks and paths, and on the exterior of the proposed buildings, to provide illumination for exterior dwelling unit entrances, parking areas, and pedestrian paths and to promote enhanced visibility and resident safety. The proposed development conforms to this design standard.

**(3) *Fences, walls, and plant materials shall not be installed between street-facing dwelling units and public or private streets in locations that obstruct the visibility of dwelling unit entrances from the street. For purposes of this standard, the term "obstructed visibility" means the entry is not in view from the street along one-half or more of the dwelling unit's frontage.***

**Finding:** As indicated in the applicant's written statement, there are no fences or plant materials located in areas within the development that obstruct visibility of dwelling units entrances from the street. The proposed development conforms to this design standard.

- (4) *Landscaping and fencing adjacent to common open space, parking areas, and dwelling unit entryways shall be limited to a maximum height of three feet to encourage visual surveillance of such areas.***

**Finding:** As indicated in the applicant's written statement, no fencing or plant materials are included within the development that obstruct visibility. The proposed development conforms to this design guideline.

**D. Parking and Site Design (702.020(d)):**

- (1) *To minimize large expanses of continuous pavement, parking areas greater than 6,700 square feet in area shall be physically and visually separated with landscaped planter bays that are a minimum of nine feet in width. Individual parking areas may be connected by an aisle or driveway (see Figure 702-3).***

**Finding:** As shown on the proposed site plans and landscape plans, the off-street parking areas provided within the development are broken up at regular intervals by landscaped planter bays. The widths of the proposed landscape planter bays vary throughout the site and most meet the minimum required width of 9 feet. There are some planter bays, however, that don't conform to the minimum required 9-foot width.

In order to ensure that all of the proposed parking lot planter bays included within the development conform to this design standard, the following condition of approval is recommended:

**Condition 4:** All parking area planter bays shall be a minimum of 9 feet in width.

The proposed development, as recommended to be conditioned, conforms to this design standard.

- (2) *To minimize the visual impact of on-site parking and to enhance the pedestrian experience, off-street surface parking areas and vehicle maneuvering areas shall be located behind or beside buildings and structures. Off-street surface parking areas and vehicle maneuvering areas shall not be located between a building or structure and a street.***

**Finding:** The proposed parking areas within the development are located behind or to the side of the proposed buildings. There are no proposed parking areas within the development that are located between a building and a street. The proposed development conforms to this design standard.

- (3) *Where a development site abuts, and is located uphill from, property zoned Residential Agriculture (RA) or Single Family Residential (RS), and the slope of the development site within 40 feet of the abutting RA or RS zoned property is 15 percent or greater, parking areas shall be set back not less than 20 feet from the property line of the abutting RA or RS zoned property to ensure parking areas are designed to consider site***

***topography and minimize visual impacts on abutting residential properties.***

**Finding:** The subject properties and all abutting properties are zoned FMU (Fairview Mixed-Use). The subject properties do not abut any properties zoned RA or RS. This design review standard is therefore not applicable to the proposed development.

- (4) *To ensure safe pedestrian access to and throughout a development site, pedestrian pathways shall be provided that connect to and between buildings, common open space, and parking areas, and that connect the development to the public sidewalks.***

**Finding:** As shown on the site plans for the proposed development, an interconnected system of sidewalks and pedestrian pathways are distributed throughout the site and along existing and proposed streets to provide convenient pedestrian access to buildings, common open space areas, parking areas, and surrounding future developments.

The proposed internal pedestrian circulation system, together with the sidewalks that will be provided along existing and proposed streets, ensure the proposed development is served by a network of pathways providing connectivity between buildings, common open space, parking areas, and surrounding uses. The proposed development conforms to this design standard.

**E. Façade and Building Design (702.020(e)):**

- (1) *To preclude long monotonous exterior walls, buildings shall have no dimension greater than 150 feet.***

**Finding:** The applicant's written statement indicates that the designs of the various buildings within the development do not have long flat walls or roof lines. The applicant explains the proposed buildings incorporate offset breaks into their front and rear facades, balconies, and dormers to break up overall building mass and add visual interest.

As shown on the proposed site plans, none of the buildings within the development have a dimension greater than 150 feet. The proposed development conforms to this design standard.

- (2) *Where a development site abuts property zoned Residential Agricultural (RA) or Single Family Residential (RS), buildings shall be setback from the abutting RA or RS zoned property as set forth in Table 702-5 to provide appropriate transitions between new buildings and structures on site and existing buildings and structures on abutting sites.***

**Finding:** The subject properties and all abutting properties are zoned FMU (Fairview Mixed-Use). The subject properties do not abut any properties zoned RA or RS. This design review standard is therefore not applicable to the proposed development.

- (3) To enhance compatibility between new buildings on site and abutting residential sites, balconies located on building facades that face RA or RS zoned properties, unless separated by a street, shall have fully sight-obscuring railings.**

**Finding:** The subject properties and all abutting properties are zoned FMU (Fairview Mixed-Use). The subject properties do not abut or face any adjacent properties zoned RA or RS. This design review standard is therefore not applicable to the proposed development.

- (4) Building Frontage (Fairview Alternative Multi-Family Design Standard)**

- **Parcels greater than 75 ft. wide build to greater than 70 percent of frontage in VC;**
- **Parcels greater than 75 ft. wide build to greater than 50 percent of frontage in AU and MI.**

**Finding:** As indicated in the applicant’s written statement and shown on the proposed site plans, the proposed development does not meet the minimum building frontage requirements adjacent to streets within the VC, AU, and MI areas. Because the proposed development does not meet minimum building frontage requirements, a Class 2 Adjustment to this design standard has been requested as part of the application.

Analysis of the Class 2 Adjustment request and findings evaluating its conformance with the Class 2 Adjustment approval criteria are included in Section 11 of this report.

- (5) To orient buildings to the street, any ground-level unit, cluster of units, interior lobbies, or portions thereof, located within 25 feet of the property line abutting a street shall have a building entrance facing that street, with direct pedestrian access to adjacent sidewalks.**

**Finding:** As shown on the site plans for the proposed development there are several buildings with ground floor units located within 25 feet of the property line abutting a street. As such, the ground level dwelling units within those buildings are required to have a building entrance facing the street with a direct pedestrian connection from the unit to the adjacent sidewalks.

The buildings within the proposed development that this design standard applies to and whether or not the buildings meet the standard is included in the following table:

<b>Buildings Located within 25 feet of Street</b>		
	<b>Requirement Met</b>	<b>Requirement Not Met</b>
West Development	Building 27	Buildings 29, 30, 34 & 35
East Development	None	Buildings 20, 21 22, 23, 24, & 26

As identified in the above table, there are several buildings in the development located within 25 feet of a property line abutting a street that do not have a building entrance facing the street and a direct pedestrian access to the adjacent sidewalk.

Because not all of the buildings within the development conform to this standard, a Class 2 Adjustment to this design standard has been requested as part of the application.

Analysis of the Class 2 Adjustment request and findings evaluating its conformance with the Class 2 Adjustment approval criteria are included in Section 11 of this report.

- (6) A porch or architecturally defined entry area shall be provided for each ground level dwelling unit. Shared porches or entry areas shall be provided to not more than four dwelling units. Individual and common entryways shall be articulated with a differentiated roof, awning, stoop, forecourt, arcade or portico.**

**Finding:** As is illustrated by the elevation drawings for the proposed buildings within the development (**Attachment D**), all buildings include architecturally defined and covered entryways in the form of a covered entry porch area. The proposed development conforms to this design standard.

- (7) Roof-mounted mechanical equipment, other than vents or ventilators, shall be screened from ground level view. Screening shall be as high as the top of the mechanical equipment, and shall be integrated with exterior building design.**

**Finding:** As shown on the elevation drawings for the proposed buildings within the development, none of the buildings include roof-mounted mechanical equipment. This design standard is therefore not applicable to the proposed development.

- (8) To reinforce the residential character of the neighborhood, flat roofs, and the roof ridges of sloping roofs, shall not exceed a horizontal length of 100 feet without providing differences in elevation of at least four feet in height. In lieu of providing differences in elevation, a cross gable or dormer that is a minimum of four feet in length may be provided. (See Figure 702-4)**

**Finding:** As shown by the building elevations for the proposed development, none of the roof lines of the buildings exceed 100 feet in length without providing a vertical roofline offset of at least four feet. By limiting the overall lengths of building roof lines within the development and providing offsets to those roof lines, the residential character of the development is reinforced. The proposed development conforms to this design standard.

- (9) To minimize the appearance of building bulk, each floor of each building's vertical face that is 80 feet in length or longer shall incorporate**

***one or more of the design elements below (see examples in Figure 702-5). Design elements shall vary from other wall surfaces by a minimum of four feet and such changes in plane shall have a minimum width of six feet.***

***(A) Offsets (recesses and extensions).***

***(B) Covered deck.***

***(C) Covered balcony.***

***(D) Cantilevered balcony, provided at least half of its depth is recessed.***

***(E) Covered entrance.***

**Finding:** As shown on the building elevation drawings (**Attachment D**), the proposed buildings within the development include building offsets, differentiated roof lines, balconies, articulated common entry areas, and utilization of different materials throughout the proposed facades, including shake siding, trim board, lap siding, and stone around the pillars, to eliminate the appearance of long monotonous exterior walls and visually break up the overall mass of the proposed buildings. The proposed development conforms to this design standard.

***(10) To visually break up the building's vertical mass, the first floor of each building, except for single-story buildings, shall be distinguished from its upper floors by at least one of the following (see examples in Figure 702-6):***

***(A) Change in materials.***

***(B) Change in color.***

***(C) Molding or other horizontally-distinguishing transition piece.***

**Finding:** As illustrated by the elevation drawings for the proposed buildings (**Attachment D**), the ground floors of the buildings are distinguished from their upper floors through utilization of different types of siding and horizontal trim board elements. The proposed development conforms to this design standard.

***(11) Side Yard Façade Setback Plane (Fairview Alternative Multi-Family Design Standard)***

- ***Vertical 28 ft. in height plus 45-degree additional setback above 28 ft. in height.***

**Finding:** The alternative multiple family design review standards of the refinement plan require those portions of buildings that are greater than 28 feet in height and located adjacent to a side yard to have an additional setback equal to 45 degrees. As shown on the building elevations for the development, the proposed buildings are greater than 28 feet in height and those located adjacent to an interior side yard do not include the required 45-degree setback plane above 28 feet in height.

Because the proposed development does not conform to this design standard, a Class 2 Adjustment to the standard has been requested as part of the application.



Analysis of the Class 2 Adjustment request and findings evaluating its conformance with the Class 2 Adjustment approval criteria are included in Section 11 of this report.

## 9. FAIRVIEW REFINEMENT PLAN MAJOR AMENDMENT APPROVAL CRITERIA

Salem Revised Code (SRC) 530.035(e)(2) sets forth the following criteria that must be met before approval can be granted to an application for a Fairview Refinement Plan Major Amendment. The following subsections are organized with approval criteria shown in ***bold italic***, followed by findings evaluating the proposed development's conformance with the criteria. Lack of compliance with the following criteria is grounds for denial of the Fairview Refinement Plan Major Amendment application, or for the issuance of certain conditions to ensure the criteria are met.

As indicated in the public hearing notice for the proposed development, a major amendment to eleven provisions/development standards of the Fairview Refinement Plan II refinement plan has been requested by the applicant in conjunction with the proposed development. The requested amendments include the following:

- a) Maximum Allowed Dwelling Units. Increase the number of dwelling units allowed within the refinement plan from 280 to 457;
- b) Maximum Lot Depth. Allow the lot for the East development to exceed the maximum lot depth requirements of the AU (Adaptive Use) and MI (Mixed-Intensity) areas of the refinement plan;
- c) Minimum Floor-Area-Ratio (FAR). Allow the minimum required floor-area-ratio (lot coverage) of the West development located within the VC (Village Center) area of the refinement plan to be less than 0.75 FAR;
- d) Maximum Accessory Structure Footprint. Allow the proposed carports within the development to exceed the maximum 1,000 square-foot building footprint required in the VC, AU, and MI areas.
- e) Minimum Building Frontage. Allow less than 70 percent of the lot/street frontage of the West development within the VC area of the refinement plan along Lindburg Road and Strong Road to be occupied by buildings placed at the minimum setback line;
- f) Maximum Building Setback Abutting Street. Allow Buildings 20, 23, 25, 26, 27, 28, 29, 30, 31, 32, 33, and 35 to be setback greater than the maximum 20-foot setback abutting a street required in the VC, AU, and MI areas of the refinement plan;
- g) Minimum Building Setback Abutting Street. Allow Buildings 22 and 24 to be setback less than the minimum required 10-foot setback abutting a street required in the AU and MI areas of the refinement plan;
- h) Minimum Parking Setback Abutting Street. Allow the proposed off-street parking spaces on the south side of Chapel Drive within the East development and the proposed off-street parking spaces along the southern property line of the West development to be setback less than the minimum 20-foot setback abutting a street required in the VC and MI areas of the refinement plan;

- i) Minimum Parking Setback Abutting Interior Side Property Line. Allow the off-street parking spaces located within one of the proposed parking lots within the East development abutting phase 1 of The Grove apartments to be less than the minimum 10-foot setback required abutting an interior side property line in the AU area of the refinement plan;
- j) Maximum Allowed Driveway Approaches per Parcel. Allow three driveway approaches onto the private streets within the East development where a maximum of two driveway approaches per parcel are allowed onto private streets within the AU and MI areas of the refinement plan; and
- k) Maximum Driveway Width. Allow the driveway approaches onto the proposed private streets within the East and West developments to exceed the maximum driveway approach widths required under the VC, AU, and MI areas of the refinement plan.

Subsequent to the public hearing notice being provided, the applicant, on June 1, 2021, submitted a revised set of plans in order to bring the proposed development into greater conformance with the applicable requirements of the refinement plan. As a result, **major amendments are no longer required for:**

- Maximum Accessory Structure Footprint. A major amendment is no longer needed because the size of the carports within the development have been reduced so they are no longer greater than 1,000 square feet.
- Minimum Building Setback Abutting Street for Building 22. A major amendment is no longer needed because the location of the building has been revised so it no longer encroaches into the minimum required 10-foot setback abutting Chapel Drive.
- Maximum Parking Setback Abutting Chapel Drive. A major amendment is no longer needed because the proposed parking spaces within the East development adjacent to Buildings 24 and 25 on the south side of Chapel Drive have been relocated to conform to the minimum required 20-foot setback adjacent to the street.

As a result of the revised plans there is also one instance where a building which was originally sited in conformance with the maximum building setback abutting a street no longer conforms to the maximum setback after the proposed revision. This applies to Building 34. Under the original plans Building 34 was located within the required 10-foot to 20-foot setback range abutting Strong Road SE. After the proposed revision to the site plan, however, the eastern half of the building now exceeds the maximum 20-foot setback adjacent to Strong Road. As such a major amendment is now necessary for this building also.

***SRC 530.035(e)(2)(A): The proposed amendment conforms to the applicable provisions of the Salem Area Comprehensive Plan.***

**Finding:** Analysis of the proposed major amendment for conformance with the applicable Goals and Policies of the Salem Area Comprehensive Plan is as follows:

The subject property is designated Mixed-Use on the Salem Area Comprehensive Plan Map. As such, the written statement provided by the applicant (**Attachment E**)

identifies the following Salem Area Comprehensive Plan goals and policies that are applicable to the Mixed-Use comprehensive plan designation as being applicable to the proposed amendment:

▪ **Mixed-Use Intent Statement (pages 7 to 8 of Salem Comprehensive Policies Plan):**

*The Mixed-Use designation encompasses the opportunity for a variety of housing opportunities, including but not limited to, single-family detached, single-family attached, garden apartments, apartments, communal living, and row houses. The location, density, and style of housing is governed by the zoning code. What differentiates this classification from Single-Family and Multi-Family Residential categories is an expansion of the types of uses deemed compatible with residential development. In addition to nonresidential uses (such as schools and other educational facilities, parks, open space, and religious organizations), this classification permits the mixing of shopping and services, convenience stores, commercial office (including live-work units) and compatible commercial and/or industrial uses within neighborhoods and structures.*

*The purpose of this designation is to:*

- 1. Allow a mixture of complementary land uses that may include housing of all types, retail, offices, commercial services, and civic uses to create economic and social vitality and the ability to meet multiple needs in compact areas;*
- 2. Develop mixed-use areas that are safe, comfortable and attractive to pedestrians;*
- 3. Provide flexibility in the siting and design of new development and redevelopment to optimize the use of natural resources, energy alternatives, improvements in development tools and techniques, and changes in the marketplace;*
- 4. Recognize streets as public places that encourage pedestrian, bicycle, and transit use;*
- 5. Facilitate efficient use of land by encouraging compact, high-density development and minimizing the amount of land that is needed for surface parking; and*
- 6. Encourage and facilitate development that supports public transit and the utilization of alternative modes of transportation.*

The written statement provided by the applicant indicates that the request is in conformance with intent of the mixed-use designation. Development of the subject property will provide a needed type of housing while facilitating a high-density development that is creative and pedestrian friendly.

**Finding:** Staff concurs with the findings included in the applicant's written statement. The requested major amendments to increase the number of units in the refinement plan and to deviate from lot, setback, building frontage, FAR, and driveway development standards will not result in development that is inconsistent with the comprehensive plan provision.

- **Mixed-Use Development Goal (page 33 of Salem Comprehensive Policies Plan):**

*To provide a mixture of complementary land uses that may include housing, retail, offices, services, industrial and civic uses, to create economic and social vitality.*

The written statement provided by the applicant indicates that the refinement plans are setup to provide a mixture of uses throughout the Fairview area. The applicant's proposal does not change the uses within the Fairview Refinement Plan. The amendment is to allow an increase in units allowed and deviation from some of the development standards. The proposed units will help to meet the Mixed-Use goals and housing needs by providing a needed housing type, like high density. The Fairview Plan is set up to allow a mixture of housing types to achieve its goal.

**Finding:** Staff concurs with the findings included in the applicant's written statement. The proposed major amendment allows for additional housing within the Fairview Refinement Plan II refinement plan which is consistent with the mix of uses envisioned by this comprehensive plan policy, the Fairview Plan, and the refinement plan. Development of additional housing along with an increased number of people living in the area has the potential to make commercial development with the FMU zone more attractive than it has been since the zone was adopted in 2003; thereby potentially helping to promote a future mix of residential and non-residential uses consistent with this comprehensive plan policy.

- **Mixed-Use Development Policy F.1 – Development (page 34 of Salem Comprehensive Policies Plan):**

*Encourage efficient use of land by facilitating compact, high-density development and minimizing the amount of land that is needed to accommodate automobile parking. Where appropriate, revise mixed-use design standards to make them simpler and easier to use, including making mixed-use design standards consistent across mixed-use areas.*

The written statement provided by the applicant indicates that the amendment is to allow an increase in units allowed and deviation from some of the development standards. The proposed units will help to meet the Mixed-Use Goals and housing needs. Development of the subject property will provide a needed type of housing while facilitating a high-density development that is creative and pedestrian friendly.

**Finding:** Staff concurs with the findings included in the applicant's written statement. The proposed major amendment allows for an increased amount of needed multiple family housing within the refinement plan. The proposal is consistent with the comprehensive plan policy.

- **Mixed-Use Development Policy F.2 – Development (page 34 of Salem Comprehensive Policies Plan):**

*Encourage development that preserves private open space for residents and employees of mixed-use development.*

The written statement provided by the applicant indicates that the Fairview Refinement Plan identifies designated open space areas and requirements. The proposal is not deviating from open space Development Standards. Open space is provided through the proposed development as outlined on the Open Space and Landscape Plans. Private open space via a balcony or patio will be provided for each unit per the Multi-Family Design Standards. A tree plan has been provided as well to identify the preservation of trees within the development sites.

**Finding:** Staff concurs with the findings included in the applicant's written statement. The proposed amendment to increase the number of residential units allowed in the refinement plan and to deviate from the identified development standards does not in any way discourage or prevent the provision of open space. As shown on the site plan and open space plan, a mixture of open space areas both common and private are included in the proposed development.

- **Mixed-Use Development Policy F.3 – Priorities for Mobility and Access (page 34 of Salem Comprehensive Policies Plan):**

*Facilitate development (land use mix, density, connectivity, design, and orientation) that reduces the need for, and frequency of, SOV trips and supports public transit, where applicable.*

The written statement provided by the applicant indicates that the pedestrian paths and walkable areas provide a more pedestrian friendly development to potentially reduce vehicle trips. However, currently there is no public transit available directly to the site.

**Finding:** Staff concurs with the findings included in the applicant's written statement. The proposed development provides a network of sidewalks and paths to facilitate bicycle and pedestrian connectivity throughout the development and to properties in the surrounding area.

- **Mixed-Use Development Policy F.4 – Priorities for Mobility and Access (page 34 of Salem Comprehensive Policies Plan):**

*Reinforce streets as public places that encourage pedestrian and bicycle travel.*

The written statement provided by the applicant indicates that the streets within the proposed development provide connections to adjacent developments via paved streets and/or paved pedestrian paths. The streets encourage pedestrian and bicycle travel throughout the development and surrounding developments.

**Finding:** Staff concurs with the findings included in the applicant's written statement. Streets within the development are consistent with the requirements of the refinement plan and a network of sidewalks and paths to facilitate bicycle and pedestrian connectivity throughout the development and to properties in the surrounding area is provided.

- **Mixed-Use Development Policy F.5 – Priorities for Mobility and Access (page 34 of Salem Comprehensive Policies Plan):**

*Provide roadway and pedestrian connections to residential areas.*

The written statement provided by the applicant indicates that pedestrian connectivity is an important development aspect of the Fairview Refinement Plan II. The streets and pathways within the proposed development provided connections to adjacent residential developments as shown on the site plans.

**Finding:** Staff concurs with the findings included in the applicant's written statement. The proposed development provides a network of sidewalks and paths to facilitate bicycle and pedestrian connectivity throughout the development and to properties in the surrounding area. Public and private through the subject properties and the surrounding area provide roadway connections to residential areas.

- **Mixed-Use Development Policy F.6 – Design (page 34 of Salem Comprehensive Policies Plan):**

*Develop commercial and mixed-use areas that are safe, comfortable and attractive to pedestrians.*

The written statement provided by the applicant indicates that development of the subject property will provide a needed type of housing while facilitating a high-density development that is attractive and pedestrian friendly.

**Finding:** Staff concurs with the findings included in the applicant's written statement. The proposal conforms to this comprehensive plan policy.

- **Mixed-Use Development Policy F.7 – Design (page 34 of Salem Comprehensive Policies Plan):**

*Provide flexibility in the siting and design of new developments, facilities, and redevelopment to respond to changes in the marketplace and infrastructure systems.*

The written statement provided by the applicant indicates that as shown on the site plan, the majority of the buildings are placed in close proximity to the street to provide a pedestrian friendly building.

**Finding:** The proposed major amendments included with this application are representative of the importance of providing for flexibility in the siting and design of new development developments in order to respond to changes in the marketplace. The amendment to allow for an increased number of residential units in the refinement plan is a result of the effects of the marketplace. With the lack of development of commercial uses on the property, there is an increasing demand for residential uses within the site. The proposed amendments to the development standards of the refinement plan provide for flexibility in the siting and design of the proposed development.

▪ **Mixed-Use Development Policy F.8 – Design (page 34 of Salem Comprehensive Policies Plan):**

*Provide appropriate transitions between mixed-use areas and adjacent single-use neighborhoods.*

The written statement provided by the applicant indicates that there are several adopted refinement plans within the Fairview area. All the plans work together to provide connectivity to adjacent uses.

**Finding:** The subject properties and all adjacent properties are located within the FMU zone which allows a wide variety of uses. Because the properties are not adjacent to a single-use neighborhood, this comprehensive plan policy is not applicable to the proposed development.

***SRC 530.035(e)(2)(B): The proposed amendment is compatible with adjoining land uses.***

The written statement provided by the applicant indicates that the proposed residential uses are compatible with the adjoining land uses that are either existing or proposed for future development.

**Finding:** Staff concurs with the findings included in the applicant's written statement. The subject properties are located within a mixed-use zone which allows a variety of uses, including residential uses.

***SRC 530.035(e)(2)(C): The proposed amendment is physically feasible, given consideration of existing or proposed infrastructure and public services.***

The written statement provided by the applicant indicates that the existing and required public facilities remain unchanged from the adopted 2009 Fairview Refinement Plan approvals. The applicant is required to provide the needed infrastructure for the development, which includes sewer, water, storm drainage and transportation facilities. The submitted plans show that the proposed buildings can be serviced by the infrastructure to support the development and are designed to City standards.

The proposal is for urban development in an area where future extensions of those services can be provided in the most feasible, efficient, and economical manner. The City's capital improvement program and its minimum code standards for public facilities provide a means for improving and updating public facilities systems (water and sewer). All necessary and appropriate public services and facilities essential for development will be provided to this property at levels that are adequate to serve the proposed use.

The City maintains an infrastructure of public services that includes sewer, water, and storm drainage facilities. The City will specify any needed changes to the existing service levels prior to development.

Other private service providers supply garbage, telephone, television, postal and internet services as needed by the development. The required public services and

facilities to serve new development will be determined by the City at the time development permits are requested.

An updated Traffic Analysis dated January 12, 2021 has been provided to staff. The proposed amendment is physically feasible and provides all required infrastructure and public services for development of the site.

**Finding:** Staff concurs with the findings included in the applicant's written statement. The proposed increase in the residential units allowed within the refinement plan is physically feasible and allows, as indicated in the applicant's written statement, a proportional share of dwelling units within the Fairview Refinement Plan II refinement plan as compared to other approved refinement plans, while at the same time allowing a sufficient number of dwelling units for the remaining portions of the Fairview site that do not have approved refinement plans to be developed at densities slightly above the minimums established in the FMU zone but not exceeding the maximum 2,000 overall units allowed.

***SRC 530.035(e)(2)(D): The proposed amendment conforms to the following goals:***

***(i) Encourage mixed-use development, improved protection of open spaces and natural features, and greater housing and transportation options;***

The written statement provided by the applicant indicates that the Refinement Plan encourages mix-use development and provides areas within the Plan for commercial and residential development. The proposed sites are within areas designated and zoned for residential development. The proposal will provide housing that is needed in this area, while protecting designated open space areas. The proposal will also provide common and private open space through the development.

**Finding:** Staff concurs with the findings included in the applicant's written statement. The proposed amendment provides for greater housing options in the form of additional multiple family housing units within the Fairview site which is currently predominantly developed with single family uses. The proposed development of the property provides for the protection of open space and natural features of the site.

***(ii) Encourage the innovative integration of park and school uses;***

The written statement provided by the applicant indicates that the proposed development is located directly to the south of the designated park land and located adjacent to an existing school (Heritage School). Leslie Middle school is located less than a mile from the site.

**Finding:** Staff concurs with the findings included in the applicant's written statement. The proposed amendments do not affect the ability to encourage innovative part and school uses.

***(iii) Encourage the principles of sustainable development and sustainable business practices;***



The written statement provided by the applicant indicates that the proposal encourages sustainable development by providing 143,900 square feet of open space through the development. Also preserving any identified open space areas and trees within the site that have been adopted within the Fairview Refinement Plan II.

**Finding:** Staff concurs with the findings included in the applicant's written statement. The proposal conforms to this goal.

**(iv) Support affordable housing options and mixed-income neighborhoods;**

The written statement provided by the applicant indicates that the proposal is for 183-units total within both sites. Apartments are deemed to be an affordable housing option that will also help to achieve a mixed-income neighborhood with all the existing single-family dwellings within the Fairview development.

**Finding:** Staff concurs with the findings included in the applicant's written statement. The proposed amendment allows for additional needed multiple family housing to be developed within the FMU zone. The proposal conforms to this goal.

**(v) Facilitate the resourceful use of land through the efficient arrangement of land uses, buildings, circulation systems, open space and infrastructure;**

The written statement provided by the applicant indicates that the proposed development and amendments facilitate resourceful use of land by providing more than adequate open space throughout the site, an efficient layout of the buildings, and a safe and efficient circulation system throughout the development for both vehicles and pedestrians.

**Finding:** Staff concurs with the findings included in the applicant's written statement. The proposal conforms to this goal.

**(vi) Encourage economic opportunities that comply with and support business practices;**

The written statement provided by the applicant indicates that the proposal encourages economic opportunities by providing jobs via the development of the site.

**Finding:** Staff concurs with the findings included in the applicant's written statement. The proposal conforms to this goal.

**(vii) Recognize the historic significance of buildings, structures, and sites, including archaeological sites, through appropriate means, including, but not limited to, obtaining official historic resource designation; and**

There are no historic buildings or structures located on the subject properties. All historic buildings within the Fairview Master Plan Areas have been documented.

**Finding:** Staff concurs with the findings included with the applicant's written statement. There are historic buildings or structures on the properties, but they are located with the City's Historic and Cultural Resources Protection zone due to the potential for archaeological resources.

**(viii) Encourage energy conservation and improved air and water quality.**

The written statement provided by the applicant indicates that the proposal is for 183-units within the 2 areas. There are no commercial uses proposed within these developments. However, the Fairview Plan does have areas designated for commercial use. Pedestrian paths and paved sidewalks are provided throughout to help minimize the use of vehicle transportation within the Fairview area.

**Finding:** The proposed amendments will not impact air and water quality and do not involve any policies pertaining to energy conservation.

## **10. CLASS 1 ADJUSTMENT APPROVAL CRITERIA**

Salem Revised Code (SRC) 250.005(d)(1) sets forth the following criteria that must be met before approval can be granted to an application for a Class 1 Adjustment. The following subsections are organized with approval criteria shown in **bold italic**, followed by findings evaluating the proposed development's conformance with the criteria. Lack of compliance with the following criteria is grounds for denial of the Class 1 Adjustment application, or for the issuance of certain conditions to ensure the criteria are met.

***SRC 250.005(d)(1)(A): The purpose underlying the specific development standard proposed for adjustment is:***

- (i) Clearly inapplicable to the proposed development; or***
- (ii) Clearly satisfied by the proposed development.***

The written statement provided by the applicant (**Attachment E**) indicates, in summary that the refinement plan requires a minimum of one vehicle parking space per every dwelling unit. The applicant is therefore required to provide a minimum of 78 on-site vehicle parking spaces. As shown on the site plan, 139 on-site parking space are being provided where a maximum of 137 are allowed. The applicant indicates that the purpose of keeping within minimum and maximum parking requirements is to avoid large parking lots within developments. The applicant explains that parking area has been designed with 9-foot-wide planter islands to minimize large expanses of parking and that the two additional parking spaces provided beyond the maximum help serve the property in a better way by providing for the residents which is better for this development.

**Finding:** As shown on the proposed site plan (**Attachment C**), the East development includes a total of 139 parking spaces. Of these spaces, 106 are located on site and the remaining 33 spaces are located along the private streets within the development.

As indicated by the applicant, the refinement plan requires a minimum of one parking space per dwelling unit. Based on the 78 units proposed within the East development,

the maximum allowed parking for this portion of the development, pursuant to SRC 806.015(d), is 137 spaces.

The underlying purpose of the maximum off-street parking requirement is to ensure the efficient use of land and to minimize the amount of land that is unnecessarily utilized for off-street parking. This purpose is consistent with the Mixed-Use comprehensive plan designation for the property which includes policies under the Comprehensive Policies Plan which call for:

- Encouraging efficient use of land by facilitating compact, high-density development and minimizing the amount of land that is needed to accommodate automobile parking (*Mixed-Use Development Policy 1 – Development*); and
- Facilitating development (land use mix, density, connectivity, design, and orientation) that reduces the need for, and frequency of, SOV trips and supports public transit, where applicable (*Mixed-Use Development Policy 3 – Priorities for Mobility and Access*).

It is also consistent with the Fairview Plan, which calls for development sustainability, promotion of alternative modes of transportation, and reduced dependence on automobiles.

In the case of the proposed development, the underlying minimum off-street parking required for the development in the refinement plan is slightly less than the normal 1.5 spaces per dwelling unit that would otherwise be required for this development if it were not located within the FMU zone and the Fairview Refinement Plan II refinement plan.

If the normal minimum 1.5 spaces per dwelling unit requirement were applied to the proposed development, the corresponding maximum number of spaces allowed would be 205 spaces (or 2.6 spaces per dwelling unit) instead of 137 spaces (or 1.76 spaces per dwelling unit).

The 139 spaces (or 1.78 spaces per dwelling unit) proposed with the development is only two spaces over the maximum allowed parking and still results in fewer off-street parking spaces than would otherwise be allowed without an adjustment if the property were not located with the refinement plan. Because the maximum number of spaces is proposed to only be increased by two, the underlying purpose of the maximum parking standard is maintained and therefore clearly satisfied by the proposed development. This approval criterion is met.

***SRC 250.005(d)(1)(B): The proposed adjustment will not unreasonably impact surrounding existing or potential uses or development.***

**Finding:** The proposed adjustment to increase the maximum number of parking spaces allowed for the East development by two spaces will not unreasonably impact surrounding existing or potential uses or development. Instead, the adjustment allows for a small increase above an already minimized maximum allowed amount of off-street parking to help the proposed development meet its anticipated parking demand in an area the City that although is planned to promote and encourage alternative means of transportation, still lacks facilities, such as transit, to help successfully implement that vision. This approval criterion is met.

## 11. CLASS 2 ADJUSTMENT APPROVAL CRITERIA

Salem Revised Code (SRC) 250.005(d)(2) sets forth the following criteria that must be met before approval can be granted to an application for a Class 2 Adjustment. The following subsections are organized with approval criteria shown in ***bold italic***, followed by findings evaluating the proposed development's conformance with the criteria. Lack of compliance with the following criteria is grounds for denial of the Class 2 Adjustment application, or for the issuance of certain conditions to ensure the criteria are met.

As indicated in the public hearing notice for the proposed development, a Class 2 Adjustment to eight development standards of the Salem Revised was requested by the applicant in conjunction with the proposed development. The requested adjustments include the following:

- a) Trees Around the Perimeter of Buildings. Decrease the minimum number of trees required to be planted around the perimeter of buildings (SRC 702.020(b)(4));
- b) Trees Around the Perimeter of Parking Areas & Planter Bay Width. Decrease the minimum number of trees required to be planted around the perimeter of off-street parking areas and allow some of the proposed parking lot planter bays within the development to be less than the minimum required nine feet in width (SRC 702.020(b)(7));
- c) Building Frontage. Allow less than 70 percent of the lot/street frontage of the West development within the VC area of the refinement plan along proposed Village Center Loop to be occupied by buildings placed at the setback line (SRC 702.020(e)(4));
- d) Building Frontage. Allow less than 50 percent of the lot/street frontages of the East development within the AU and MI areas of the refinement plan along streets to be occupied by buildings placed at the setback line (SRC 702.020(e)(4));
- e) Ground Floor Unit Entrances Facing Streets & Pedestrian Access. Allow ground floor dwelling units located within 25 feet of a property line abutting a street to be developed without a building entrance facing the street or a direct pedestrian connection to the adjacent sidewalk (SRC 702.020(e)(5));
- f) Interior Side Setback Plane. Allow proposed buildings located adjacent to interior side property lines to be developed without a 45-degree façade setback plane for those portions of the buildings that are greater than 28 feet in height;
- g) Dead-End Parking Areas without Turnarounds. Allow dead-end off-street parking areas within the East development to be constructed without a turnaround (SRC 806.040(a)); and
- h) Turnarounds for Waste Collection Vehicles. Allow the proposed trash/recycling areas to be developed without on-site turnaround areas for waste collection service vehicles (SRC 800.055(f)(2)).

On June 1, 2021, revised plans were submitted by the applicant in order to bring the development into greater conformance with some of the development standards that an adjustment had been requested for. As a result of the revisions to the plans, **Class 2 Adjustments are no longer required for:**

- Dead-End Parking Areas without Turnarounds. An adjustment is no longer needed because the parking areas within the east development have been revised to provide turnaround areas with the parking areas.
- Turnarounds for Waste Collection Vehicles. An adjustment is no longer needed because the location and orientation of the trash/recycling areas within the development have been changed. However, the revisions have created other issues concerning conformance with the applicable solid waste service area vehicle access standards of SRC 800.055(f), but conditions of approval are recommended in conjunction with the site plan review approval requiring the proposed trash/recycling areas to be designed to comply with the applicable standards.

***SRC 250.005(d)(2)(A): The purpose underlying the specific development standard proposed for adjustment is:***

- (i) Clearly inapplicable to the proposed development; or***  
***(ii) Equally or better met by the proposed development.***

**Finding:** The proposal includes a Class 2 Adjustment to six of the development standards included in the SRC. The Class 2 Adjustment requests approval to:

- a) Decrease the minimum number of trees required to be planted around the perimeter of buildings (SRC 702.020(b)(4));
- b) Decrease the minimum number of trees required to be planted around the perimeter of off-street parking areas and allow some of the proposed parking lot planter bays within the development to be less than the minimum required nine feet in width (SRC 702.020(b)(7));
- c) Allow less than 70 percent of the lot/street frontage of the West development within the VC area of the refinement plan along proposed Village Center Loop to be occupied by buildings placed at the setback line (SRC 702.020(e)(4));
- d) Allow less than 50 percent of the lot/street frontages of the East development within the AU and MI areas of the refinement plan along streets to be occupied by buildings placed at the setback line (SRC 702.020(e)(4));
- e) Allow ground floor dwelling units located within 25 feet of a property line abutting a street to be developed without a building entrance facing the street or a direct pedestrian connection to the adjacent sidewalk (SRC 702.020(e)(5));
- f) Allow proposed buildings located adjacent to interior side property lines to be developed without a 45-degree façade setback plane for those portions of the buildings that are greater than 28 feet in height;

**Trees Around the Perimeter of Buildings (SRC 702.020(b)(4)):**

The written statement provided by the applicant indicates that landscaping has been provided around the proposed buildings as shown on the plans.

The landscaping provided, however, does not conform to the minimum required number of plant units required for the planting of trees around the perimeter of buildings under SRC 702.040(b)(4). The proposed buildings that do not include the minimum number of required trees include Buildings 20, 22, 27, 28, 30, 31, and 32.

Because no justification for the reduced number of trees around the perimeter of buildings has not been provided, and because it appears that it is possible to revise the landscape plans to provide the minimum required number of trees around all buildings, the requested adjustment is recommended to be DENIED and instead the following condition be placed on the design review approval to ensure conformance with SRC 702.020(b)(4):

**Condition 5:** The landscaping plans shall be revised to include trees at a minimum density of 10 plant units per 60 linear feet of exterior building wall. Such trees shall be located not more than 25 feet from the edge of the building footprint.

**Trees Around the Perimeter of Parking Areas & Planter Bay Width (Fairview Alternative Multi-Family Design Standard):**

As shown on the landscape plans, canopy trees are not provided at the minimum required spacing of one tree per 30 feet of parking area perimeter. In addition, not all of the planter bays within the proposed parking areas meet the minimum required 9-foot planter bay width.

The written statement provided by the applicant indicates that the purpose of this requirement is to provide shade and break up all the paved areas. This helps to provide a pedestrian friendly development. The applicant explains that SRC 702.020(b)(7) requires a minimum 50-foot spacing requirement around the perimeter of parking lots. However, the Refinement Plan, requires a 30-foot spacing around the perimeter of parking lots. Therefore, the applicant is requesting an adjustment to the 30-foot spacing requirement for the planting of canopy trees around the perimeter of the parking lots. The applicant indicates that as shown on the landscape plan, there are large canopy trees provided throughout the parking bays and the development. Some canopy trees are provided every 30 feet. However, due to the size and shape of the site, a canopy tree cannot feasible be provided every 30 feet. This would result in the loss of landscaped areas, pedestrian paths, and parking.

The applicant explains that there are 252 trees being planted throughout the perimeter of the parking lots and the site. Therefore, the purpose of this requirement is to provide shaded areas for a pedestrian friendly development has been met. The apartment development will provide landscaped areas throughout the site, which include the planting of trees along with pedestrian paths/sidewalks and buildings. All of which will create a pedestrian friendly development with shaded areas for the residents.

Staff partially concurs with the findings included in the applicant's statement. One of the underlying purposes of this standard is, as the applicant indicates, to provide shade and to break up large continuous expanses of parking. Another important element is the stormwater quality function that canopy trees provide within parking areas to reduce stormwater discharge.

The revised landscape plans attached with this report that were submitted by the applicant on June 1, 2021, do increase the number of trees planted around the perimeter of the parking areas within the development to the 30-foot spacing required in the refinement plan where possible next to open space areas and within parking lot

perimeter setbacks not abutting buildings. In addition, the spacing of the planter bays within the interior of the parking lots themselves also conform the minimum spacing requirement of one bay for every 12 parking spaces established under SRC 702.020(b)(7)(B). Because number of trees planted around the perimeter of the parking areas has been increased in those areas where canopy trees can be physically accommodated based on their size and because the parking lot planter bay spacing within the parking areas otherwise meet the minimum spacing standard established under the design standards, the proposed adjustment equally meets the underlying purpose of the development standard.

**Building Frontage (Fairview Alternative Multiple Family Design Standard):**

As shown on the site plans, the proposed development does not meet the minimum 70 percent building frontage requirement adjacent to streets within the VC area of the site or the minimum 50 percent building frontage requirement adjacent to streets within the MI and AU areas of the site.

The written statement provided by the applicant indicates that the purpose of this requirement is to provide a pedestrian friendly development with building located close to the sidewalks instead of parking areas. The apartment development will provide landscaped areas throughout the site along with pedestrian paths/sidewalks throughout and visual appealing buildings. All of which will create a pedestrian friendly development.

Staff concurs with the findings included in the applicant's written statement. Although the minimum building frontages are not met along all streets within the development, the majority of the buildings are located within relative close proximity to the public and private streets on the boundaries of, and within, the development except where physically precluded by points of driveway access, required storm water facilities, other buildings, or the shape/configuration of the properties themselves. In no instances within the development are parking areas located between buildings and streets and the relative close proximity of the buildings to the street together with their three-story height will help to frame the street right-of-way as intended under this design standard. This approval criterion is met.

**Ground Floor Unit Entrances Facing Streets & Pedestrian Access (SRC 702.020(e)(5)):**

Buildings 20, 22, 23, 24, 26, 29, 30, 34, and 35 have ground units located within 25 feet of an abutting street, but do not have building entrances facing the street or direct pedestrian connections from the unit entrances to the adjacent sidewalks as required under SRC 702.020(e)(5).

The written statement provided by the applicant indicates that the purpose of this requirement is to provide a pedestrian friendly development with buildings entrances facing the street. The applicant explains that all buildings face the interior of the lot. The rear side of Buildings 20, 34, and 35 face Strong Road. The rear side of Buildings 23 and 24 faces Reed Road. The rear side of Building 22 faces Chapel Drive. The side of Buildings 23 and 24 face Chapel Drive. The side of Building 26 faces Reed Road and Lindburg Road. The rear side of Buildings 29 and 30 face VC Loop. The street side

of these buildings (rear/side) will be designed to be visually appealing, by providing similar design as is being provided for the front building facade for all buildings. In order to be consistent with the front facade of the building; windows, offsets, and architectural features will be incorporated in the portions of the building facing the right-of-way.

The applicant indicates that all buildings within the development have direct pedestrian access onto sidewalks. The internal pedestrian circulation system consists of hard 6-foot wide surfaced sidewalks that provide easily identifiable and safe connections between the residential units, parking, recreation areas, manager's apartment, and the trash disposal area. The pedestrian system connects the buildings to the public sidewalk system within Strong Road, Reed Road, Lindburg Road, VC Loop, and Chapel Drive via the proposed internal sidewalk system.

The applicant explains that due to the slope of the site, providing additional pedestrian paths is not feasible due to ADA requirements. Therefore, due to the slope, and not being able to meet ADA requirements for paths to the street, this standard is better met by providing the paths as outlined on the site plan. Please see revised site plans and grading plans.

Staff does not concur with the findings included in the applicant's written statement. The underlying purpose of this standard is to orient the ground floor dwelling units with buildings to the street, rather than turning their back to the street. Providing building entrances facing streets is an important element in creating vibrant, engaging, and active pedestrian environments. Facing ground floor dwelling units entrances away from the street and not providing any direct pedestrian connection from those units to the street does not equally or better meet the underlying intent of the standard. At a minimum any ground floor dwelling unit located within 25 feet of a street should have a direct pedestrian connection to the street as was done for the previous phase of The Grove apartments at Fairview. Because Buildings 20, 22, 23, 24, 26, 29, 30, 34, and 35 have no ground floor entrances facing the street and no direct pedestrian connection to the adjacent sidewalks, the proposed adjustment to this design review requirement does not equally or better meet the underlying intent of the standard. This approval criterion is therefore not met and it is recommended that this adjustment be DENIED unless revised plans are submitted which meet the standard or otherwise equally or better meet the underlying purpose of the standard.

**Interior Side Setback Plane (*Fairview Alternative Multi-Family Design Standard*):**

Buildings 20, 21, 32, 33, and 34 within the development adjacent to side yards do not include a minimum 45-degree side yard façade setback plane for those portions of the buildings above 28 feet in height.

The written statement provided by the applicant indicates that the purpose of this standard is to provide additional side setbacks along the boundary of the development and the underlying purpose of this standard will be equally met with landscaping and adequate setbacks adjacent to all property lines.

Staff concurs with the findings included within the applicant's written statement. The underlying purpose of this standard is equally met by the proposed development.



***SRC 250.005(d)(2)(B): If located within a residential zone, the proposed development will not detract from the livability or appearance of the residential area.***

**Finding:** The subject properties are zoned FMU (Fairview Mixed-Use). The FMU zone allows for a wide variety of uses that may be established and therefore is not categorized as a residential zone. Because the subject property is not located within a residential zone, this approval criterion is not applicable to the proposed development.

***SRC 250.005(d)(2)(C): If more than one adjustment has been requested, the cumulative effect of all the adjustments result in a project which is still consistent with the overall purpose of the zone.***

**Finding:** Pursuant to SRC 530.001, the overall purpose of the FMU zone is to encourage innovative planning resulting in mixed-use development, improved protection of open spaces and natural features, and greater housing and transportation options. The provisions of the Fairview Refinement Plan II refinement plan implement this overall purpose.

Though eight adjustments were originally requested in conjunction with the proposed development and only six are now needed, four are recommended to be approved. These four adjustments will not cumulatively result in a project that is inconsistent with the overall purposes of the FMU zone, the Fairview Plan, or the Fairview Refinement Plan II refinement plan.

The proposed development will introduce a housing type intended for the former Fairview Training Center site and envisioned in the refinement plan. The proposed development provides for pedestrian connectivity within and through the site, includes safe and convenient vehicular access, and preserves existing trees and provides common open space and recreational amenities.

The four adjustments recommended for approval are limited to the minimum necessary to accommodate the proposed development without compromising the purpose of the zone and refinement plan. This approval criterion is met.

## **12. CLASS 2 DRIVEWAY APPROACH PERMIT APPROVAL CRITERIA**

Pursuant to SRC 804.001 and the driveway approach standards included under Section 7, Table 1, of the refinement plan, the driveway approach standards of SRC Chapter 804 (Driveway Approaches) apply to driveway approaches onto public streets and the driveway/curb cut standards of the refinement plan apply only to driveway approaches onto private streets.

The proposed development includes two driveway approaches onto public streets, Lindburg Road SE and Strong Road SE. Both Lindburg Road and Strong Road are designated as a collector streets under the City's transportation System Plan (TSP). Pursuant to SRC 804.025(a)(2), a Class 2 Driveway Approach Permit is required for a driveway approach onto a collector street serving a use other than Single Family or Two Family. A Class 2 Driveway Approach permit is therefore required for the proposed driveway approaches onto Lindburg Road and Strong Road.

Salem Revised Code (SRC) 804.025(d) sets forth the following criteria that must be

met before approval can be granted to an application for a Class 2 Driveway Approach Permit. The following subsections are organized with approval criteria shown in ***bold italic***, followed by findings evaluating the proposed development's conformance with the criteria. Lack of compliance with the following criteria is grounds for denial of the Class 2 Driveway Approach Permit, or for the issuance of certain conditions to ensure the criteria are met.

***SRC 804.025(d)(1): The proposed driveway approach meets the standards of this chapter and the Public Works Design Standards;***

**Finding:** The Public Works Department reviewed the proposed driveway approaches for conformance with the requirements of SRC Chapter 804 and provided comments indicating that the proposed driveway approaches meet the standards of SRC Chapter 804 and the Public Works Design Standards. This criterion is met.

***SRC 804.025(d)(2): No site conditions prevent placing the driveway approach in the required location;***

**Finding:** The written statement provided by the applicant (**Attachment E**) indicates that the location of driveways was taken into consideration prior to laying out the site. The location of the proposed driveways takes into consideration the location of the internal streets adjacent to the site and existing development. Therefore, all factors were taken into consideration and there are no conditions on the site that prevent the driveway approaches.

The Public Works Department also reviewed the proposal and provided comments indicating that there are no site conditions prohibiting the location of the proposed driveway. This criterion is met.

***SRC 804.025(d)(3): The number of driveway approaches onto an arterial are minimized;***

**Finding:** Lindburg Road and Strong Road are designated as collector streets under the TSP. No access to an arterial street is proposed. This criterion is therefore not applicable to the proposed development.

***SRC 804.025(d)(4): The proposed driveway approach, where possible:***

***(A) Is shared with an adjacent property; or***

***(B) Takes access from the lowest classification of street abutting the property;***

**Finding:** Due to the size of the properties and the configuration of the proposed development several driveway approaches are provided. Within the West development two driveway approaches are proposed onto a private street, Village Center Loop. Within the East development three driveway approaches are proposed onto private streets, two onto Mountain Street SE and one onto Chapel Drive SE. All of the driveway approaches proposed, other than the private street driveway approaches onto Lindburg Road and Strong Road, take access onto lower classification private local streets. As such, the driveway approaches have been, where possible, provided to lower classification streets abutting with or abutting the properties. This approval criterion is met.

***SRC 804.025(d)(5): The proposed driveway approach meets vision clearance standards;***

**Finding:** The proposed driveway approaches onto Lindburg Road and Strong Road meet the vision clearance standards of SRC Chapter 805. This approval criterion is met.

***SRC 804.025(d)(6): The proposed driveway approach does not create traffic hazards and provides for safe turning movements and access;***

**Finding:** The Public Works Department reviewed the proposed driveway approaches for conformance with the requirements of SRC Chapter 804 and indicated that no evidence has been submitted to indicate that the proposed driveways will create traffic hazards or unsafe turning movements. Additionally, staff analysis of the proposed driveways indicates that they will not create a traffic hazard and will provide for safe turning movements for access to the subject property. This approval criterion is met.

***SRC 804.025(d)(7): The proposed driveway approach does not result in significant adverse impacts to the vicinity;***

**Finding:** The Public Works Department reviewed the proposed driveway approaches and indicated that staff analysis of the proposed driveways and the evidence that has been submitted indicate that the location of the proposed driveways will not have any adverse impacts to the adjacent properties or streets. This approval criterion is met.

***SRC 804.025(d)(8): The proposed driveway approach minimizes impact to the functionality of adjacent streets and intersections; and***

**Finding:** The Public Works Department reviewed the proposed driveway approaches for conformance with the requirements of SRC Chapter 804 and provided comments indicating that the proposed driveway approaches are located on Collector streets and do not create a significant impact to adjacent streets and intersections. This approval criterion is met.

***SRC 804.025(d)(9): The proposed driveway approach balances the adverse impacts to residentially zoned property and the functionality of adjacent streets.***

**Finding:** The proposed development is located on, and surrounded by, property zoned FMU (Fairview Mixed-Use) and is therefore not in a purely residential zoned area. The proposed driveways will not have an adverse impact to residentially zone property or the functionality of adjacent streets. This criterion is met.

## **RECOMMENDATION**

Staff recommends that the Planning Commission adopt the facts and findings of the staff report and take the following action for the subject properties totaling approximately 10.74 acres in size, zoned FMU (Fairview Mixed-Use) within the Fairview Refinement Plan II refinement plan, and located at 2110 Strong Road SE and the 4000 to 4100 Blocks of Reed Road SE (Marion County Assessor Map and Tax Lot Numbers: 083W0200100 and 083W11A01002).

A. **APPROVE** the Class 3 Site Plan Review for the proposed development, subject to the following conditions of approval:

- Condition 1:** Prior to issuance of building buildings permits for the proposed West development, the final plat for the Legacy Heights Subdivision (Case No. SUB-FRPA20-03) shall be recorded.
- Condition 2:** Obtain all state and federal permits as indicated by the Department of State Lands (DSL).
- Condition 3:** All trash/recycling areas shall conform to the solid waste service area standards of SRC 800.055.
- Condition 4:** Design and construct all City infrastructure, including streets, according to the refinement plan and PWDS.
- Condition 5:** Convey land for dedication to equal a half-width right-of-way of 36 feet along the entire frontage of the subject property measured from the newly established offset centerline of Reed Road SE, and as shown on public construction plans under permit number 21-101403-PC.
- Condition 6:** Construct a full-street improvement along the frontage of Reed Road SE to Minor Arterial street standards from Strong Road SE to approximately 500 feet south of Lindberg Road SE, and as shown on public construction plans under permit number 21-101403-PC.
- Condition 7:** Complete incomplete portions of the boundary street improvements along the property frontages of Strong Road SE and Lindberg Road SE.
- Condition 8:** Construct Village Center Loop SE (private) from Lindburg Road SE to Strong Road SE. The private/public street intersections shall be constructed pursuant to PWDS.
- Condition 9:** Construct an extension of Mountain Street SE (private) from its existing terminus at the northern boundary of the eastern property to Chapel Drive SE (private).
- Condition 10:** Construct incomplete portions of the boundary street improvements along the property frontages of Chapel Drive SE (private).
- Condition 11:** Dedicate public access easements for the proposed private streets within the development.
- Condition 12:** Dedicate a public access easement for the proposed 10-foot-wide multi-use path through the subject property.
- Condition 13:** Construct an 18-inch S-1 water main within Reed Road SE from Strong Road SE to approximately 500 feet south of Lindberg Road SE.
- Condition 14:** Design and construct a storm drainage system at the time of development in compliance with Salem Revised Code (SRC) Chapter 71 and Public Works Design Standards (PWDS).

B. **APPROVE** the Class 3 Design Review for the proposed development subject to the following conditions of approval:

- Condition 1:** A minimum of two plant units shall be provided adjacent to the primary entryway of each dwelling unit or combination of dwelling units.
- Condition 2:** Shrubs shall be distributed around the perimeter of buildings at a minimum density of one plant unit per 15 linear feet of exterior building wall.
- Condition 3:** The private ground floor patios included within the development shall be visually separated from common open space through the use of perimeter landscaping.
- Condition 4:** All parking area planter bays shall be a minimum of 9 feet in width.

C. **APPROVE** the Fairview Refinement Plan Major Amendment to:

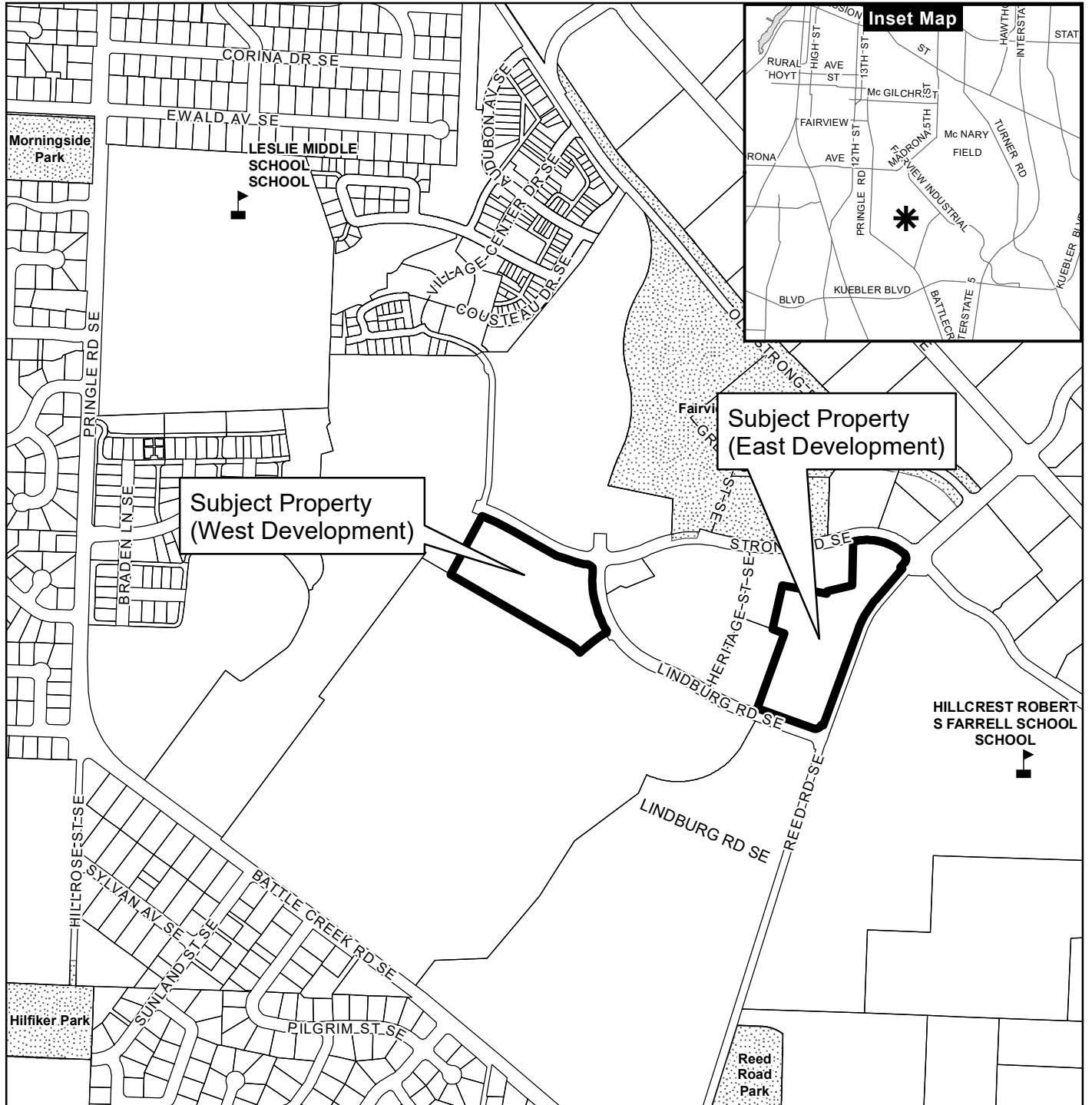
- a) Increase the number of dwelling units allowed within the refinement plan from 280 to 457;
- b) Allow the lot for the East development to exceed the maximum lot depth requirements of the AU (Adaptive Use) and MI (Mixed-Intensity) areas of the refinement plan;
- c) Allow the minimum required floor-area-ratio (lot coverage) of the West development located within the VC (Village Center) area of the refinement plan to be less than 0.75 FAR;
- d) Allow less than 70 percent of the lot/street frontage of the West development within the VC area of the refinement plan along Lindburg Road and Strong Road to be occupied by buildings placed at the minimum setback line;
- e) Allow Buildings 20, 23, 24, 25, 26, 27, 28, 29, 30, 31, 32, 33, 34, and 35 to be setback greater than the maximum 20-foot setback abutting a street required in the VC, AU, and MI areas of the refinement plan;
- f) Allow Building 24 to be setback less than the minimum required 10-foot setback abutting a street required in the AU and MI areas of the refinement plan;
- g) Allow the proposed off-street parking spaces along the southern property line of the West development to be setback less than the minimum 20-foot setback abutting a street required in the VC and MI areas of the refinement plan;
- h) Allow the off-street parking spaces located within one of the proposed parking lots within the East development abutting phase 1 of The Grove apartments to be less than the minimum 10-foot setback required abutting an interior side property line in the AU area of the refinement plan;
- i) Allow three driveway approaches onto the private streets within the East development where a maximum of two driveway approaches per parcel are allowed onto private streets within the AU and MI areas of the refinement plan; and
- j) Allow the driveway approaches onto the proposed private streets within the East and West developments to exceed the maximum driveway approach widths required under the VC, AU, and MI areas of the refinement plan.

- D. **APPROVE** the Class 1 Adjustment to increase the maximum number of parking spaces allowed within the East development from 137 spaces to 139 spaces.
- E. **DENY** the Class 2 Adjustment to decrease the minimum number of trees required to be planted around the perimeter of buildings and place the following condition on the design review approval to ensure conformance with the requirements of SRC 702.020(b)(4):
- Condition 5:** The landscaping plans shall be revised to include trees at a minimum density of 10 plant units per 60 linear feet of exterior building wall. Such trees shall be located not more than 25 feet from the edge of the building footprint.
- F. **DENY** the Class 2 Adjustment to allow ground floor dwelling units located within 25 feet of a property line abutting a street to be developed without a building entrance facing the street or a direct pedestrian connection to the adjacent sidewalk.
- G. **APPROVE** the Class 2 Adjustment to:
- a) Decrease the minimum number of trees required to be planted around the perimeter of off-street parking areas;
  - b) Allow less than 70 percent of the lot/street frontage of the West development within the VC area of the refinement plan along proposed Village Center Loop to be occupied by buildings placed at the setback line;
  - c) Allow less than 50 percent of the lot/street frontages of the East development within the AU and MI areas of the refinement plan along streets to be occupied by buildings placed at the setback line; and
  - d) Allow proposed buildings located adjacent to interior side property lines to be developed without a 45-degree façade setback plane for those portions of the buildings that are greater than 28 feet in height.
- H. **APPROVE** the Class 2 Driveway Approach Permit for the proposed driveway approaches within the West development onto Strong Road SE and Lindburg Road SE.








- Attachments:**
- A. Vicinity Map
  - B. Existing Conditions Plan
  - C. Revised Site, Open Space, Landscaping, Grading, and Utility Plans
  - D. Building Elevations
  - E. Applicant's Written Statement
  - F. Public Works Department Comments
  - G. FMU Area Zoning
  - H. Tree Conservation Plan Map

Prepared by Bryce Bishop, Planner III

# Vicinity Map 2110 Strong Road SE & 4000 to 4100 Blocks of Reed Road SE



**Legend**

-  Taxlots
-  Urban Growth Boundary
-  City Limits
-  Outside Salem City Limits
-  Historic District
-  Schools
-  Parks



0 100 200 400 Feet



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Owner / Developer:

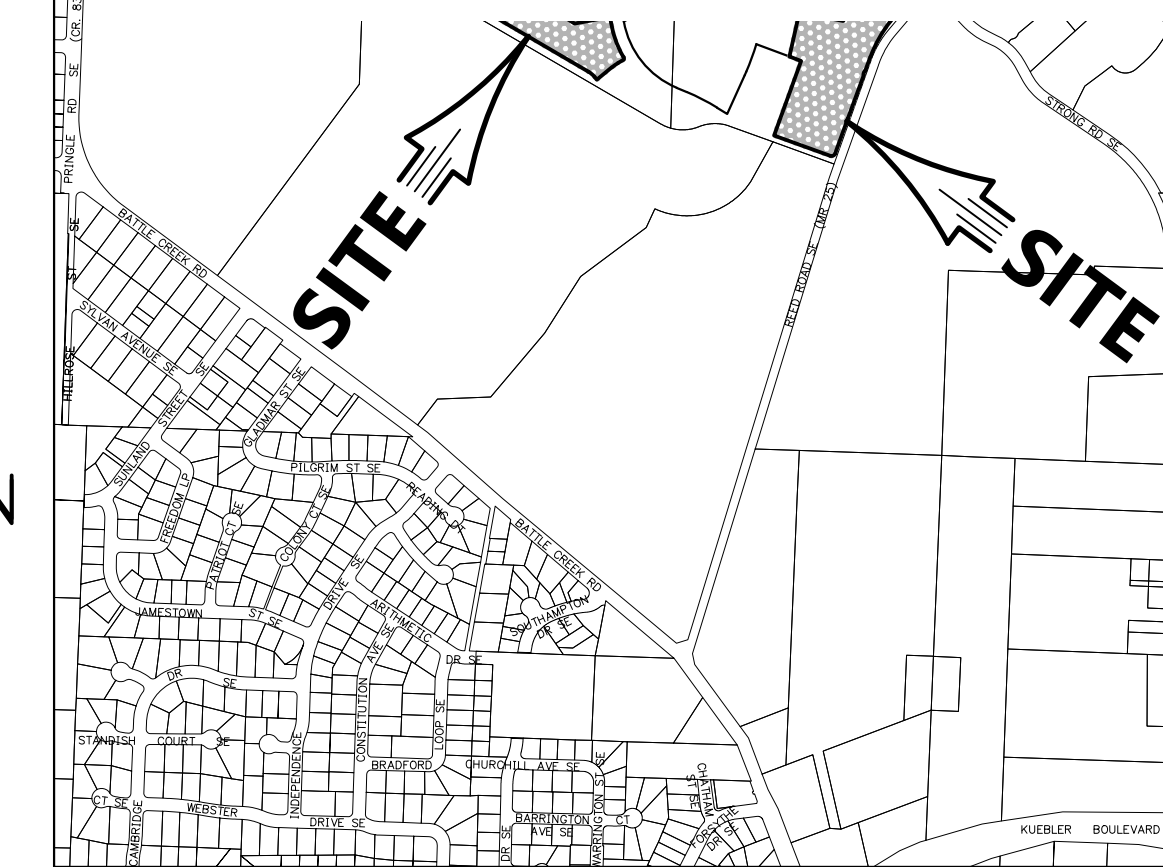
**MWIC GROVE, LLC**

201 FERRY STREET S.E.  
SUITE 400  
SALEM, OREGON 97301

# THE GROVE AT FAIRVIEW 2

SEC. 11, T. 8 S., R. 3 W., W.M.  
CITY OF SALEM  
MARION COUNTY, OREGON

# Attachment B



VICINITY MAP

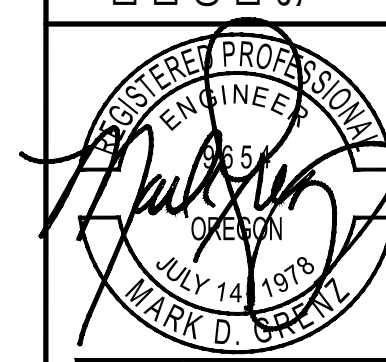
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1155 S. 31st ST., S.E.  
PH: (503) 363-9227  
www.mtengineering.net

COVER SHEET

THE GROVE AT FAIRVIEW 2

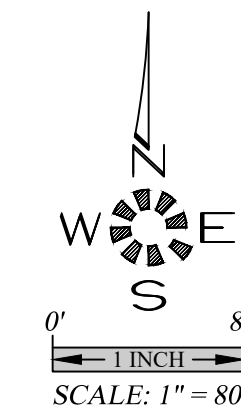
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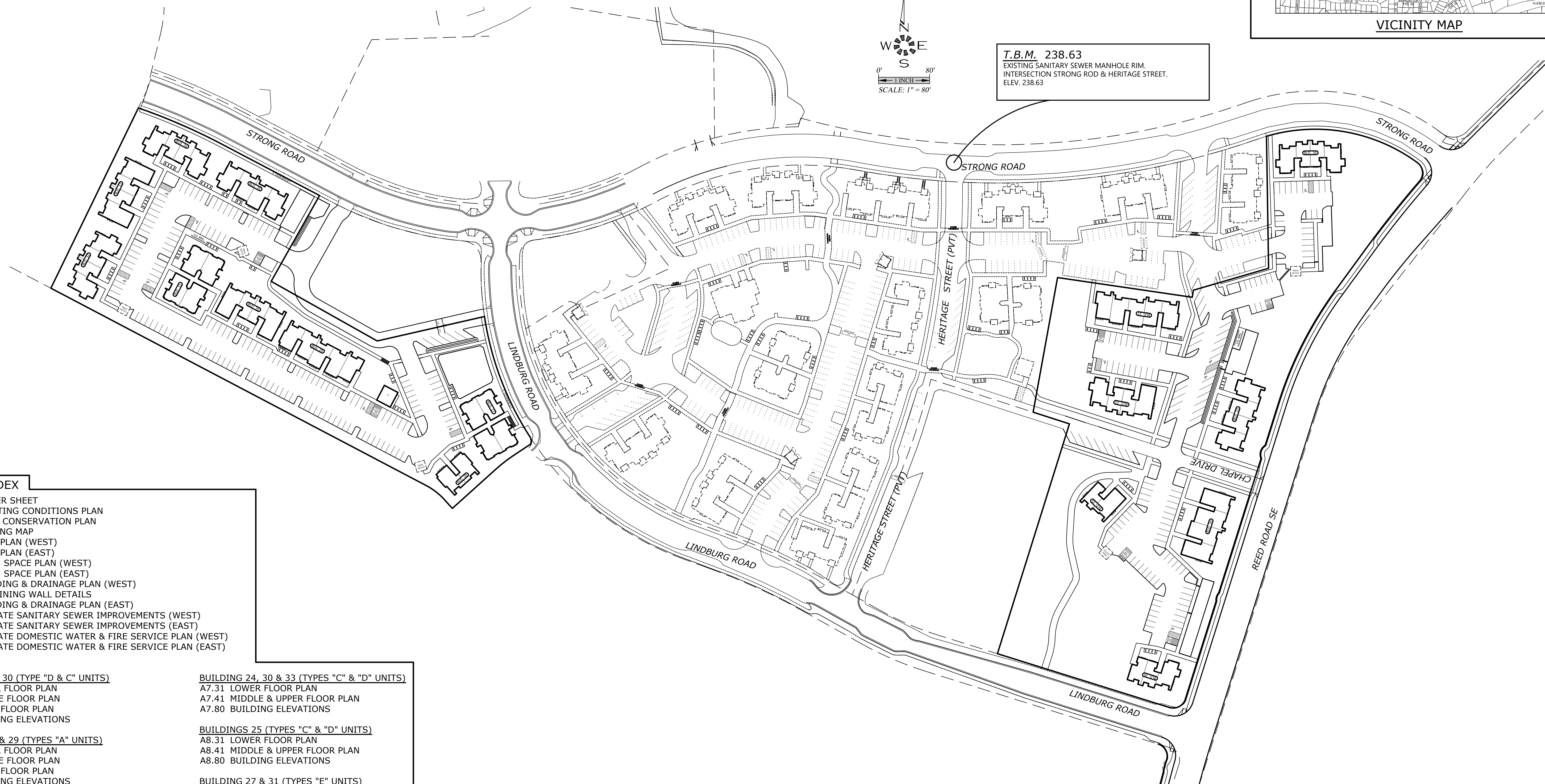


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**SDR1**



T.B.M. 238.63  
EXISTING SANITARY SEWER MANHOLE RIM.  
INTERSECTION STRONG ROAD & HERITAGE STREET.  
ELEV. 238.63



### SHEET INDEX

- SDR1 COVER SHEET
- SDR2 EXISTING CONDITIONS PLAN
- SDR3 TREE CONSERVATION PLAN
- SDR4 ZONING MAP
- SDR5 SITE PLAN (WEST)
- SDR6 SITE PLAN (EAST)
- SDR7 OPEN SPACE PLAN (WEST)
- SDR8 OPEN SPACE PLAN (EAST)
- SDR9 GRADING & DRAINAGE PLAN (WEST)
- SDR10 RETAINING WALL DETAILS
- SDR11 GRADING & DRAINAGE PLAN (EAST)
- SDR12 PRIVATE SANITARY SEWER IMPROVEMENTS (WEST)
- SDR13 PRIVATE SANITARY SEWER IMPROVEMENTS (EAST)
- SDR14 PRIVATE DOMESTIC WATER & FIRE SERVICE PLAN (WEST)
- SDR15 PRIVATE DOMESTIC WATER & FIRE SERVICE PLAN (EAST)

- BUILDING 24, 30 (TYPE "D" & "C" UNITS)**
- A1.31 LOWER FLOOR PLAN
  - A1.41 MIDDLE FLOOR PLAN
  - A1.50 UPPER FLOOR PLAN
  - A1.80 BUILDING ELEVATIONS

- BUILDING 21 & 29 (TYPES "A" UNITS)**
- A2.31 LOWER FLOOR PLAN
  - A2.41 MIDDLE FLOOR PLAN
  - A2.50 UPPER FLOOR PLAN
  - A2.90 BUILDING ELEVATIONS

- BUILDING 23 & 35 (TYPE "A" & "B" UNITS)**
- A3.31 LOWER FLOOR PLAN
  - A3.41 MIDDLE & UPPER FLOOR PLAN
  - A3.80 BUILDING ELEVATIONS

- BUILDING 20, 22, 32 & 34 (TYPES "B" & "C" UNITS)**
- A4.31 LOWER FLOOR PLAN
  - A4.41 MIDDLE & UPPER FLOOR PLAN
  - A4.80 BUILDING ELEVATIONS

- BUILDING 24, 30 & 33 (TYPES "C" & "D" UNITS)**
- A7.31 LOWER FLOOR PLAN
  - A7.41 MIDDLE & UPPER FLOOR PLAN
  - A7.80 BUILDING ELEVATIONS

- BUILDINGS 25 (TYPES "C" & "D" UNITS)**
- A8.31 LOWER FLOOR PLAN
  - A8.41 MIDDLE & UPPER FLOOR PLAN
  - A8.80 BUILDING ELEVATIONS

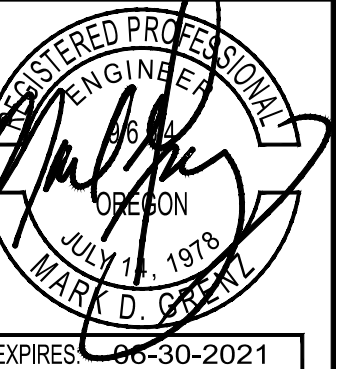
- BUILDING 27 & 31 (TYPES "E" UNITS)**
- A9.31 LOWER FLOOR PLAN
  - A9.41 MIDDLE FLOOR PLAN
  - A9.50 UPPER FLOOR PLAN
  - A9.90 BUILDING ELEVATIONS

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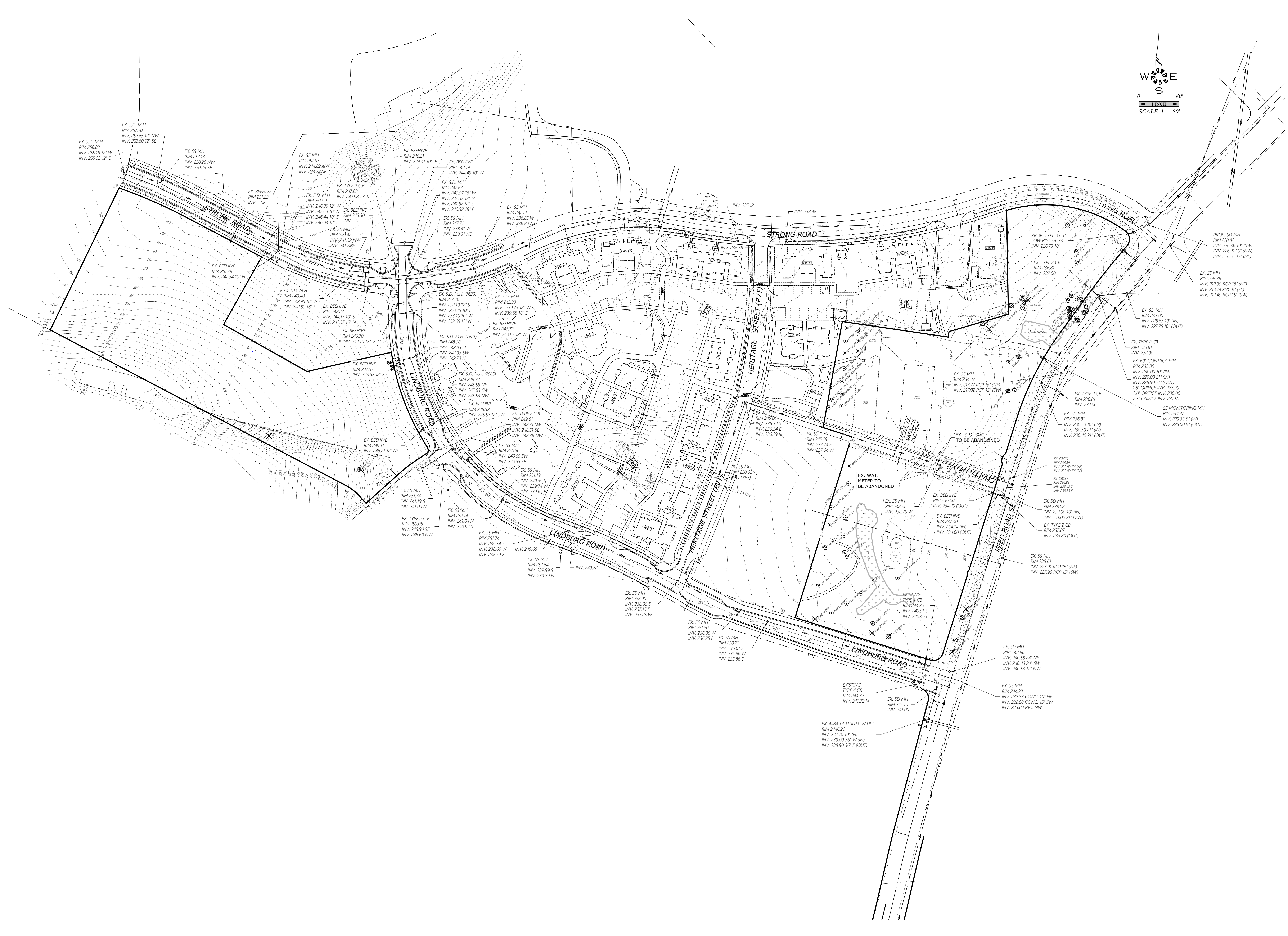
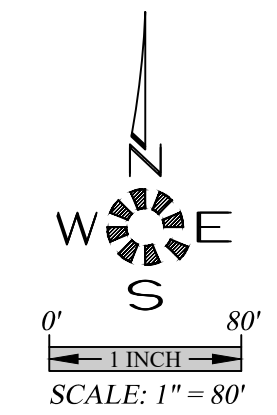
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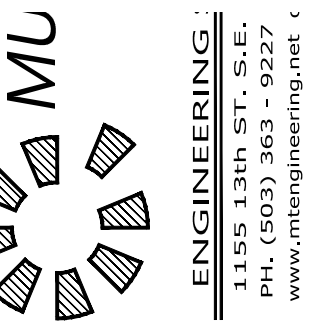
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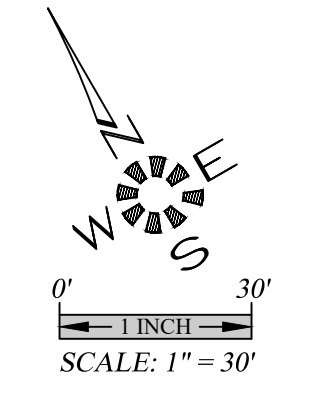
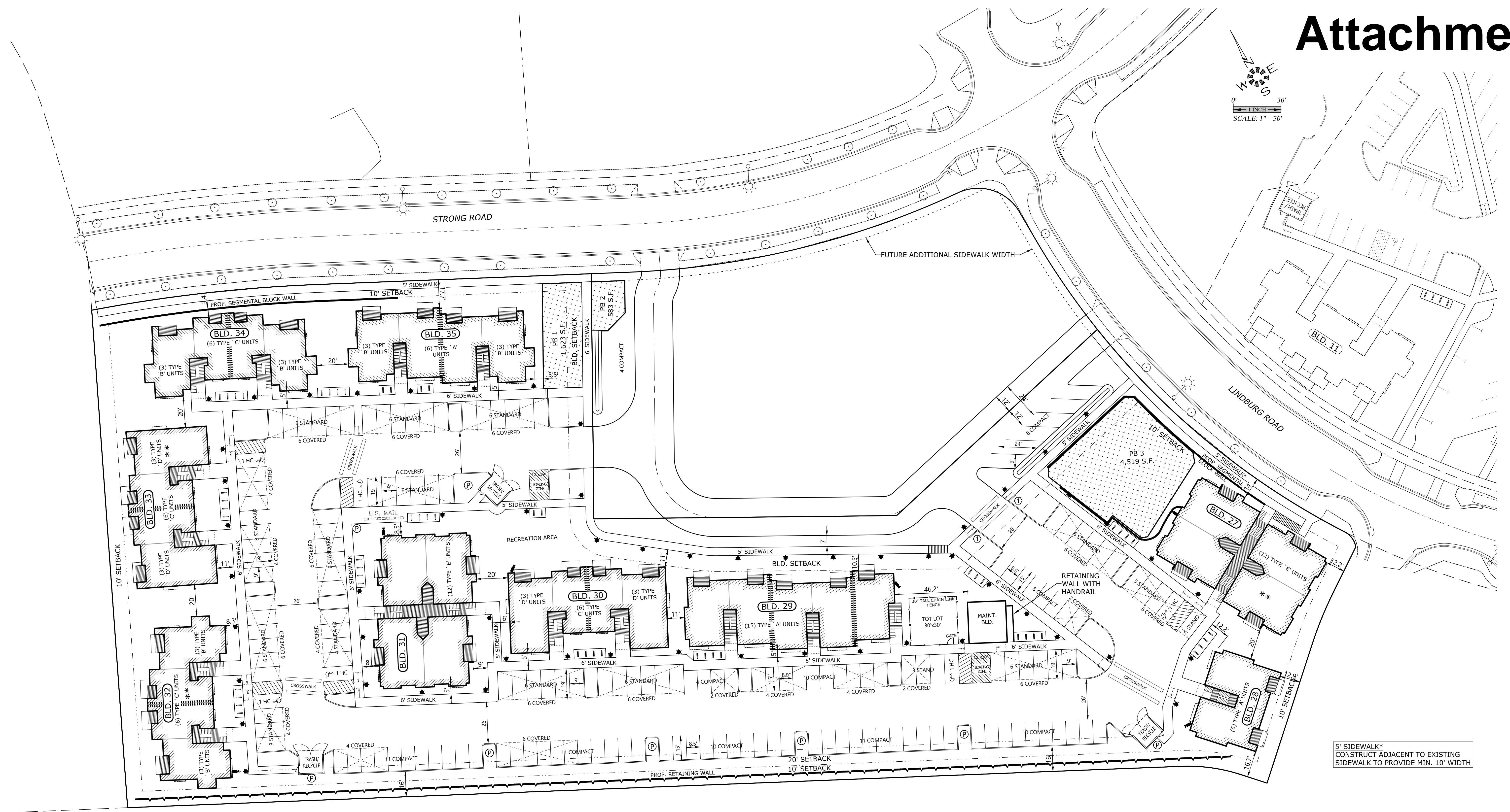
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# Attachment C



## SITE PLAN (WEST)

## THE GROVE AT FAIRVIEW 2



### ADA HANDICAP ACCESSIBILITY NOTES:

- ALL ON-SITE WALKWAYS, PEDESTRIAN CONNECTIONS TO THE PUBLIC SIDEWALK AND ROUTES TO BUILDING ENTRANCES ARE ACCESSIBLE WITH RUNNING SLOPES LESS THAN 5% AND CROSS SLOPE LESS THAN 2% MAX. LANDINGS AT BOTTOM OF STAIRS AND EXT. FACE OF ENTRANCE DOORS SHALL HAVE A SLOPE IN THE DIRECTION OF TRAVEL NOT TO EXCEED 2%.
- HANDICAP PARKING STALLS AND ACCESS AISLES ARE TO HAVE SLOPES IN ANY DIRECTION OF LESS THAN 2% MAX. GRAPHIC MARKINGS & SIGNAGE FOR HANDICAP AND VAN ACCESSIBLE STALLS WILL BE PER OSSC 2010 CHAPTER 11 AND ORS. REQUIREMENTS.
- HANDICAP ACCESSIBLE CURB RAMPS SHALL HAVE A RUNNING SLOPE NOT TO EXCEED 1:12 MAX. AND A CROSS SLOPE NOT TO EXCEED 1%.
- THE COMMUNITY BUILDING & ON-SITE LAUNDRY FACILITIES WILL BE FULLY HANDICAP ACCESSIBLE IN ACCORDANCE WITH ANSI A117.1 AND CHAPTER 11 OF THE 2010 OSSC.
- 2% OF THE LIVING UNITS OR (3) UNITS WILL BE TYPE 'A' HANDICAP ACCESSIBLE. THESE INCLUDE A 1, 2 AND 3 BEDROOM UNIT AS INDICATED ON THIS SITE PLAN. THE BALANCE OF THE GROUND FLOOR LIVING UNITS WILL BE TYPE 'B' ADAPTABLE UNITS IN ACCORDANCE WITH ANSI A117.1.

\*\* THE INDICATED LOWER FLOOR UNITS IN BUILDINGS 5 & 9 ARE TO BE TYPE A UNITS IN ACCORDANCE WITH THE 2014 OSSC SEC. 1107.6.2.1.1 (NOTED ON FLOOR PLANS). ALL OTHER LOWER FLOOR UNITS TO BE TYPE B UNITS IN ACCORDANCE WITH THE 2014 OSSC SEC. 1107.6.2.1.2

- \* - (107) 7' TALL MOUNT VERNON OUTDOOR POST LIGHT BLK.
- Ⓟ - (6) 18' TALL KAD LED 60C 100 30K R3 MVOLT RADON DBLXD POLE LIGHT
- ||||| - LOCATION OF ELECTRICAL SEPARATION WALL
- ||||| - 8 BICYCLE SPACES
- - 4 SURVEILLANCE CAMERAS
- Ⓜ - END RAMP NOT TO EXCEED 12:1 SLOPE

NOTE: BASEMENT LIMITS FOR PRIVATE STREETS ARE LOCATED 1 FOOT BEHIND BACK OF SIDEWALKS

**105 TOTAL APARTMENT UNITS**  
 27 TYPE "A" 2-Bd, 2-Ba (952 S.F.) UNITS  
 18 TYPE "B" 2-Bd, 2-Ba (1029 S.F.) UNITS  
 24 TYPE "C" 1-Bd, 1-Ba (728 S.F.) UNITS  
 12 TYPE "D" 3-Bd, 2-Ba (1204 S.F.) UNITS  
 24 TYPE "E" 2-Bd, 2-Ba (1,162 S.F.) UNITS

**173 TOTAL PARKING STALLS**  
 82 STANDARD STALLS  
 81 COMPACT STALLS  
 4 COMPACT (OFF-SITE)  
 6 HANDICAP STALLS\*  
 2 LOADING ZONE  
 106 BICYCLE SPACES

**105 TOTAL COVERED STALLS**  
 80 STANDARD STALLS\*\*  
 22 COMPACT STALLS  
 3 HANDICAP STALLS\*\*

- 1 MAINTENANCE BUILDING
- 3 TRASH/RECYCLE
- 2 RECREATION/PLAY AREAS
- 1 U.S. MAIL BOX AREA

(\* 1-MARKED "WHEELCHAIR USE ONLY")  
 (\*\* CARPORT STALLS COVER 9'x18')

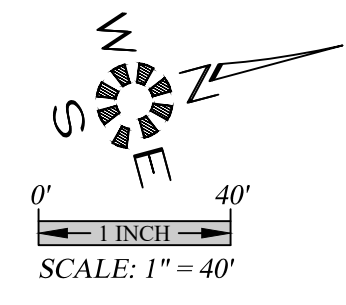
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**SDR5**



★★ THE INDICATED LOWER FLOOR UNITS IN BUILDINGS 5 & 9 ARE TO BE TYPE 'A' UNITS IN ACCORDANCE WITH THE 2014 OSSC SEC. 1107.6.2.1.1 (NOTED ON FLOOR PLANS). ALL OTHER LOWER FLOOR UNITS TO BE TYPE 'B' UNITS IN ACCORDANCE WITH THE 2014 OSSC SEC. 1107.6.2.1.2

**ADA HANDICAP ACCESSIBILITY NOTES:**

1. ALL ON-SITE WALKWAYS, PEDESTRIAN CONNECTIONS TO THE PUBLIC SIDEWALK AND ROUTES TO BUILDING ENTRANCES ARE ACCESSIBLE WITH RUNNING SLOPES LESS THAN 5% AND CROSS SLOPE LESS THAN 2% MAX. LANDINGS AT BOTTOM OF STAIRS AND EXT. FACE OF ENTRANCE DOORS SHALL HAVE A SLOPE IN THE DIRECTION OF TRAVEL NOT TO EXCEED 2%.
2. HANDICAP PARKING STALLS AND ACCESS AISLES ARE TO HAVE SLOPES IN ANY DIRECTION OF LESS THAN 2% MAX. GRAPHIC MARKINGS & SIGNAGE FOR HANDICAP AND VAN ACCESSIBLE STALLS WILL BE PER OSSC 2010 CHAPTER 11 AND ORS. REQUIREMENTS.
3. HANDICAP ACCESSIBLE CURB RAMPS SHALL HAVE A RUNNING SLOPE NOT TO EXCEED 1:12 MAX. AND A CROSS SLOPE NOT TO EXCEED 1%.
4. THE COMMUNITY BUILDING & ON-SITE LAUNDRY FACILITIES WILL BE FULLY HANDICAP ACCESSIBLE IN ACCORDANCE WITH ANSI A117.1 AND CHAPTER 11 OF THE 2010 OSSC.
5. 2% OF THE LIVING UNITS OR (3) UNITS WILL BE TYPE 'A' HANDICAP ACCESSIBLE. THESE INCLUDE A 1, 2 AND 3 BEDROOM UNIT AS INDICATED ON THIS SITE PLAN. THE BALANCE OF THE GROUND FLOOR LIVING UNITS WILL BE TYPE 'B' ADAPTABLE UNITS IN ACCORDANCE WITH ANSI A117.1.

- ★ - (107) 7' TALL MOUNT VERNON OUTDOOR POST LIGHT BLK.
- Ⓟ - (6) 18' TALL KAD LED 60C 100 30K R3 MVOLT RADON DBLXD POLE LIGHT
- ||||| - LOCATION OF ELECTRICAL SEPARATION WALL
- ||||| - 8 BICYCLE SPACES
- ||||| - 6 BICYCLE SPACES
- - 4 SURVEILLANCE CAMERAS
- Ⓜ - END RAMP NOT TO EXCEED 12:1 SLOPE

NOTE:  
 EASEMENT LIMITS FOR PRIVATE STREETS ARE LOCATED 1 FOOT BEHIND BACK OF SIDEWALKS

**78 TOTAL APARTMENT UNITS**

- 21 TYPE "A" 2-Bd, 2-Ba (952 S.F.) UNITS
- 18 TYPE "B" 2-Bd, 2-Ba (1029 S.F.) UNITS
- 21 TYPE "C" 1-Bd, 1-Ba (728 S.F.) UNITS
- 18 TYPE "D" 3-Bd, 2-Ba (1204 S.F.) UNITS

**139 TOTAL PARKING STALLS**

- 59 STANDARD STALLS
- 75 COMPACT STALLS
- 5 HANDICAP STALLS\*
- 1 LOADING ZONE
- 78 BICYCLE SPACES

**59 TOTAL COVERED STALLS**

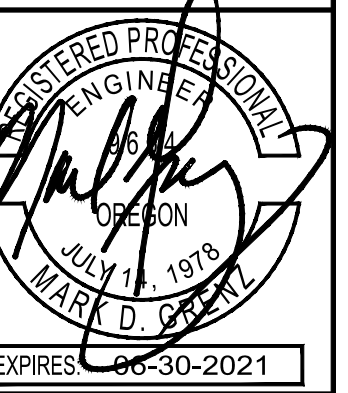
- 48 STANDARD STALLS\*\*
- 8 COMPACT STALLS
- 3 HANDICAP STALLS\*\*

(\* 1-MARKED "WHEELCHAIR USE ONLY")  
 (\*\* CARPORT STALLS COVER 9'x18')

- 2 TRASH/RECYCLE
- 1 PLAY AREA
- 1 U.S. MAIL BOX AREA

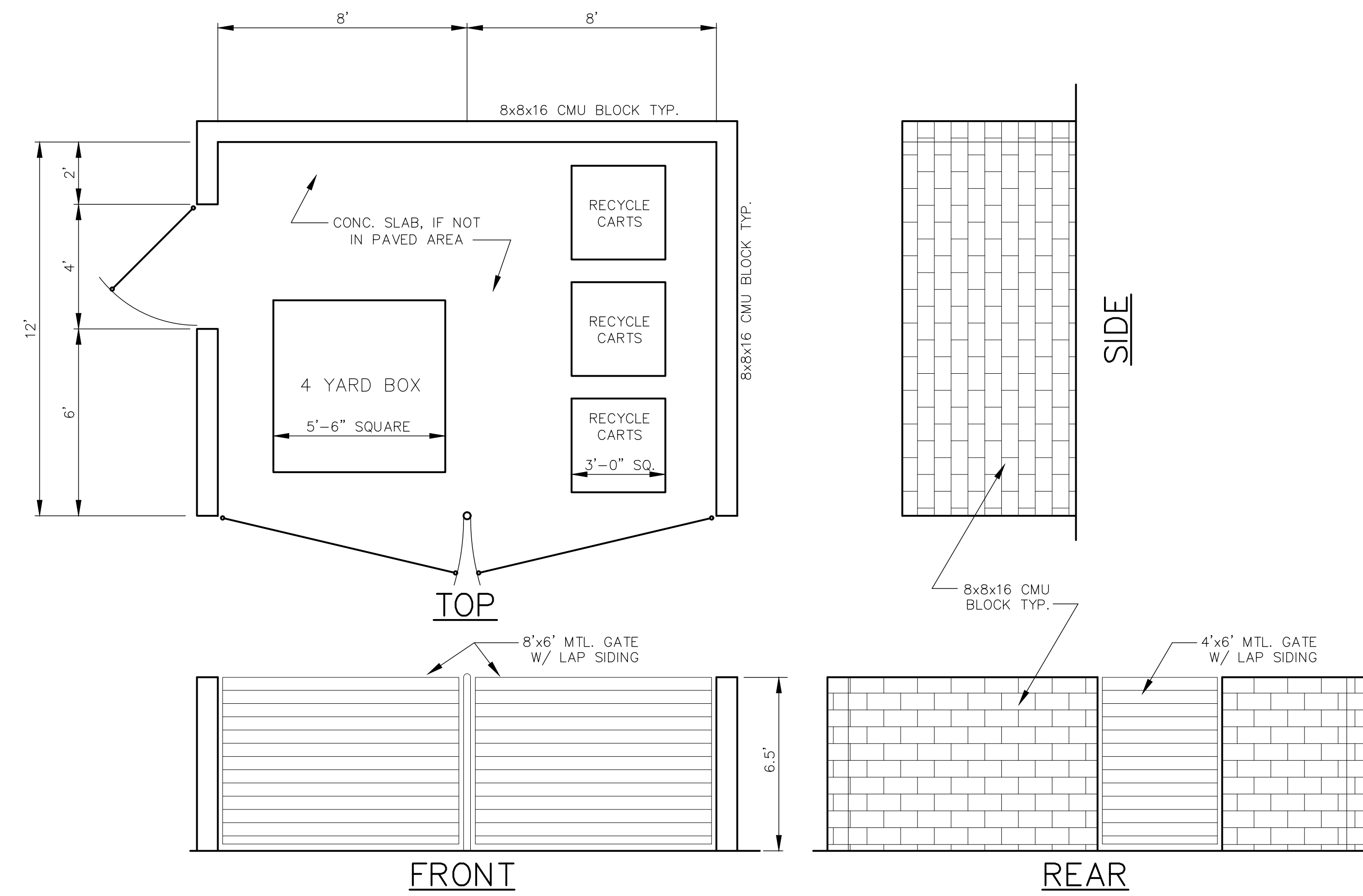
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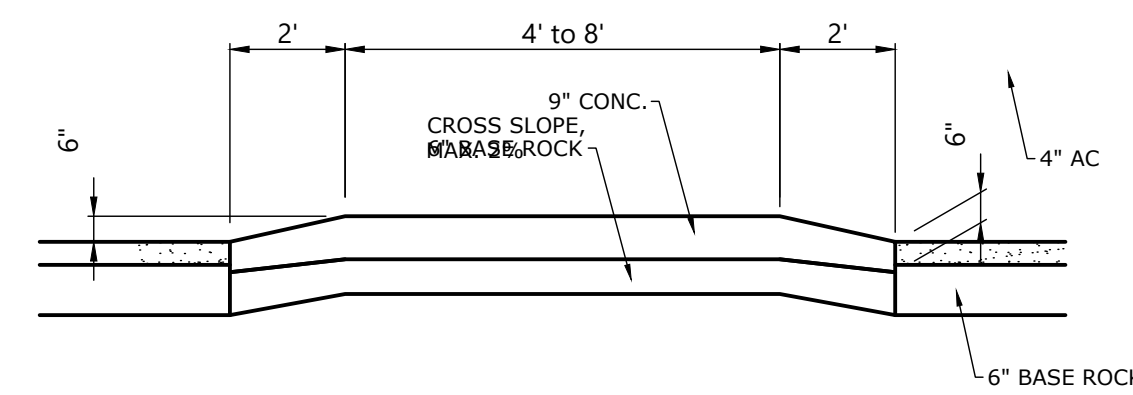
**SDR6**



FRONT REAR SIDE

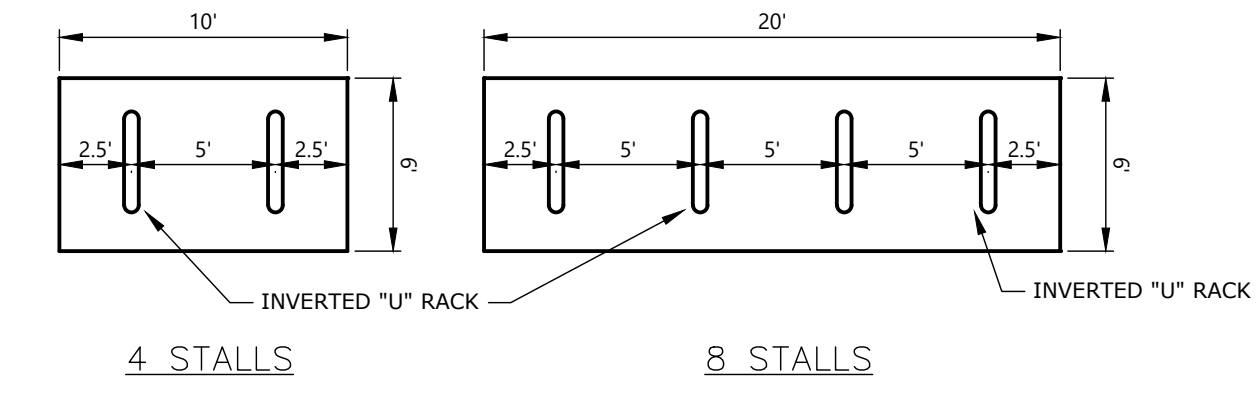
TRASH & RECYCLE ENCLOSURE

-NTS-



RAISED PESTRIAN CROSSING

-NTS-



TYPICAL BICYCLE RACKS

NTS

NO CHANGES, MODIFICATIONS OR REPRODUCTIONS TO BE MADE TO THESE DRAWINGS WITHOUT WRITTEN AUTHORIZATION FROM THE DESIGN ENGINEER.  
DIMENSIONS & NOTES TAKE PRECEDENCE OVER GRAPHICAL REPRESENTATION.

711P SDR/SITE  
Design: M.D.G.  
Drawn: C.D.S.  
Checked: B.M.G.  
Date: \_\_\_\_\_  
Scale: AS SHOWN



EXPIRES: 06-30-2021

JOB # 7111

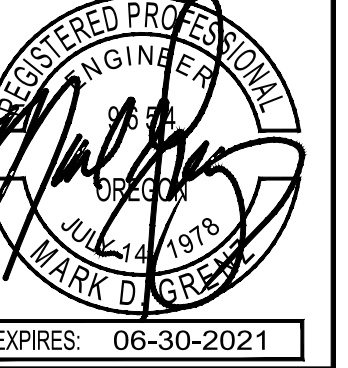
SDR7

**OPEN SPACE PLAN (WEST)**

**THE GROVE AT FAIRVIEW 2**

NO CHANGES, MODIFICATIONS OR REPRODUCTIONS TO BE MADE TO THESE DRAWINGS WITHOUT WRITTEN AUTHORIZATION FROM THE DESIGN ENGINEER.  
 DIMENSIONS & NOTES TAKE PRECEDENCE OVER GRAPHICAL REPRESENTATION.

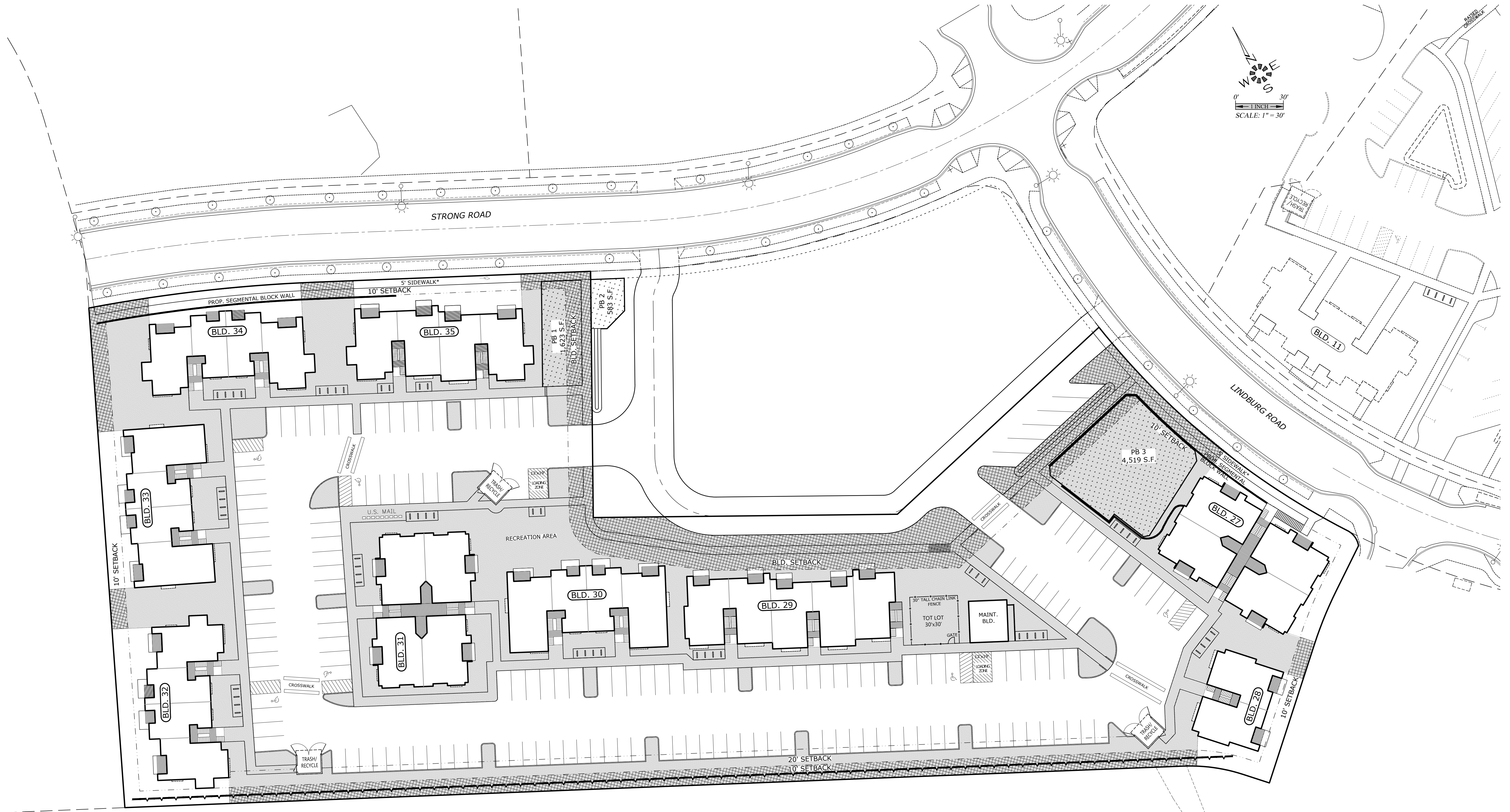
7111P 5088-05  
 Design: M.D.G.  
 Drawn: C.D.S.  
 Checked: B.M.G.  
 Date: \_\_\_\_\_  
 Scale: AS SHOWN



EXPIRES: 06-30-2021

JOB # 7111

**SDR8**



**VC ZONE**  
**SITE TOTALS:**  
 TOTAL AREA \_\_\_\_\_ 187,066 S.F. (4.29 AC.)

**OPEN SPACE:**  
 COMMON OPEN SPACE \_\_\_\_\_ 69,631 S.F. (37.22%)  
 WITHIN PERIMETER BUILDING SETBACKS \_\_\_\_\_ 20,250 S.F. (10.83%)

**PERVIOUS AREA** \_\_\_\_\_ 64,337 S.F. (34.39%)  
 LANDSCAPE AREA \_\_\_\_\_ 42,394 S.F.  
 LANDSCAPE W/IN PERIMETER SETBACKS \_\_\_\_\_ 17,097 S.F.  
 LANDSCAPE W/IN PARKING \_\_\_\_\_ 4,846 S.F.

**IMPERVIOUS AREA** \_\_\_\_\_ 122,729 S.F. (65.61%)  
 PARKING & DRIVEWAY \_\_\_\_\_ 61,716 S.F.  
 SIDEWALK \_\_\_\_\_ 19,000 S.F.  
 BUILDINGS \_\_\_\_\_ 42,013 S.F.

COMMON OPEN SPACE  
 COMMON OPEN SPACE W/IN BUILDING SETBACKS

**OPEN SPACE PLAN (EAST)**

**THE GROVE AT FAIRVIEW 2**

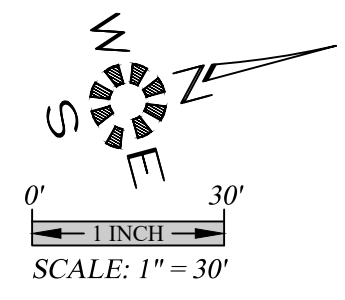
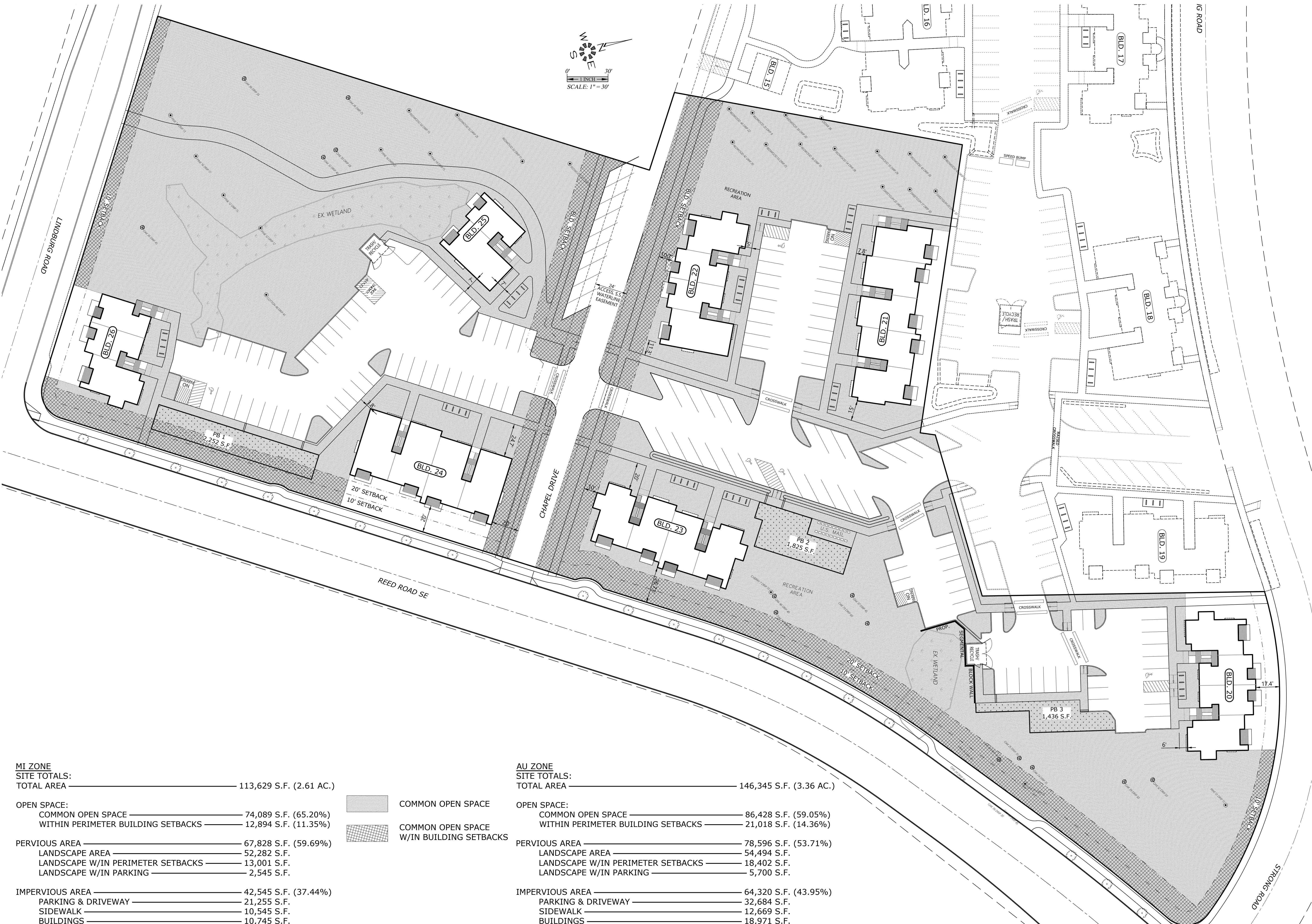
NO CHANGES, MODIFICATIONS OR REPRODUCTIONS TO BE MADE TO THESE DRAWINGS WITHOUT WRITTEN AUTHORIZATION FROM THE DESIGN ENGINEER.  
 DIMENSIONS & NOTES TAKE PRECEDENCE OVER GRAPHICAL REPRESENTATION.

7111P 508P-05  
 Design: M.D.G.  
 Drawn: C.D.S.  
 Checked: B.M.G.  
 Date: \_\_\_\_\_  
 Scale: AS SHOWN



EXPIRES: 06-30-2021  
 JOB # 7111

**SDR9**



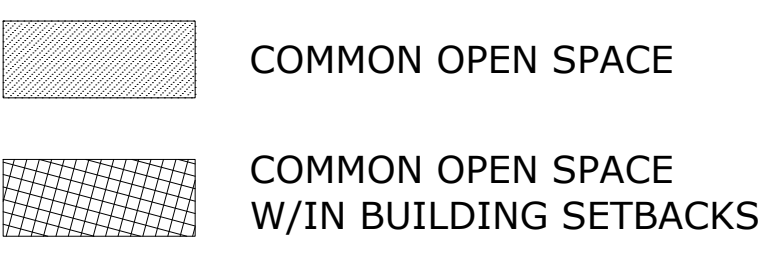
**MI ZONE**

**SITE TOTALS:**  
 TOTAL AREA 113,629 S.F. (2.61 AC.)

**OPEN SPACE:**  
 COMMON OPEN SPACE 74,089 S.F. (65.20%)  
 WITHIN PERIMETER BUILDING SETBACKS 12,894 S.F. (11.35%)

**PERVIOUS AREA** 67,828 S.F. (59.69%)  
 LANDSCAPE AREA 52,282 S.F.  
 LANDSCAPE W/IN PERIMETER SETBACKS 13,001 S.F.  
 LANDSCAPE W/IN PARKING 2,545 S.F.

**IMPERVIOUS AREA** 42,545 S.F. (37.44%)  
 PARKING & DRIVEWAY 21,255 S.F.  
 SIDEWALK 10,545 S.F.  
 BUILDINGS 10,745 S.F.



**AU ZONE**

**SITE TOTALS:**  
 TOTAL AREA 146,345 S.F. (3.36 AC.)

**OPEN SPACE:**  
 COMMON OPEN SPACE 86,428 S.F. (59.05%)  
 WITHIN PERIMETER BUILDING SETBACKS 21,018 S.F. (14.36%)

**PERVIOUS AREA** 78,596 S.F. (53.71%)  
 LANDSCAPE AREA 54,494 S.F.  
 LANDSCAPE W/IN PERIMETER SETBACKS 18,402 S.F.  
 LANDSCAPE W/IN PARKING 5,700 S.F.

**IMPERVIOUS AREA** 64,320 S.F. (43.95%)  
 PARKING & DRIVEWAY 32,684 S.F.  
 SIDEWALK 12,669 S.F.  
 BUILDINGS 18,971 S.F.

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**SCHEMATIC LANDSCAPE PLAN  
 (WEST)**

**THE GROVE AT FAIRVIEW 2**

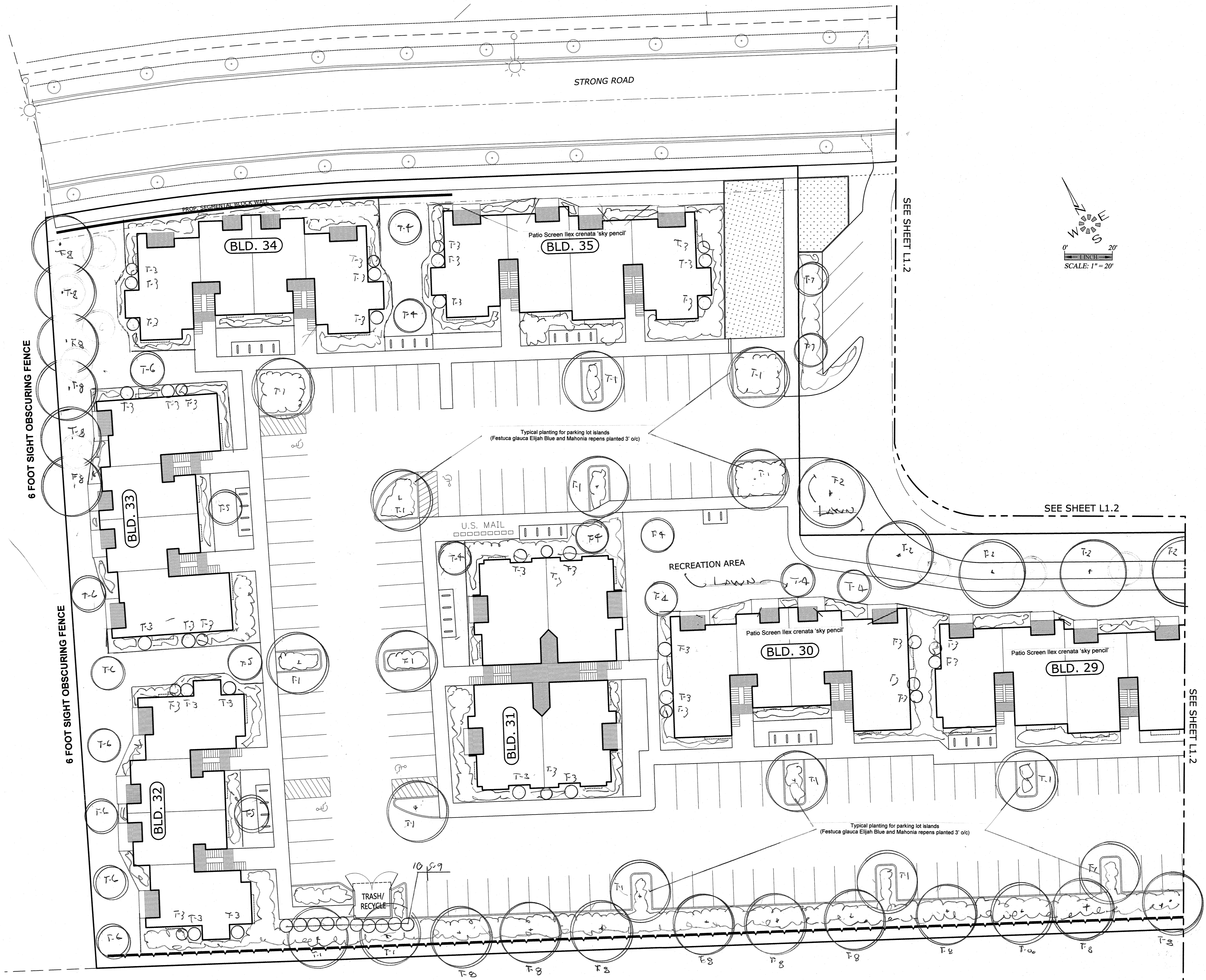
NO CHANGES, MODIFICATIONS  
 OR REPRODUCTIONS TO BE  
 MADE IN THESE DRAWINGS  
 WITHOUT THE WRITTEN  
 AUTHORIZATION FROM THE  
 DESIGN ENGINEER.  
 DIMENSIONS & NOTES TAKE  
 PRECEDENCE OVER  
 GRAPHICAL REPRESENTATION.

7111 L1.1  
 Design: M.D.G.  
 Drawn: C.D.S.  
 Checked: B.M.G.  
 Date: \_\_\_\_\_  
 Scale: AS SHOWN



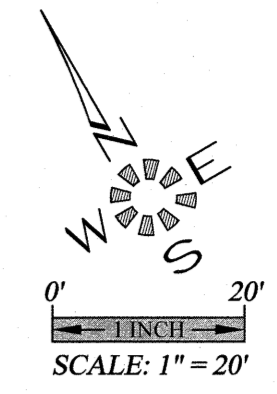
JOB # 7111

**L1.1**



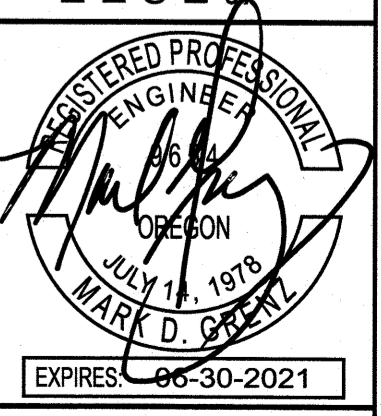
**PERMANENT UNDERGROUND IRRIGATION SYSTEM WILL BE INSTALLED**

LANDSCAPE & IRRIGATION DESIGN BY:  
**LANDSCAPE DESIGN  
 &  
 CONSULTANTS LLC**  
 Doing business since 1985  
 620 WORMWOOD ST. S.E. SALEM, OR. 97306 PHONE: (503) 551-8390



**PERMANENT UNDERGROUND IRRIGATION SYSTEM WILL BE INSTALLED**

LANDSCAPE & IRRIGATION DESIGN BY:  
**LANDSCAPE DESIGN & CONSULTANTS LLC**  
 Doing business since 1985  
 620 WORMWOOD ST. S.E. SALEM, OR. 97306 PHONE: (503) 551-8590



NO CHANGES, MODIFICATIONS OR REPRODUCTIONS TO BE MADE TO THESE DRAWINGS WITHOUT WRITTEN AUTHORIZATION FROM THE DESIGN ENGINEER. DIMENSIONS & NOTES TAKE PRECEDENCE OVER GRAPHICAL REPRESENTATION.

7/11/21 U.S.  
 Design: M.D.G.  
 Drawn: C.D.S.  
 Checked: B.M.G.  
 Date: 7/11/21  
 Scale: AS SHOWN

JOB # 7111  
**L1.2**

**SCHEMATIC LANDSCAPE PLAN (WEST)**

**THE GROVE AT FAIRVIEW 2**

**MULTI/TECH**  
 ENGINEERING SERVICES, INC.  
 1155 13th ST. S.E. SALEM, OR. 97302  
 PH: (503) 363 - 9227 FAX (503) 364-1260  
 www.multiengineering.net orce@multiengineering.net

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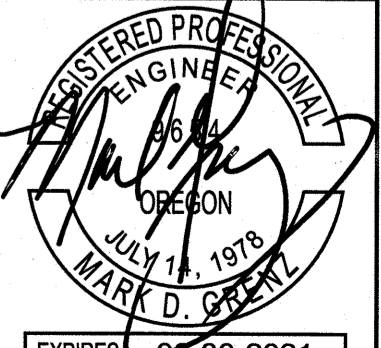


**SCHEMATIC LANDSCAPE PLAN  
 (EAST)**

**THE GROVE AT FAIRVIEW 2**

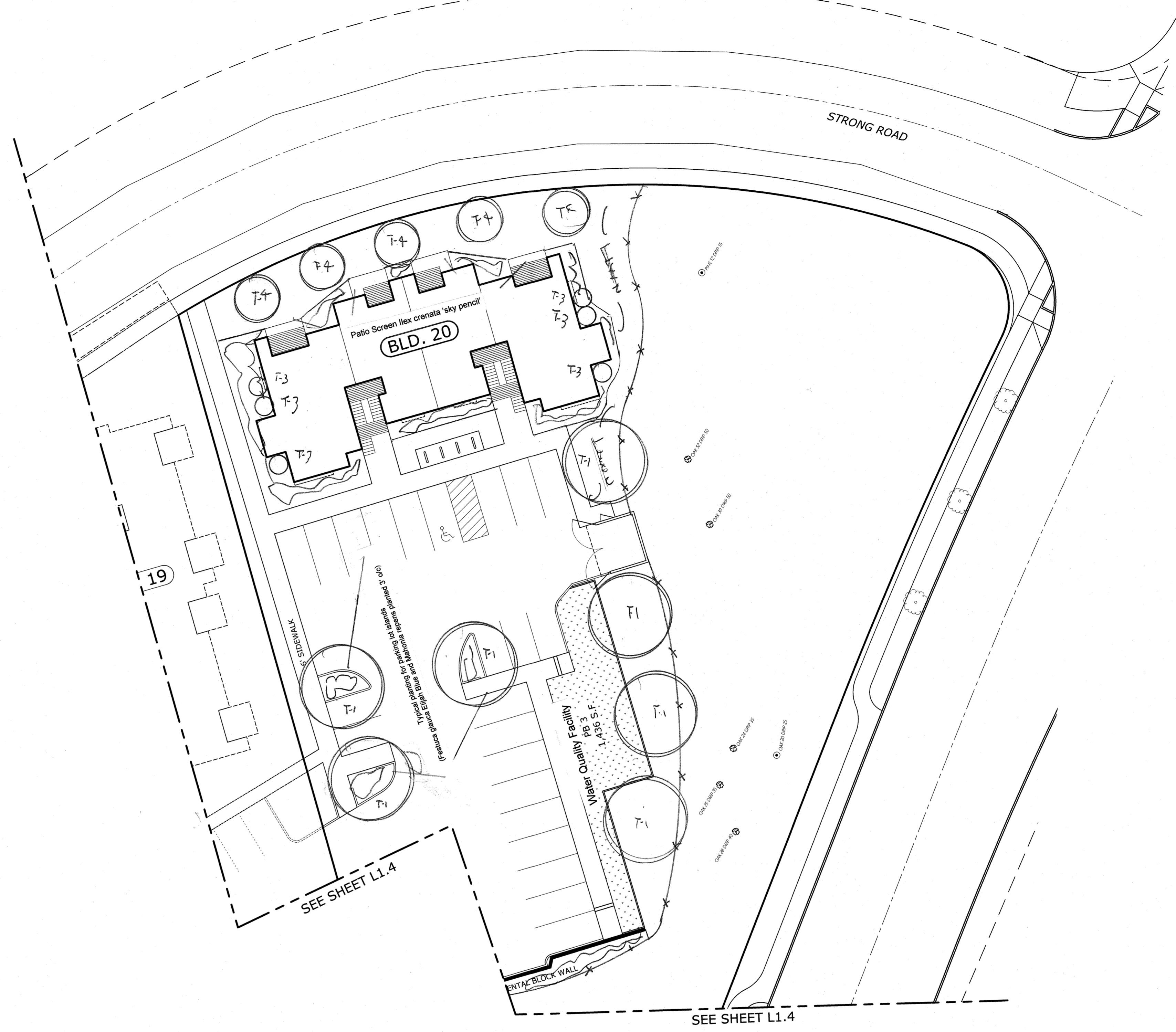
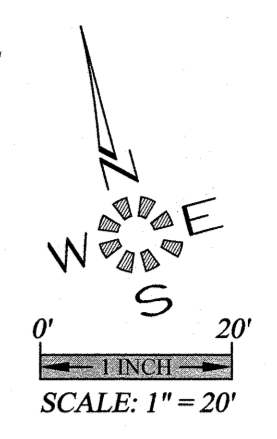
NO CHANGES, MODIFICATIONS  
 OR REPRODUCTIONS TO BE  
 MADE TO THESE DRAWINGS  
 WITHOUT WRITTEN  
 AUTHORIZATION FROM THE  
 DESIGN ENGINEER.  
 DIMENSIONS & NOTES TAKE  
 PRECEDENCE OVER  
 GRAPHICAL REPRESENTATION.

7111P L1.3  
 Design: M.D.G.  
 Drawn: C.D.S.  
 Checked: B.M.G.  
 Date: 06-30-2021  
 Scale: AS SHOWN



JOB # 7111

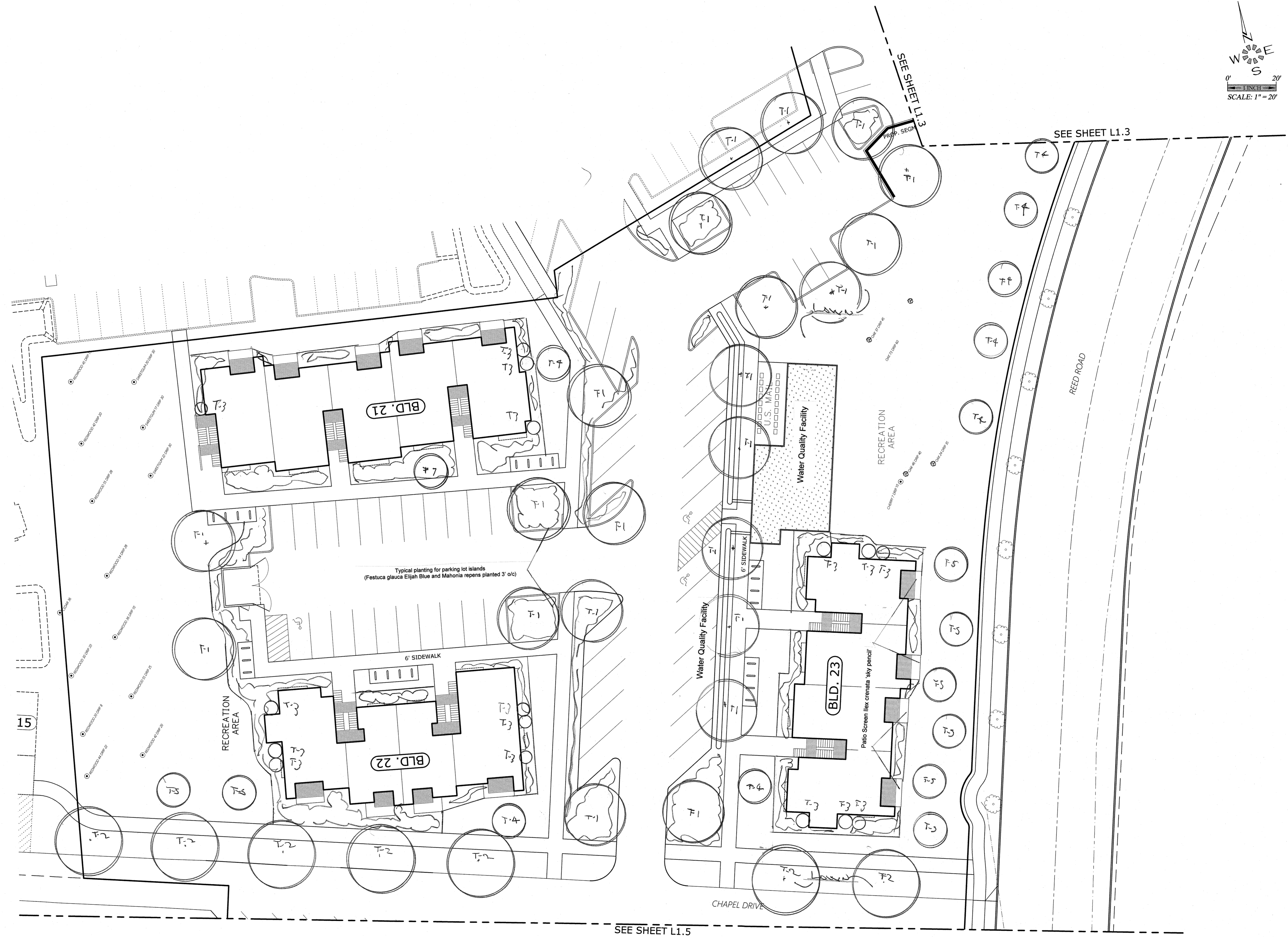
**L1.3**



**PERMANENT UNDERGROUND IRRIGATION SYSTEM WILL BE INSTALLED**

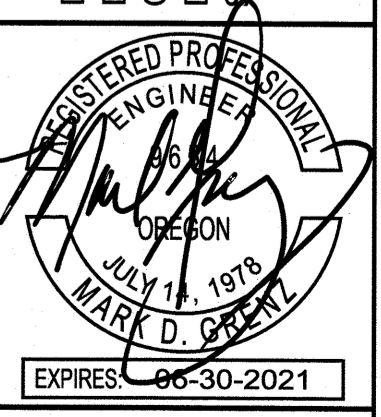
LANDSCAPE & IRRIGATION DESIGN BY:  
**LANDSCAPE DESIGN  
 &  
 CONSULTANTS LLC**  
 Doing business since 1985  
 620 WORMWOOD ST. S.E. SALEM, OR. 97306 PHONE: (503) 551-8590

J:\V1\1111-1111-1111-1111.dwg 20/11/11 10:45:20 AM 5/7/2021 3:24:02 PM 0/0/0



PERMENANT UNDERGROUND IRRIGATION SYSTEM WILL BE INSTALLED

LANDSCAPE & IRRIGATION DESIGN BY:  
**LANDSCAPE DESIGN & CONSULTANTS LLC**  
 Doing business since 1985  
 620 WORMWOOD ST. S.E. SALEM, OR 97306 PHONE: (503) 551-8590



7111 014  
 Design: M.D.G.  
 Drawn: C.D.S.  
 Checked: B.M.G.  
 Date: 06-30-2021  
 Scale: AS SHOWN

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### THE GROVE AT FAIRVIEW 2

### SCHEMATIC LANDSCAPE PLAN (EAST)

**MULTI/TECH**  
 ENGINEERING SERVICES, INC.  
 1185 13th ST. S.E. SALEM, OR. 97302  
 PH. (503) 363-9227 FAX (503) 364-1260  
 www.mtengineering.net office@mtengineering.net

JOB # 7111

**L1.4**

**SCHEMATIC LANDSCAPE PLAN  
 (EAST)**

**THE GROVE AT FAIRVIEW 2**

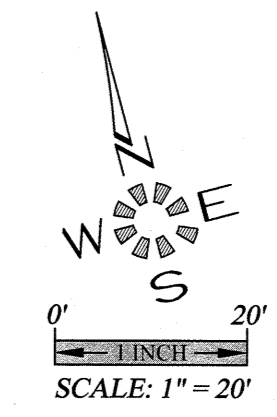
NO CHANGES, MODIFICATIONS  
 OR REPRODUCTIONS TO BE  
 MADE TO THESE DRAWINGS  
 WITHOUT THE WRITTEN  
 AUTHORIZATION OF THE  
 DESIGN ENGINEER.  
 DIMENSIONS & NOTES TAKE  
 PRECEDENCE OVER  
 GRAPHICAL REPRESENTATION.

7111P L1.5  
 Design: M.D.G.  
 Drawn: C.D.S.  
 Checked: B.M.G.  
 Date: \_\_\_\_\_  
 Scale: AS SHOWN



JOB # 7111

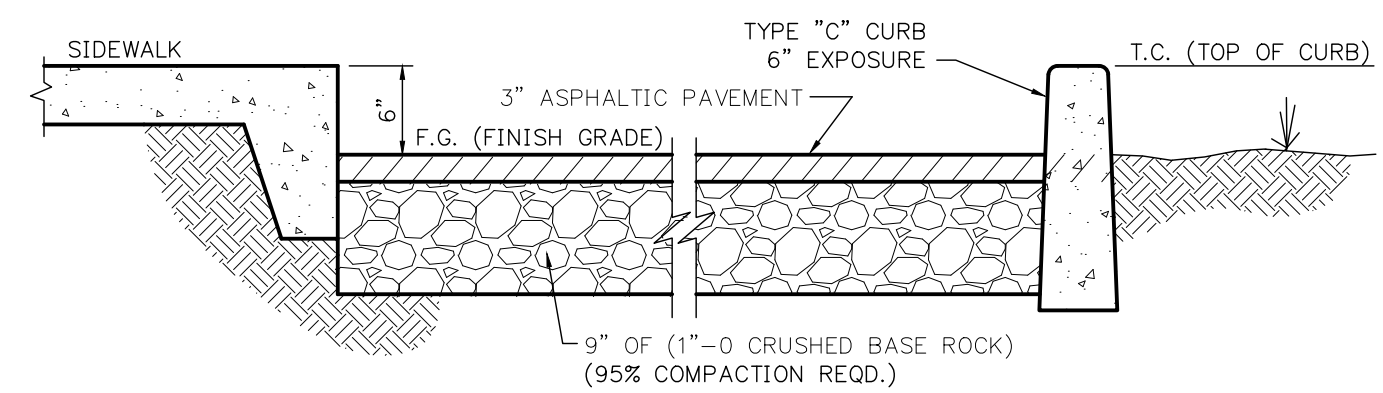
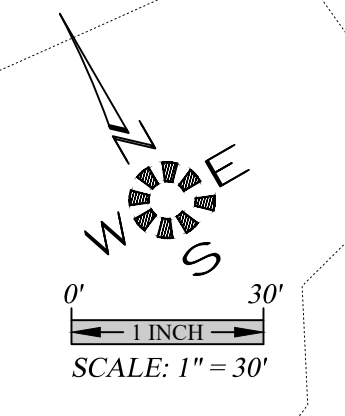
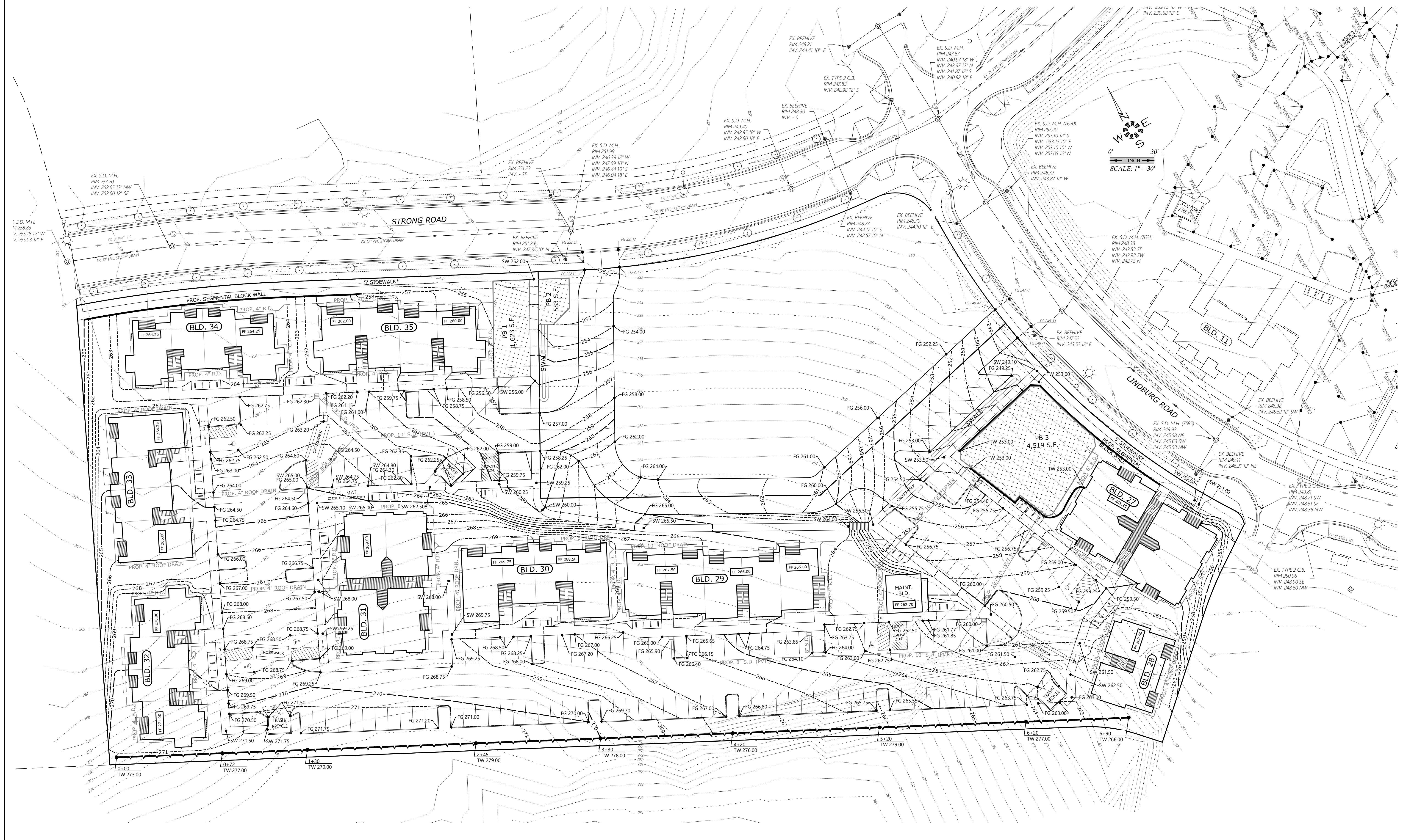
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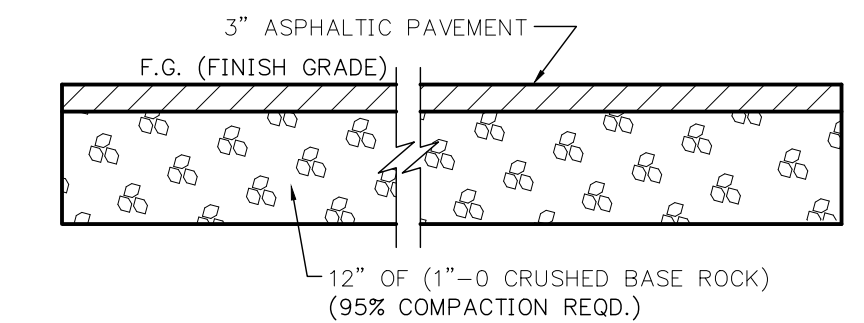
LANDSCAPE & IRRIGATION DESIGN BY:  
**LANDSCAPE DESIGN  
 &  
 CONSULTANTS LLC**  
 Doing business since 1985  
 620 WORNWOOD ST. S.E. SALEM, OR. 97306 PHONE: (503) 551-8590

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TYPICAL SIDEWALK AND PARKING SECTION  
-NTS-



TYPICAL PRIVATE ROAD AND DRIVE AISLE SECTION  
-NTS-

NO CHANGES, MODIFICATIONS  
OR REPRODUCTIONS TO BE  
MADE TO THESE DRAWINGS  
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AUTHORIZATION FROM THE  
DESIGN ENGINEER.  
DIMENSIONS & NOTES TAKE  
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GRAPHICAL REPRESENTATION.

7/11P 58016-000  
Design: M.D.G.  
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Checked: B.M.G.  
Date: 06/30/2021  
Scale: AS SHOWN



EXPIRES: 06-30-2021  
JOB # 7111

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**RETAINING WALL DETAILS**

**THE GROVE AT FAIRVIEW 2**

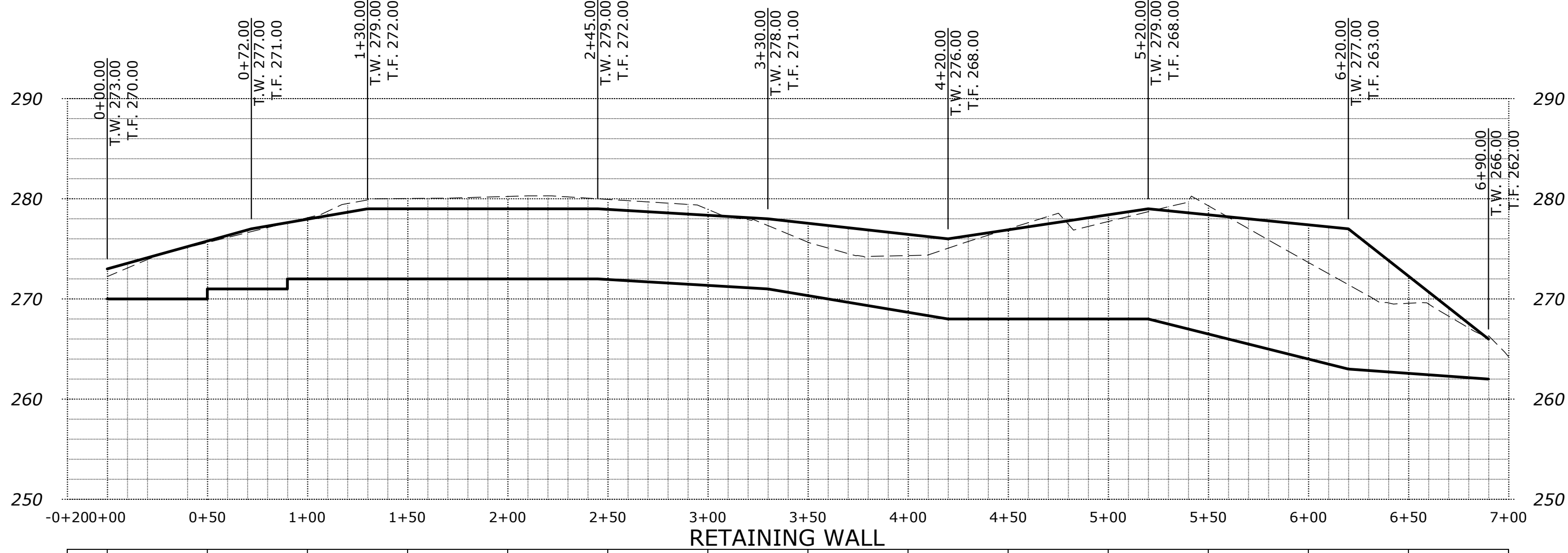
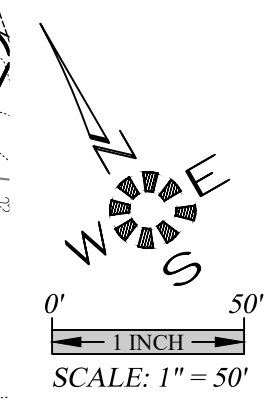
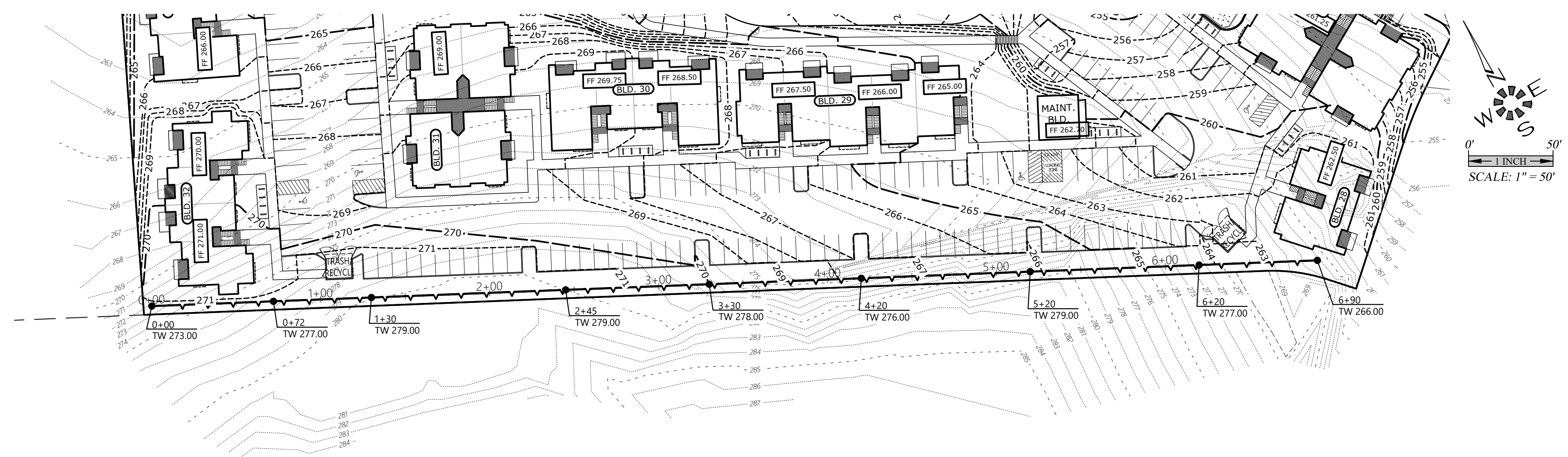
NO CHANGES, MODIFICATIONS OR REPRODUCTIONS TO BE MADE TO THESE DRAWINGS WITHOUT WRITTEN AUTHORIZATION FROM THE DESIGN ENGINEER.  
 DIMENSIONS & NOTES TAKE PRECEDENCE OVER GRAPHICAL REPRESENTATION.

711P SDR11-WALL  
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 Scale: AS SHOWN

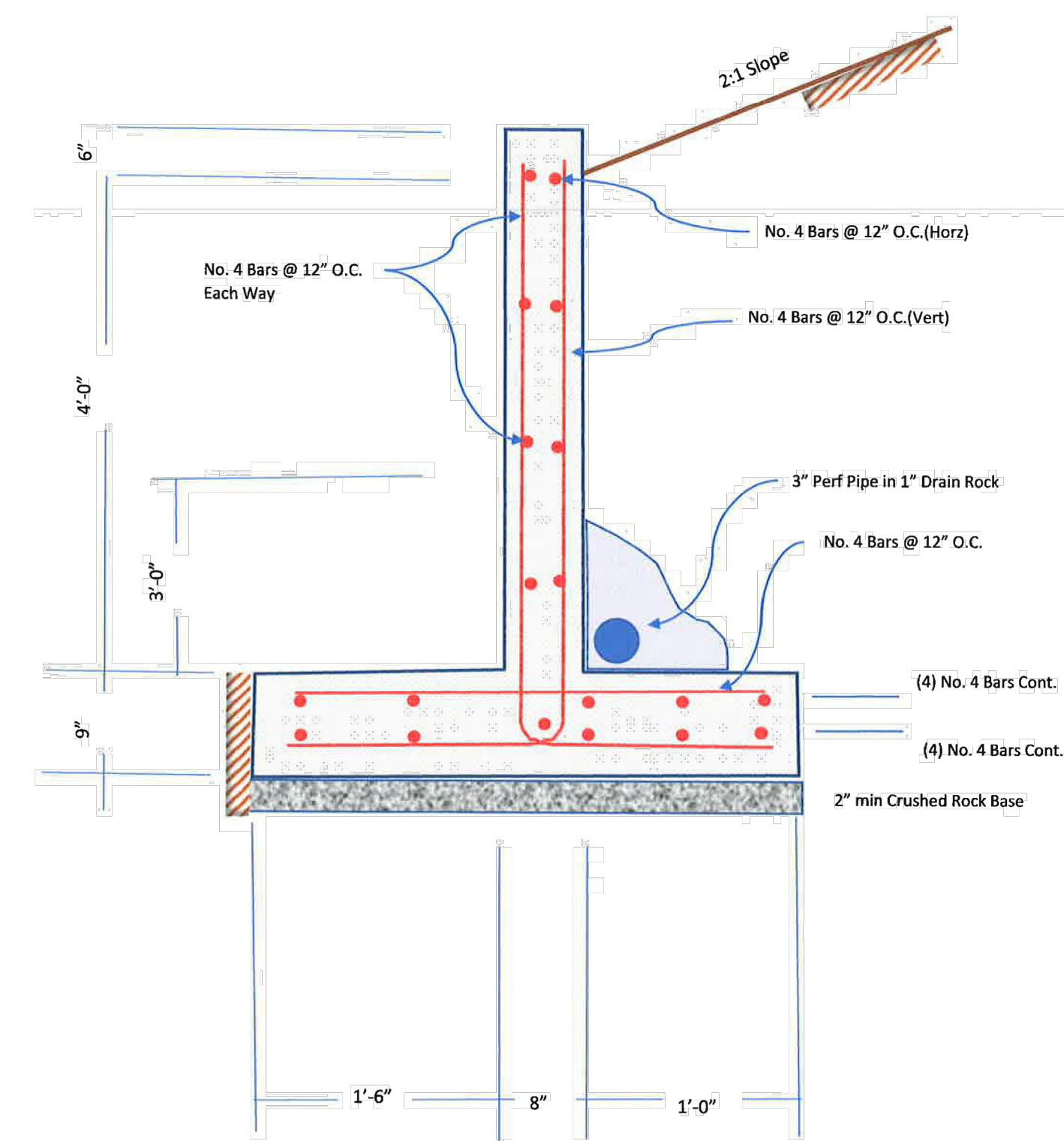


EXPIRES: 06-30-2021  
 JOB # 7111

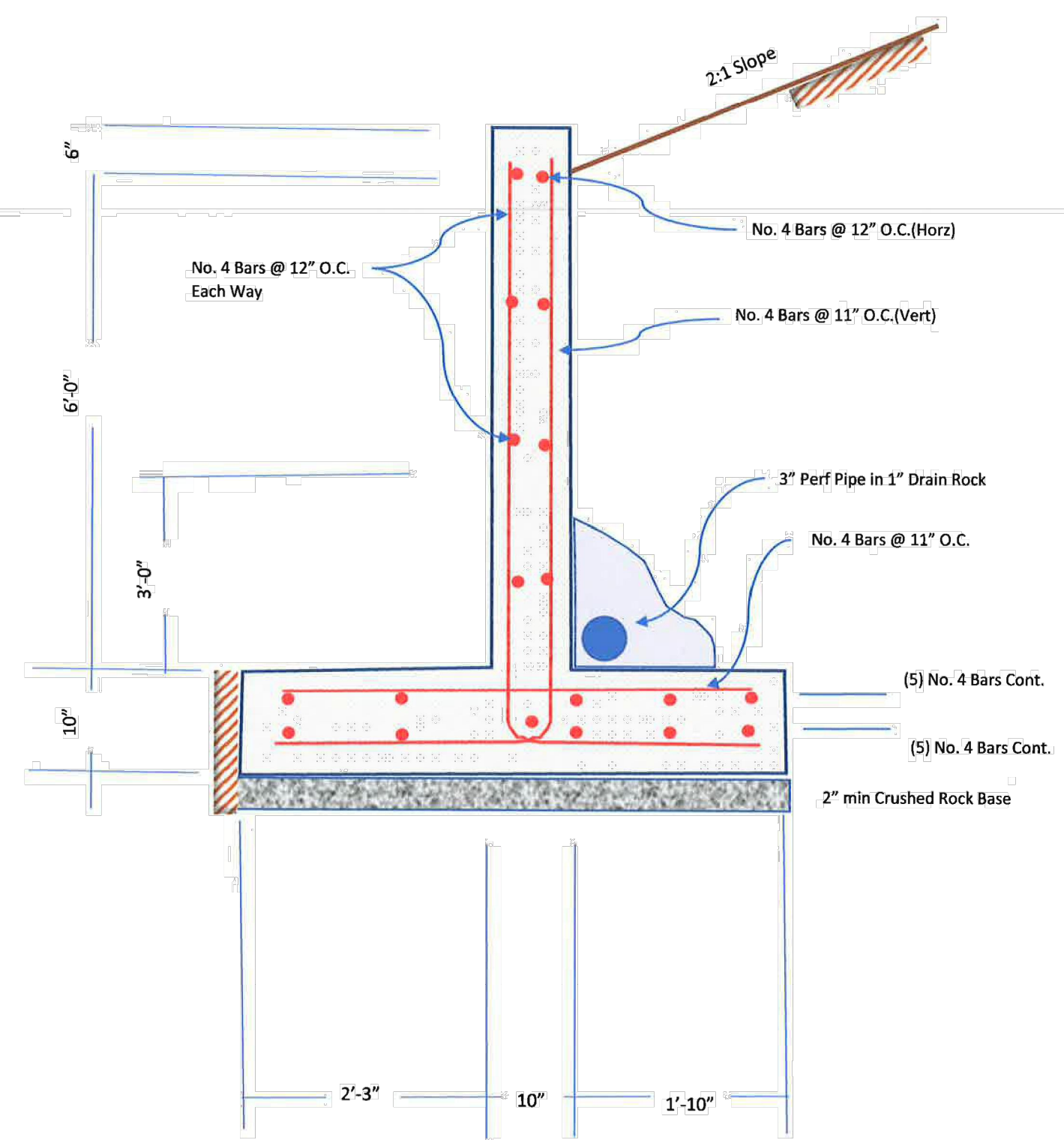
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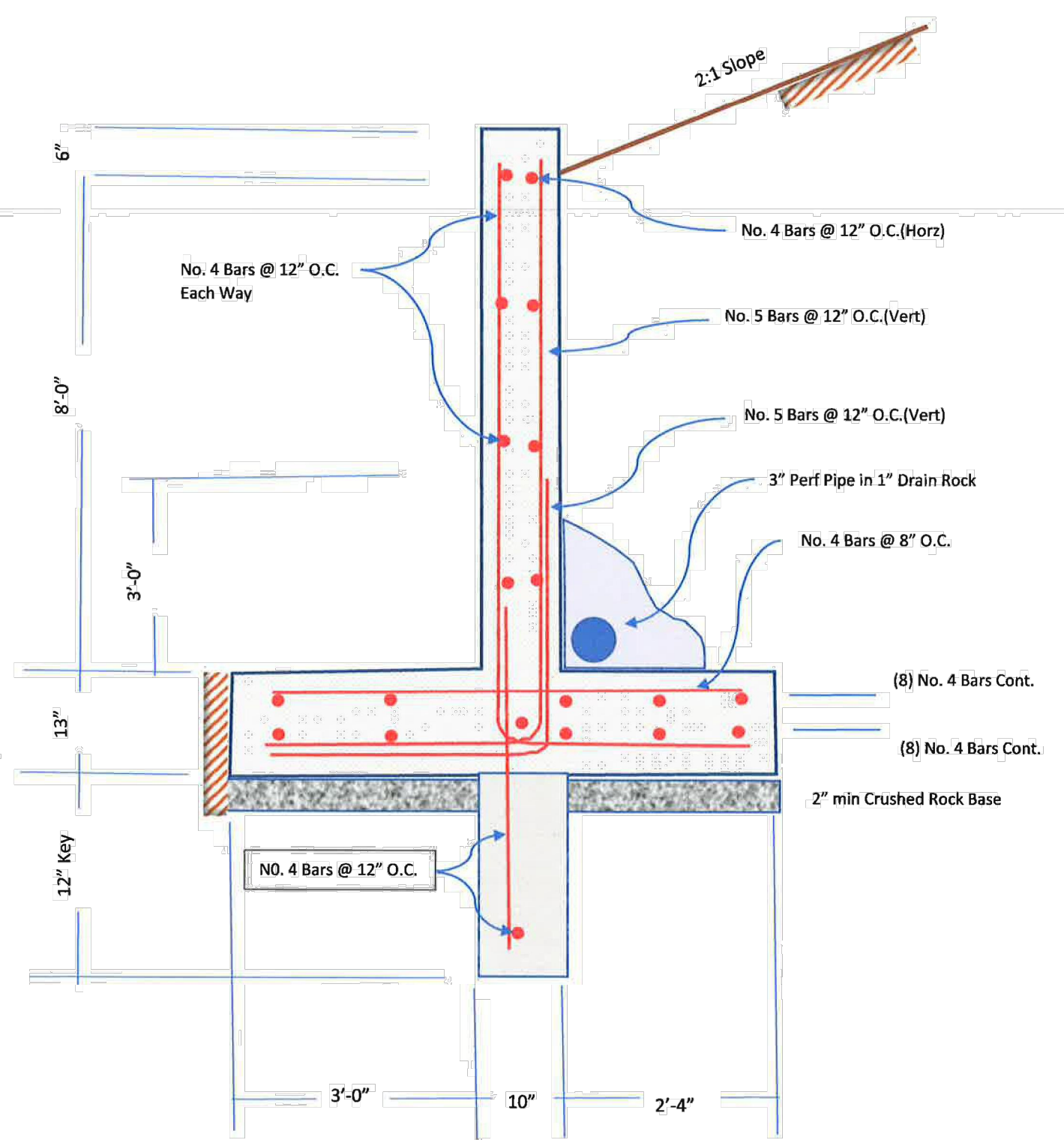
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	E.G. 275.8 C.L.	F.G. 275.78 C.L.
	E.G. 278.1 C.L.	F.G. 277.97 C.L.
	E.G. 280.9 C.L.	F.G. 279.00 C.L.
	E.G. 278.9 C.L.	F.G. 278.94 C.L.
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	E.G. 276.4 C.L.	F.G. 276.44 C.L.
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	E.G. 278.3 C.L.	F.G. 278.40 C.L.
	E.G. 277.4 C.L.	F.G. 277.40 C.L.
	E.G. 268.6 C.L.	F.G. 272.29 C.L.
	F.G. C.L.	



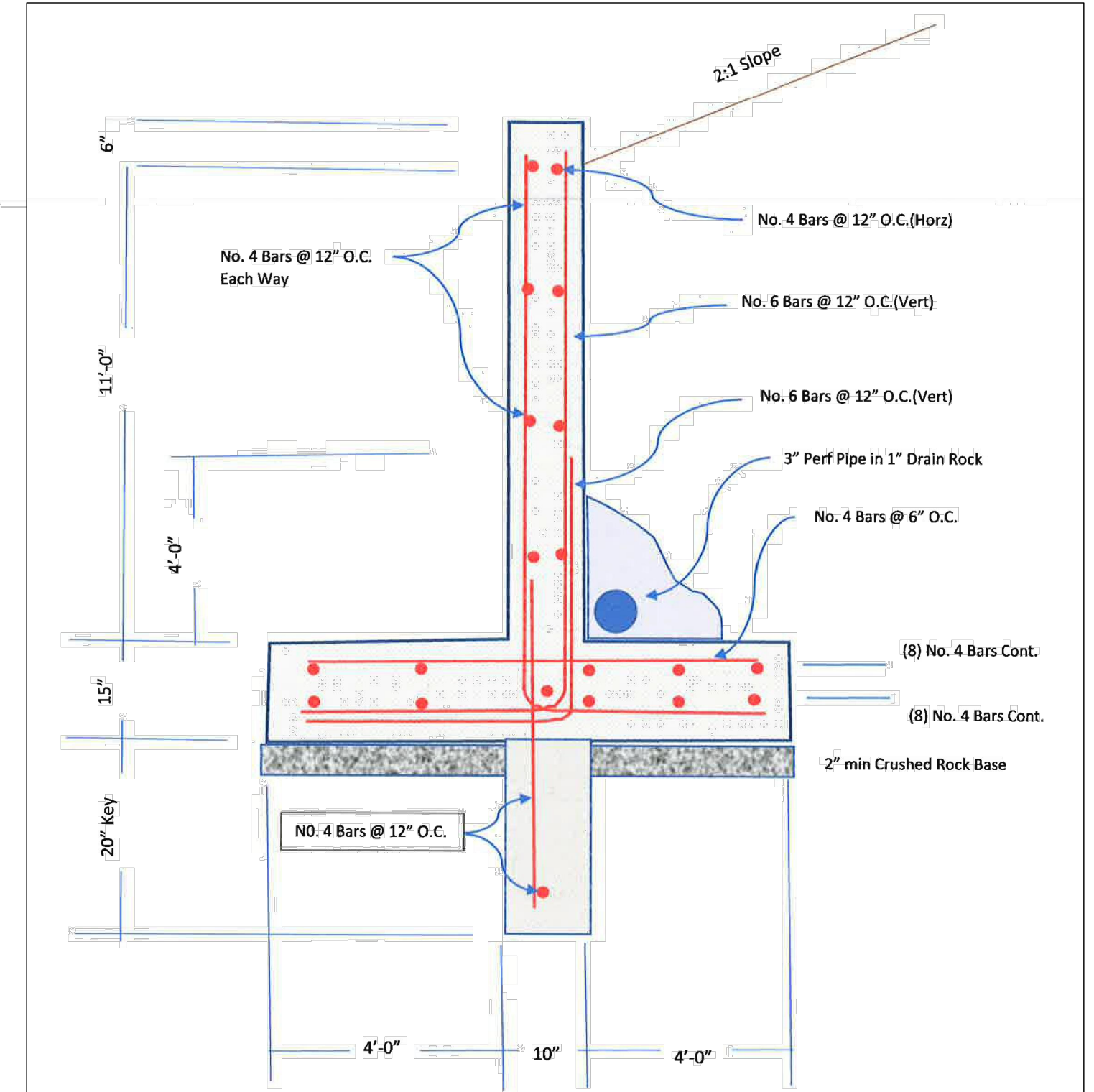
**4' RETAINING WALL**  
 -NTS-



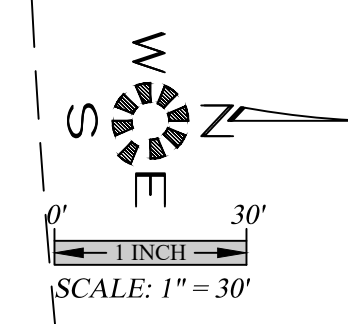
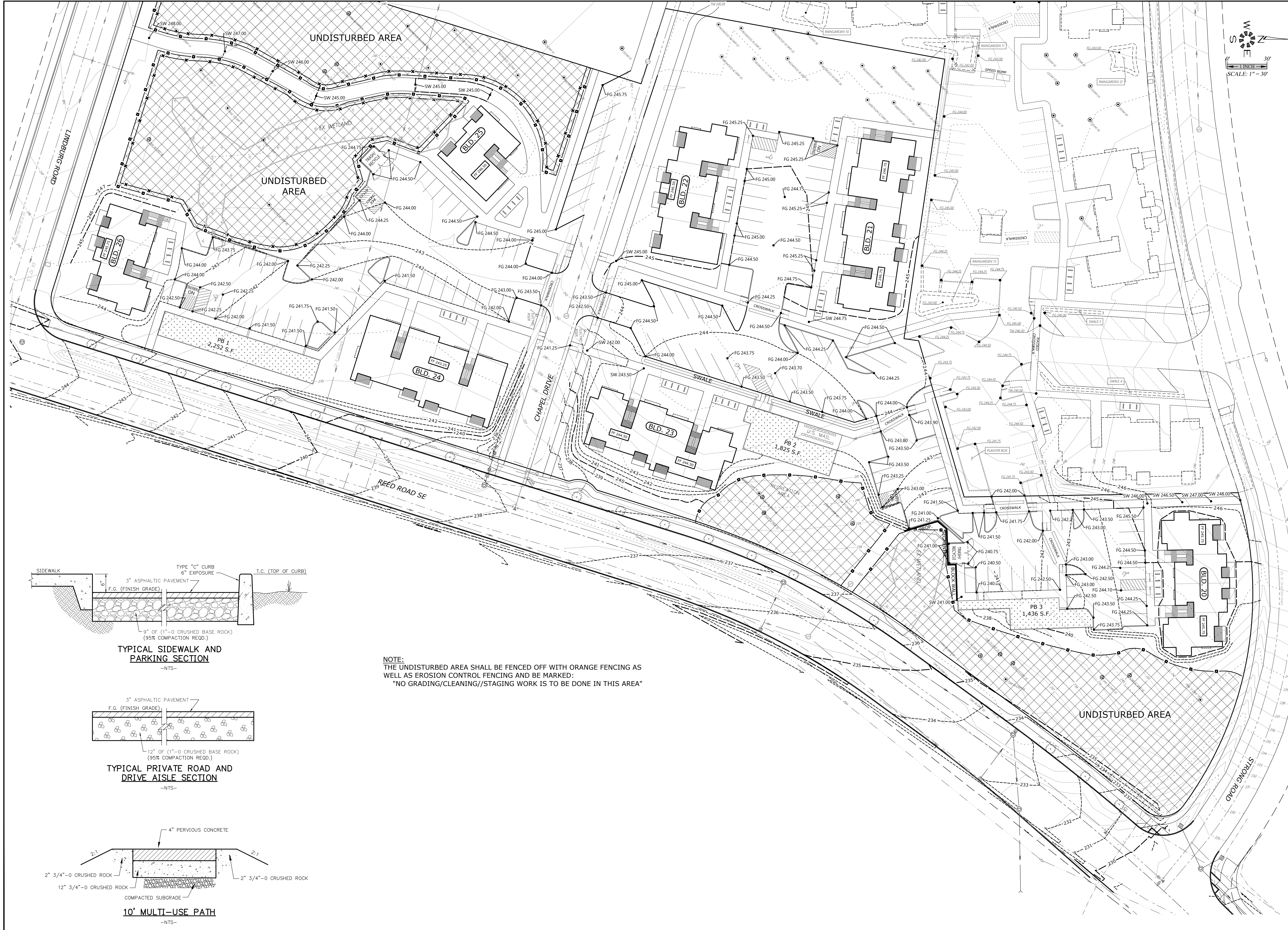
**6' RETAINING WALL**  
 -NTS-



**8' RETAINING WALL**  
 -NTS-



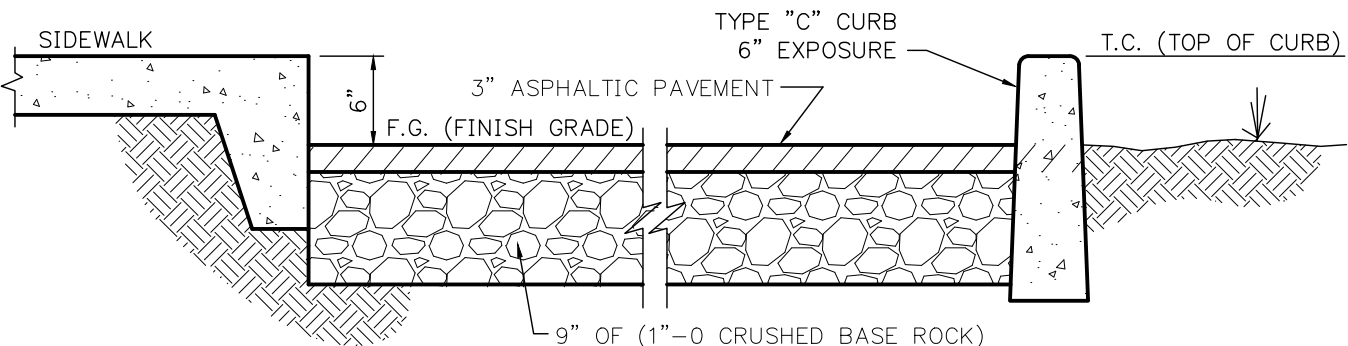
**11' RETAINING WALL**  
 -NTS-



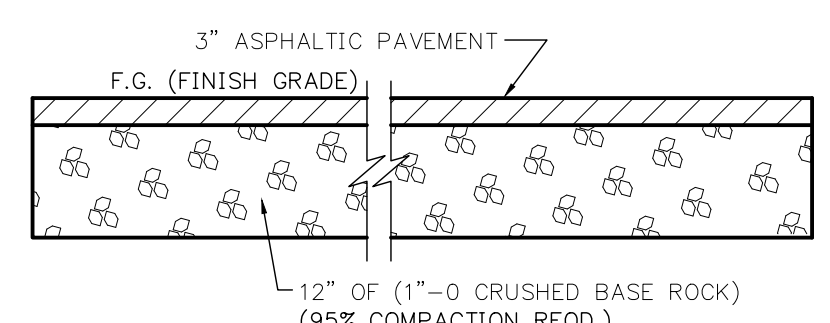
**MULTI/TECH**  
 ENGINEERING SERVICES, INC.  
 1155 13th St. S.E. Salem, OR 97302  
 PH: (503) 563-9227 FAX: (503) 364-1260  
 www.mtengineering.net office@mtengineering.net

**GRADING & DRAINAGE PLAN  
(WEST)**

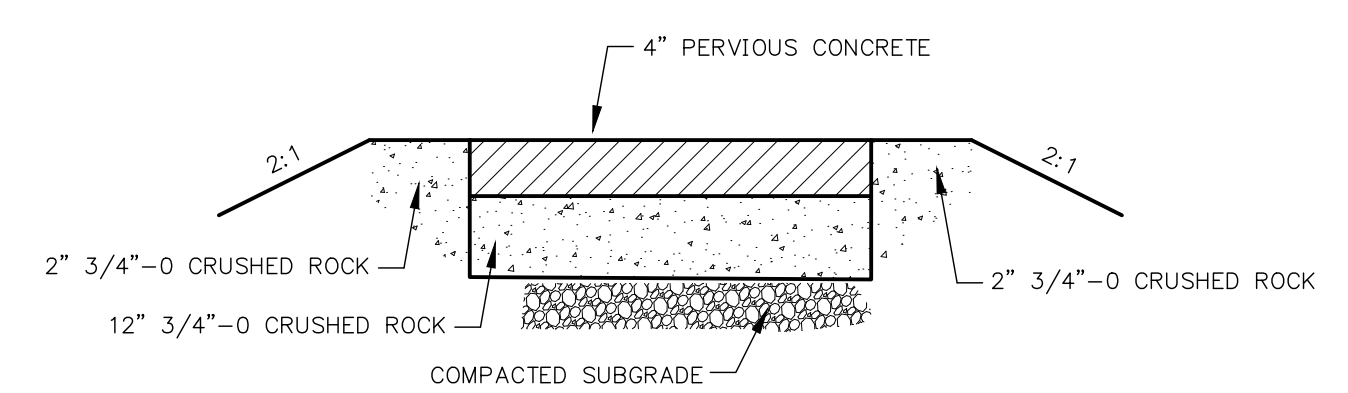
**THE GROVE AT FAIRVIEW 2**



**TYPICAL SIDEWALK AND PARKING SECTION**  
 -NTS-



**TYPICAL PRIVATE ROAD AND DRIVE AISLE SECTION**  
 -NTS-

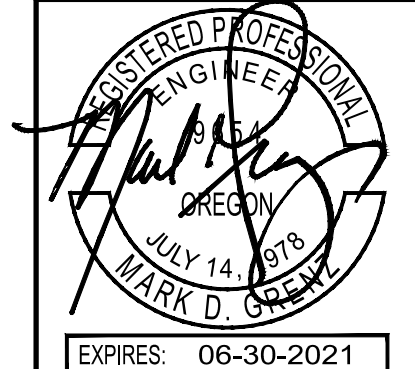


**10' MULTI-USE PATH**  
 -NTS-

**NOTE:**  
 THE UNDISTURBED AREA SHALL BE FENCED OFF WITH ORANGE FENCING AS WELL AS EROSION CONTROL FENCING AND BE MARKED.  
 "NO GRADING/CLEANING//STAGING WORK IS TO BE DONE IN THIS AREA"

NO CHANGES, MODIFICATIONS OR REPRODUCTIONS TO BE MADE TO THESE DRAWINGS WITHOUT WRITTEN AUTHORIZATION FROM THE DESIGN ENGINEER.  
 DIMENSIONS & NOTES TAKE PRECEDENCE OVER GRAPHICAL REPRESENTATION.

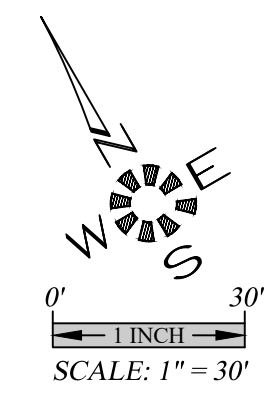
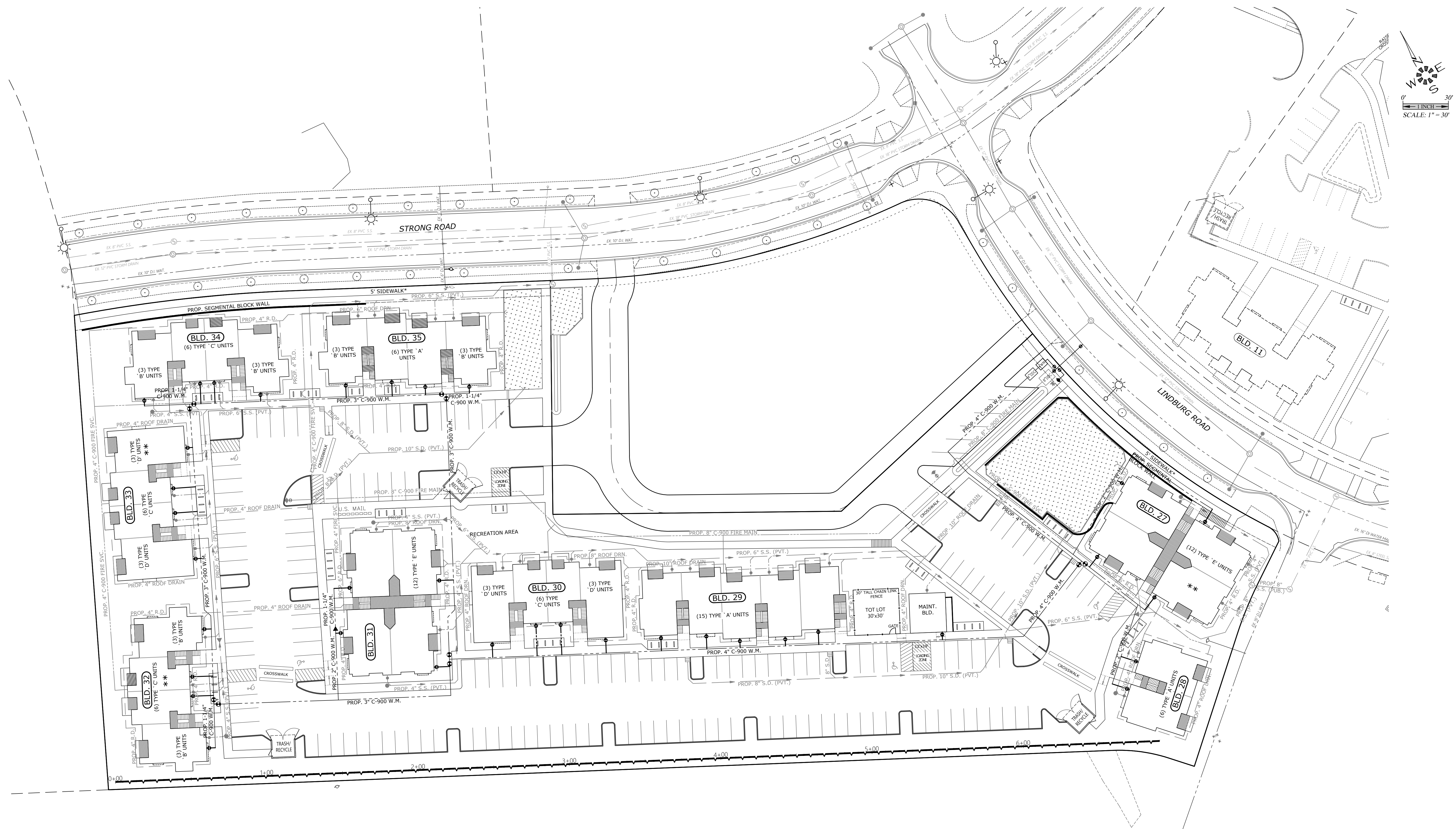
7111P 58012-080  
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 Checked: B.M.G.  
 Date: 7/14/2021  
 Scale: AS SHOWN



EXPIRES: 06-30-2021  
 JOB # 7111

**SDR12**

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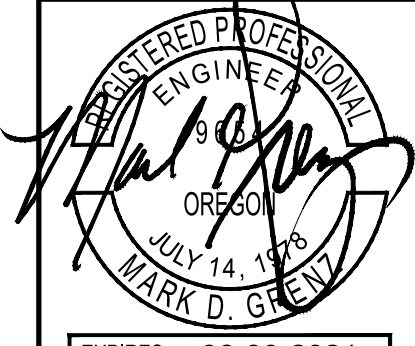


**PRIVATE DOMESTIC WATER &  
 FIRE SERVICE PLAN (WEST)**

**THE GROVE AT FAIRVIEW 2**

NO CHANGES, MODIFICATIONS  
 OR REPRODUCTIONS TO BE  
 MADE TO THESE DRAWINGS  
 WITHOUT WRITTEN  
 AUTHORIZATION FROM THE  
 DESIGN ENGINEER.  
 DIMENSIONS & NOTES TAKE  
 PRECEDENCE OVER  
 GRAPHICAL REPRESENTATION.

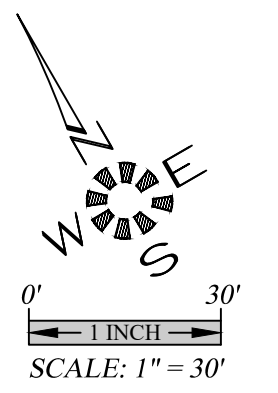
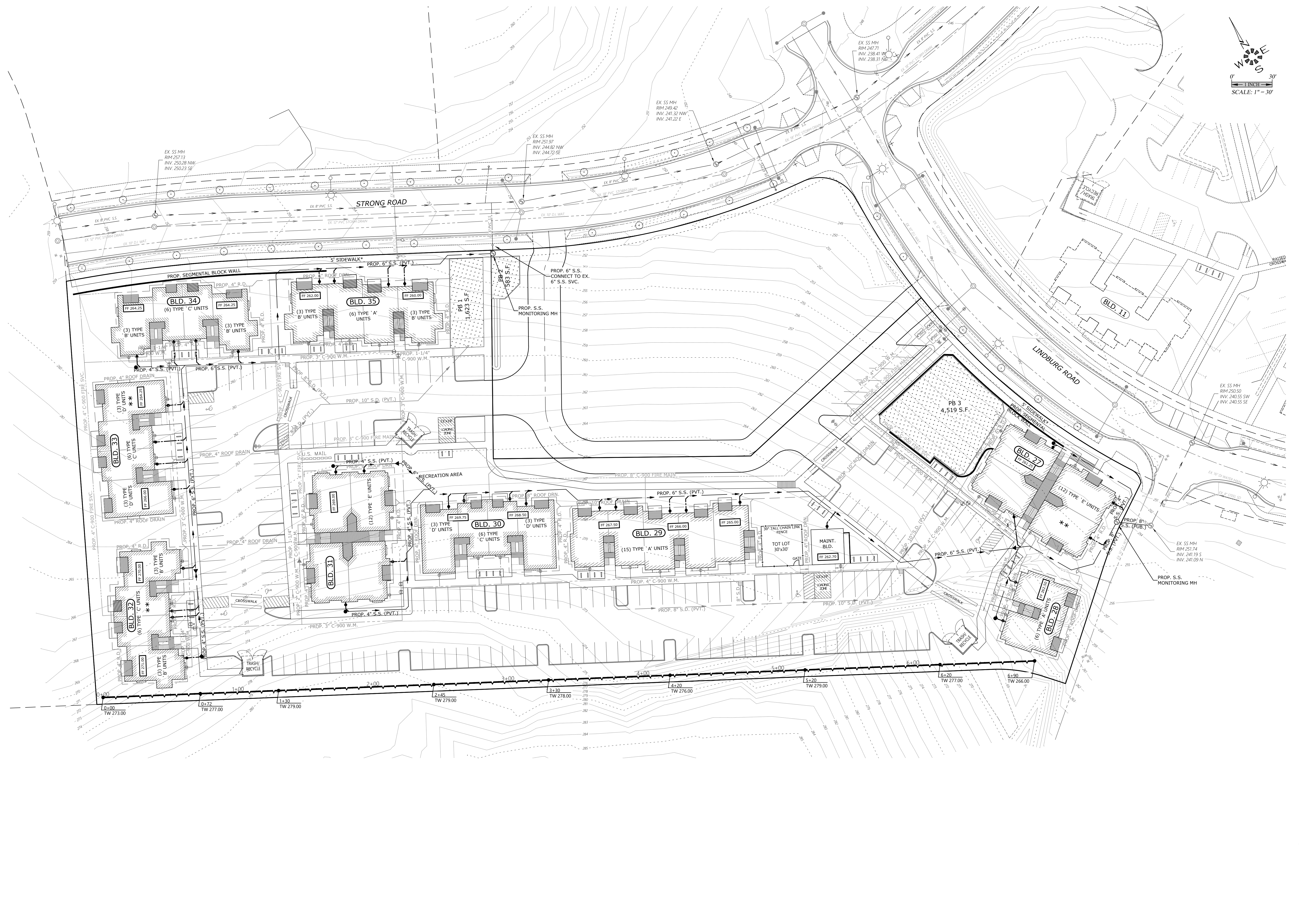
711P SDR15-WAT  
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 Drawn: C.D.S.  
 Checked: B.M.G.  
 Date: 7/14/17  
 Scale: AS SHOWN



EXPIRES: 06-30-2021  
 JOB # 7111

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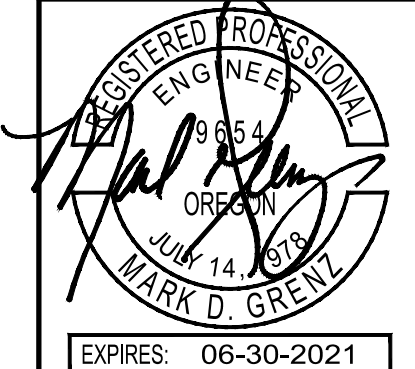


**PRIVATE SANITARY SEWER IMPROVEMENTS (WEST)**

**THE GROVE AT FAIRVIEW 2**

NO CHANGES, MODIFICATIONS OR REPRODUCTION TO BE MADE TO THESE DRAWINGS WITHOUT WRITTEN AUTHORIZATION FROM THE DESIGN ENGINEER.  
 DIMENSIONS & NOTES TAKE PRECEDENCE OVER GRAPHICAL REPRESENTATION.

711P 5041338  
 Design: M.D.G.  
 Drawn: C.D.S.  
 Checked: B.M.G.  
 Date: 06/14/2021  
 Scale: AS SHOWN



EXPIRES: 06-30-2021  
 JOB # 7111

**SDR13**

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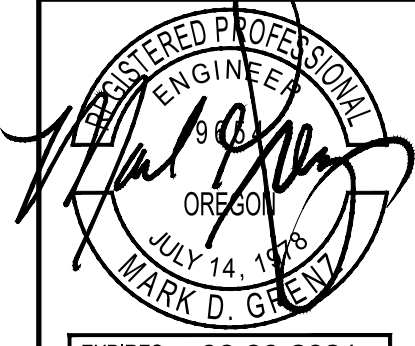


**PRIVATE DOMESTIC WATER &  
FIRE SERVICE PLAN (EAST)**

**THE GROVE AT FAIRVIEW 2**

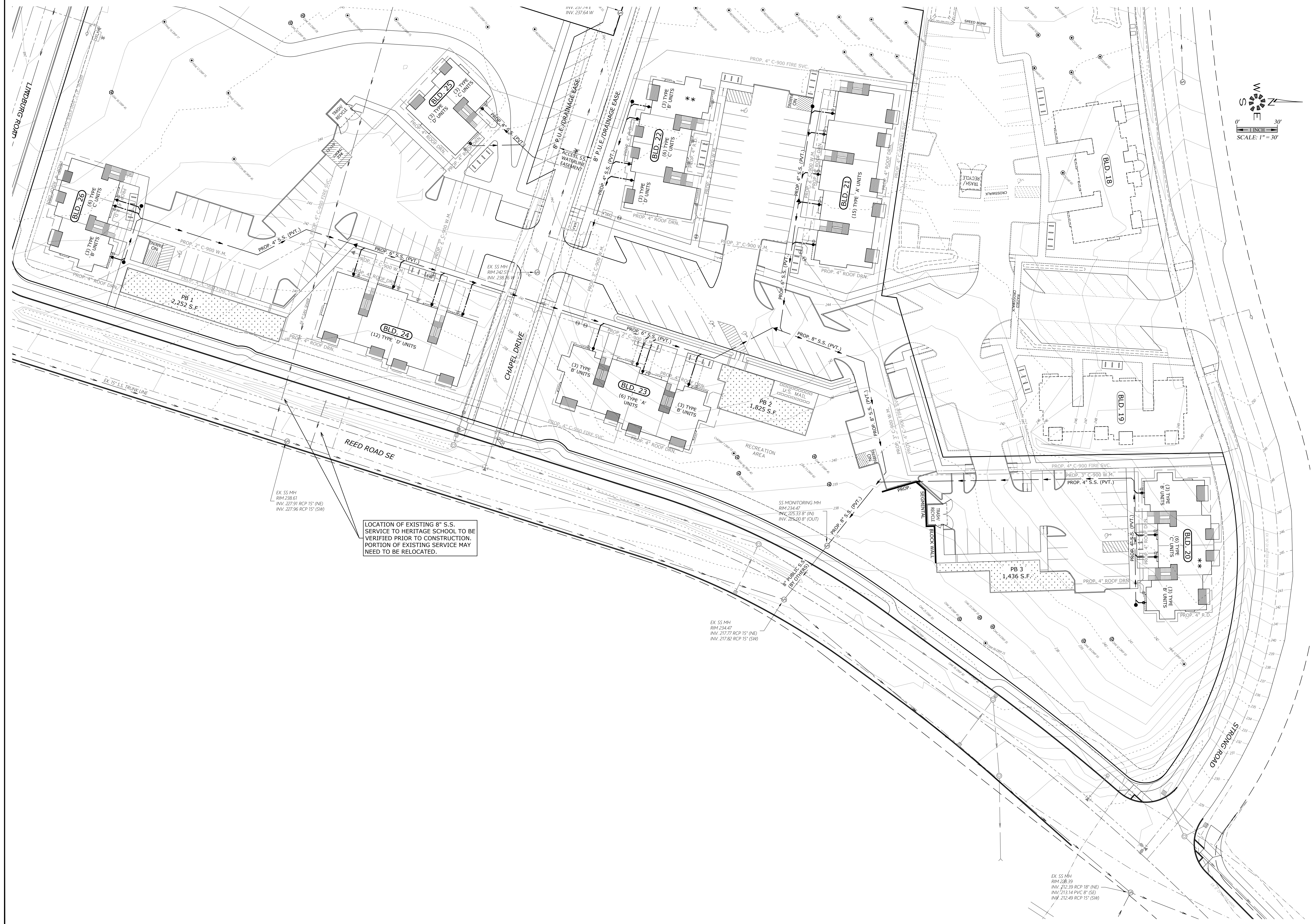
NO CHANGES, MODIFICATIONS  
OR REPRODUCTIONS TO BE  
MADE TO THESE DRAWINGS  
WITHOUT WRITTEN  
AUTHORIZATION FROM THE  
DESIGN ENGINEER.  
DIMENSIONS & NOTES TAKE  
PRECEDENCE OVER  
GRAPHICAL REPRESENTATION.

7111P SDR18.WAT  
Design: M.D.G.  
Drawn: C.D.S.  
Checked: B.M.G.  
Date: \_\_\_\_\_  
Scale: AS SHOWN

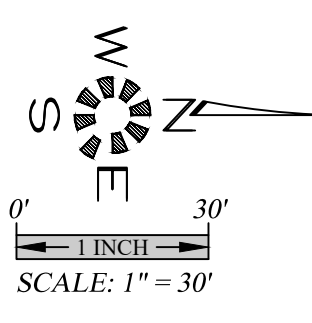


EXPIRES: 06-30-2021  
JOB # 7111

**SDR18**



LOCATION OF EXISTING 8" S.S. SERVICE TO HERITAGE SCHOOL TO BE VERIFIED PRIOR TO CONSTRUCTION. PORTION OF EXISTING SERVICE MAY NEED TO BE RELOCATED.



**PRIVATE SANITARY SEWER IMPROVEMENTS (EAST)**

**THE GROVE AT FAIRVIEW 2**

NO CHANGES, MODIFICATIONS OR REPRODUCTIONS TO BE MADE TO THESE DRAWINGS WITHOUT WRITTEN AUTHORIZATION FROM THE DESIGN ENGINEER.  
 DIMENSIONS & NOTES TAKE PRECEDENCE OVER GRAPHICAL REPRESENTATION.

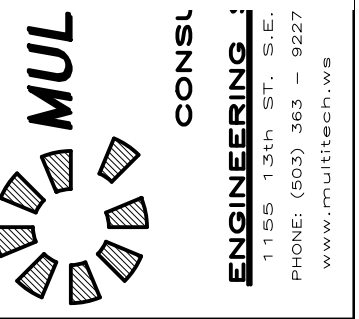
711P SDR14-08  
 Design: M.D.G.  
 Drawn: C.D.S.  
 Checked: B.M.G.  
 Date: JULY 14, 1978  
 Scale: AS SHOWN



EXPIRES: 06-30-2021  
 JOB # 7111

J:\7111\1111 - Private\sdm\sdm.dwg (20) 7/11/98 4:55:57 PM 0/0/0

# Attachment D



## BUILDING ELEVATIONS

## THE GROVE AT FAIRVIEW 2

NO CHANGES, MODIFICATIONS OR REVISIONS TO BE MADE TO THIS DRAWING WITHOUT THE WRITTEN AUTHORIZATION FROM THE DESIGN ENGINEER.  
DIMENSIONS & NOTES TAKE PRECEDENCE OVER GRAPHICAL REPRESENTATION.

Design: P.L.M.  
Drawn: G.L.D.  
Checked: M.D.G.  
Date: Dec-20  
Scale: AS SHOWN

JOB # 7111



**A4.80**



REAR ELEVATION (BLD. 20.32+34)  
SCALE: 1/8" = 1'-0"



SIDE ELEVATION (BLD. 20.32+34)  
SCALE: 1/8" = 1'-0"



FRONT ELEVATION (BLD. 20.32+34)  
SCALE: 1/8" = 1'-0"



SIDE ELEVATION (BLD. 20.32+34)  
SCALE: 1/8" = 1'-0"



REAR ELEVATION (TYPE A UNITS)  
SCALE: 1/8" = 1'-0"  
BLD. 21+29



END ELEVATION (TYPE A UNITS)  
SCALE: 1/8" = 1'-0"  
BLD. 21+29



FRONT ELEVATION (TYPE A UNITS)  
SCALE: 1/8" = 1'-0"  
BLD. 21+29

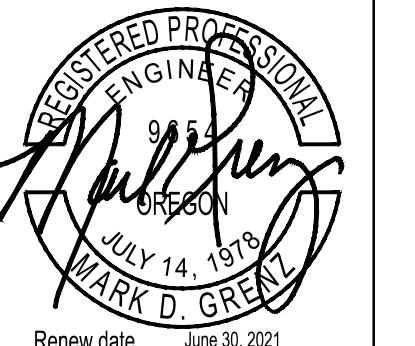


END ELEVATION (TYPE A UNITS)  
SCALE: 1/8" = 1'-0"  
BLD. 21+29

NO CHANGES, MODIFICATIONS OR REVISIONS TO BE MADE TO THIS DRAWING WITHOUT THE AUTHORIZATION FROM THE DESIGN ENGINEER.  
DIMENSIONS & NOTES TAKE PRECEDENCE OVER GRAPHICAL REPRESENTATION.

Design: P.L.M.  
Drawn: G.L.D.  
Checked: M.D.G.  
Date: Dec-20  
Scale: AS SHOWN

JOB # 7111



Renew date: June 30, 2021



TYPE B UNIT | TYPE C UNITS | TYPE D UNIT

**SOUTH ELEVATION (BLD. 22)**  
SCALE: 1/8" = 1'-0"



TYPE D UNITS

**EAST ELEVATION (BLD. 22)**  
SCALE: 1/8" = 1'-0"



TYPE D UNIT | TYPE C UNITS | TYPE B UNIT

**NORTH ELEVATION (BLD. 22)**  
SCALE: 1/8" = 1'-0"



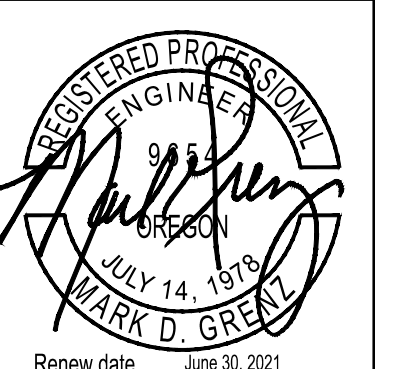
TYPE B UNITS

**WEST ELEVATION (BLD. 22)**  
SCALE: 1/8" = 1'-0"

NO CHANGES, MODIFICATIONS OR REVISIONS TO BE MADE TO THIS DRAWING WITHOUT THE AUTHORIZATION FROM THE DESIGN ENGINEER.  
DIMENSIONS & NOTES TAKE PRECEDENCE OVER GRAPHICAL REPRESENTATION.

DESIGN: P.L.M. / A.B. REBEV  
DRAWN: G.L.D.  
CHECKED: M.D.G.  
DATE: Apr-21  
SCALE: AS SHOWN

JOB # 7111



Renew date: June 30, 2021

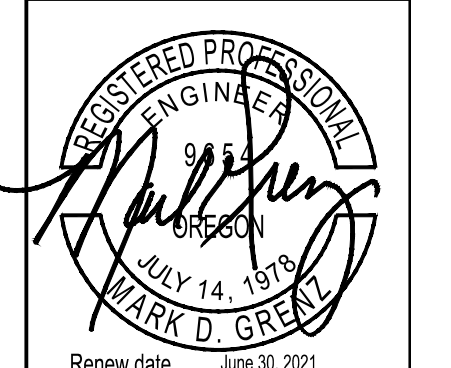
**BUILDING ELEVATIONS**

**THE GROVE AT FAIRVIEW 2**

NO CHANGES, MODIFICATIONS OR REVISIONS TO BE MADE TO THIS DRAWING WITHOUT THE AUTHORIZATION FROM THE DESIGN ENGINEER.  
 DIMENSIONS & NOTES TAKE PRECEDENCE OVER GRAPHICAL REPRESENTATION.

BLD-12-206-PR-011  
 ALBRELEY  
 Design: P.L.M.  
 Drawn: G.L.D.  
 Checked: M.D.G.  
 Date: Dec-20  
 Scale: AS SHOWN

JOB # 7111

REGISTERED PROFESSIONAL ENGINEER  
  
 MARK D. GREIG  
 Renew date: June 30, 2021

**A3.80**



REAR ELEVATION (BLD. 23+35)  
 SCALE: 1/8" = 1'-0"



END ELEVATION (BLD. 23+35)  
 SCALE: 1/8" = 1'-0"



FRONT ELEVATION (BLD. 23+35)  
 SCALE: 1/8" = 1'-0"



END ELEVATION (BLD. 23+35)  
 SCALE: 1/8" = 1'-0"

J:\7111\7111-TheGrove\Fairview2\Org\_001\SRV\BLD-12-206.dwg, A3.80.dwg, 12/17/2020 1:51:55 PM, CDH/mkr

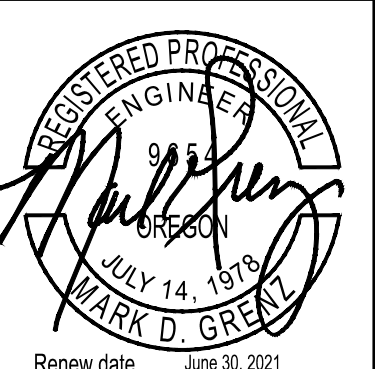
**BUILDING  
 ELEVATIONS**

**THE GROVE  
 AT FAIRVIEW 2**

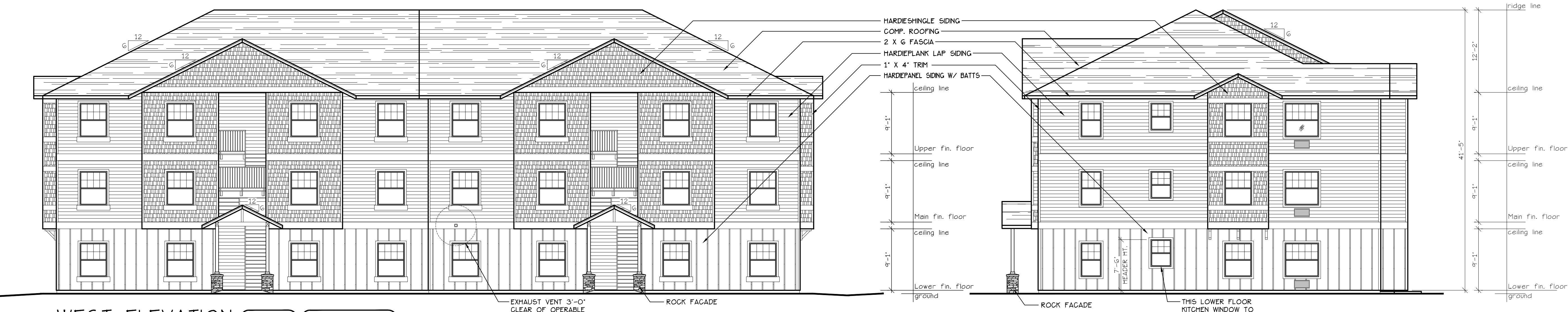
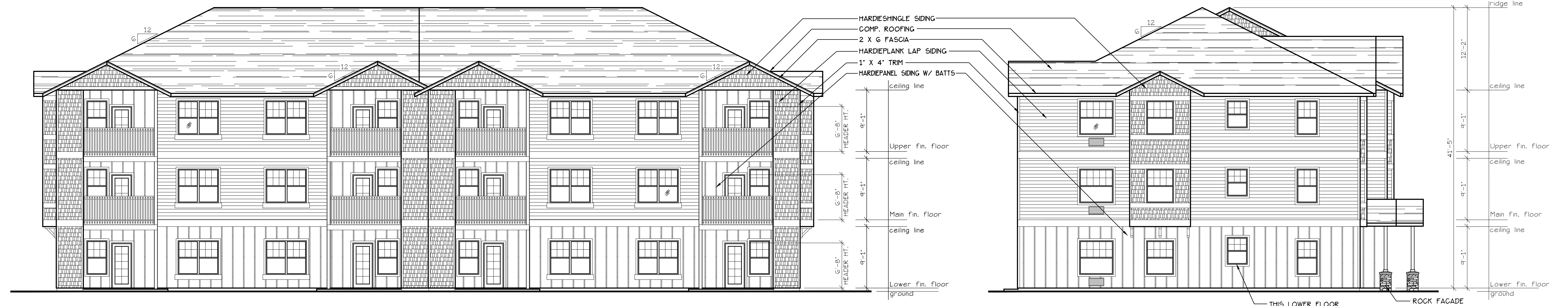
NO CHANGES, MODIFICATIONS OR  
 REVISIONS TO BE MADE TO  
 THIS DRAWING WITHOUT THE  
 AUTHORIZATION FROM THE DESIGN  
 ENGINEER.  
 DIMENSIONS & NOTES TAKE  
 PRECEDENCE OVER GRAPHICAL  
 REPRESENTATION.

8/20/22-2022/01/01  
 AT 8:45 PM  
 Design: P.L.M.  
 Drawn: G.L.D.  
 Checked: M.D.G.  
 Date: Apr-21  
 Scale: AS SHOWN

JOB # 7111



**A11.80**





**BUILDING ELEVATIONS**

**THE GROVE AT FAIRVIEW 2**

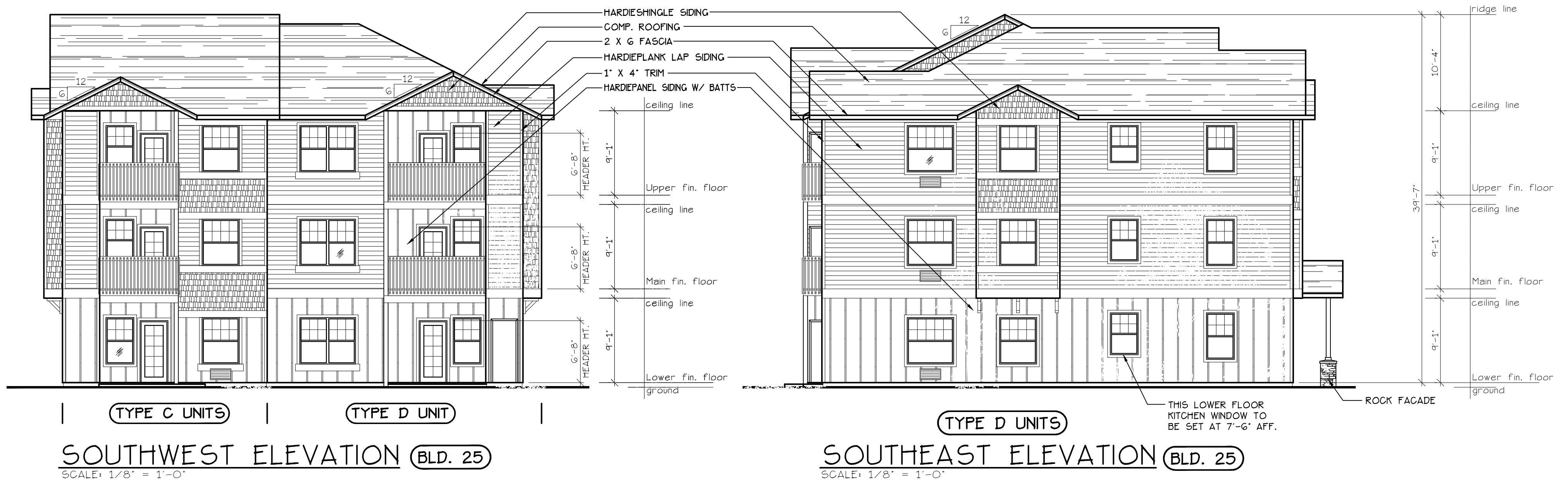
NO CHANGES, MODIFICATIONS OR REVISIONS TO BE MADE TO THIS DRAWING WITHOUT THE AUTHORIZATION FROM THE DESIGN ENGINEER.  
 DIMENSIONS & NOTES TAKE PRECEDENCE OVER GRAPHICAL REPRESENTATION.

DESIGN: P.L.M.  
 DRAWN: G.L.D.  
 CHECKED: M.D.G.  
 DATE: Dec-20  
 SCALE: AS SHOWN

JOB # 7111

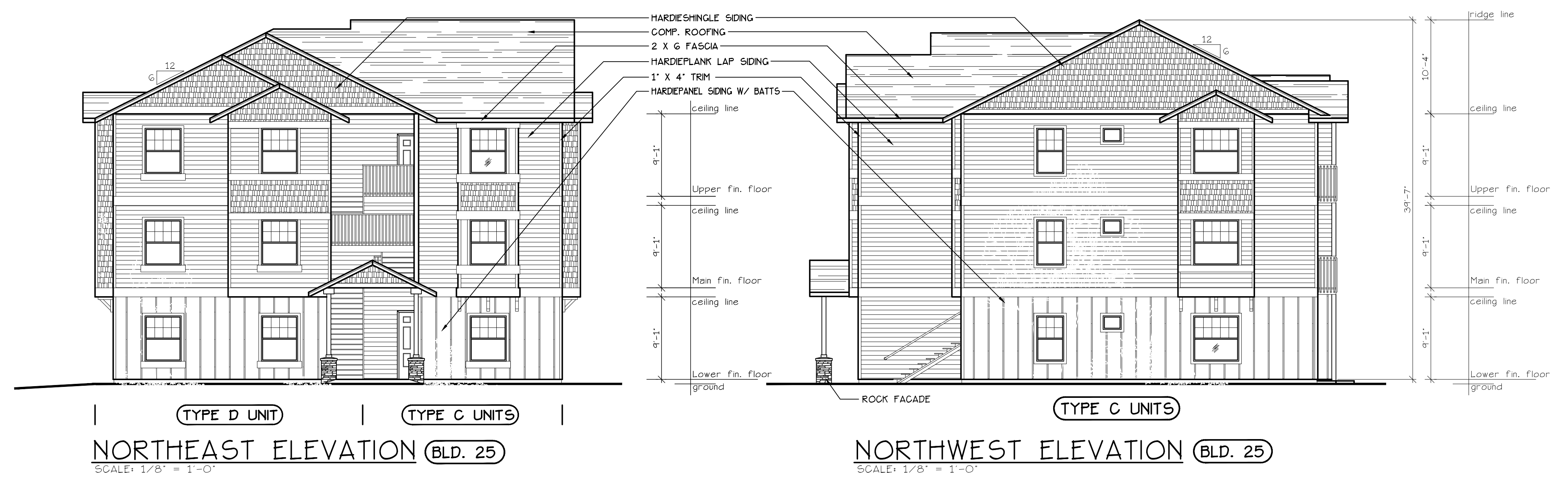


**A8.80**



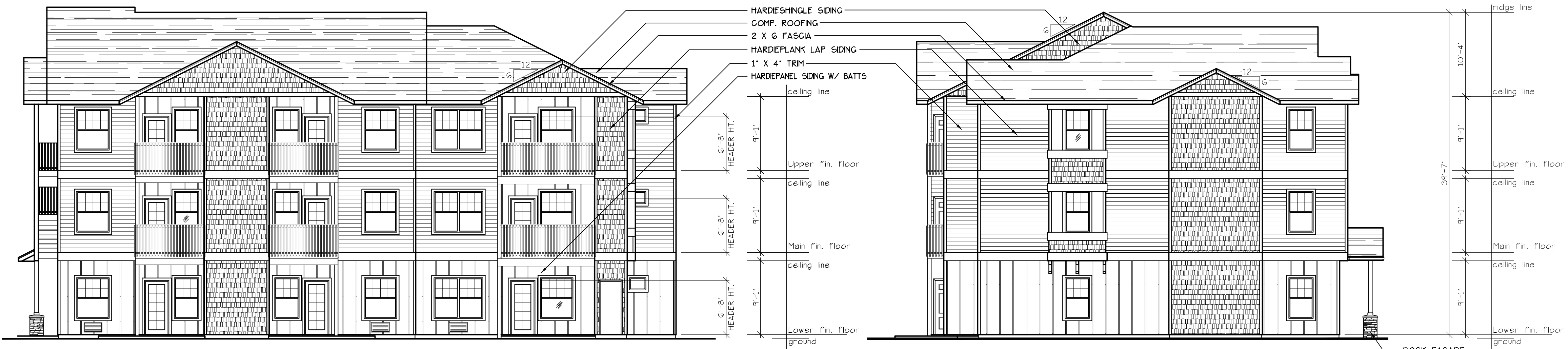
**SOUTHWEST ELEVATION (BLD. 25)**  
 SCALE: 1/8" = 1'-0"

**SOUTHEAST ELEVATION (BLD. 25)**  
 SCALE: 1/8" = 1'-0"



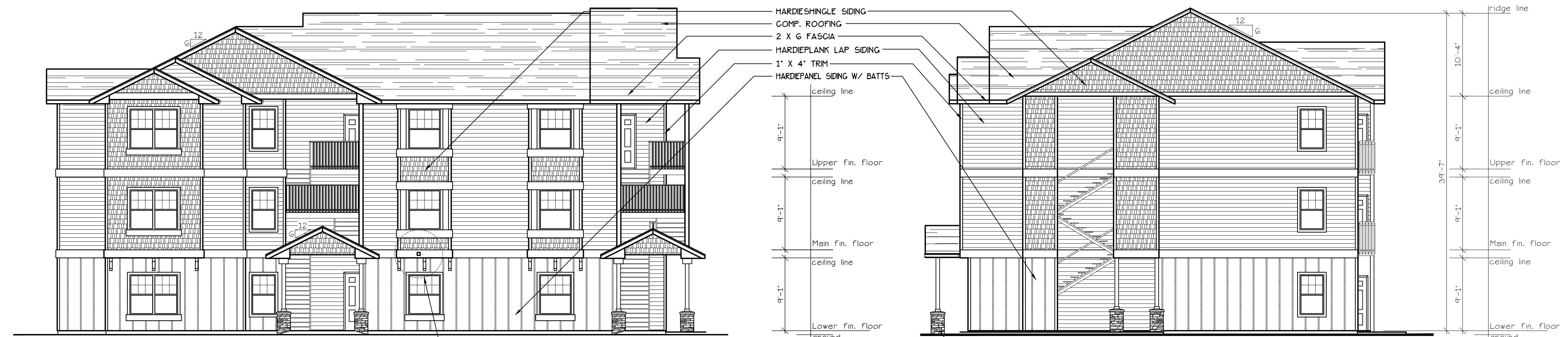
**NORTHEAST ELEVATION (BLD. 25)**  
 SCALE: 1/8" = 1'-0"

**NORTHWEST ELEVATION (BLD. 25)**  
 SCALE: 1/8" = 1'-0"



TYPE C UNITS | TYPE B UNIT  
**SOUTH ELEVATION (BLD. 26)**  
 SCALE: 1/8" = 1'-0"

TYPE B UNITS  
**EAST ELEVATION (BLD. 26)**  
 SCALE: 1/8" = 1'-0"



TYPE B UNIT | EXHAUST VENT 3'-0" CLEAR OF OPERABLE WINDOW AREA TYP. | TYPE C UNITS  
**NORTH ELEVATION (BLD. 26)**  
 SCALE: 1/8" = 1'-0"

TYPE C UNITS  
**WEST ELEVATION (BLD. 26)**  
 SCALE: 1/8" = 1'-0"

NO CHANGES, MODIFICATIONS OR REVISIONS TO BE MADE TO THIS DRAWING WITHOUT THE AUTHORIZATION FROM THE DESIGN ENGINEER.  
 DIMENSIONS & NOTES TAKE PRECEDENCE OVER GRAPHICAL REPRESENTATION.

DESIGN: P.L.M.  
 DRAWN: G.L.D.  
 CHECKED: M.D.G.  
 DATE: Jan-21  
 SCALE: AS SHOWN

JOB # 7111

REGISTERED PROFESSIONAL ENGINEER  
 Mark D. Greer  
 JULY 14, 1978  
 RENEW DATE: June 30, 2021

**A5.90**



**SOUTH ELEVATION (BLD. 27+31)**  
SCALE: 1/8" = 1'-0"  
**TYPE E UNIT**



**EAST ELEVATION (BLD. 27+31)**  
SCALE: 1/8" = 1'-0"  
**TYPE E UNIT**



**NORTH ELEVATION (BLD. 27+31)**  
SCALE: 1/8" = 1'-0"  
**TYPE E UNIT**



**WEST ELEVATION (BLD. 27+31)**  
SCALE: 1/8" = 1'-0"  
**TYPE E UNIT**

NO CHANGES, MODIFICATIONS OR REVISIONS TO BE MADE TO THESE DRAWINGS WITHOUT THE AUTHORIZATION FROM THE DESIGN ENGINEER.  
DIMENSIONS & NOTES TAKE PRECEDENCE OVER GRAPHICAL REPRESENTATION.

DESIGN: P.L.M.  
DRAWN: G.L.D.  
CHECKED: M.D.G.  
DATE: Dec-20  
SCALE: AS SHOWN

JOB # 7111

REGISTERED PROFESSIONAL ENGINEER  
*Mark D. Greely*  
JULY 14, 1978  
MARK D. GREELY  
RENEW DATE: APR 30, 2021

**A9.90**



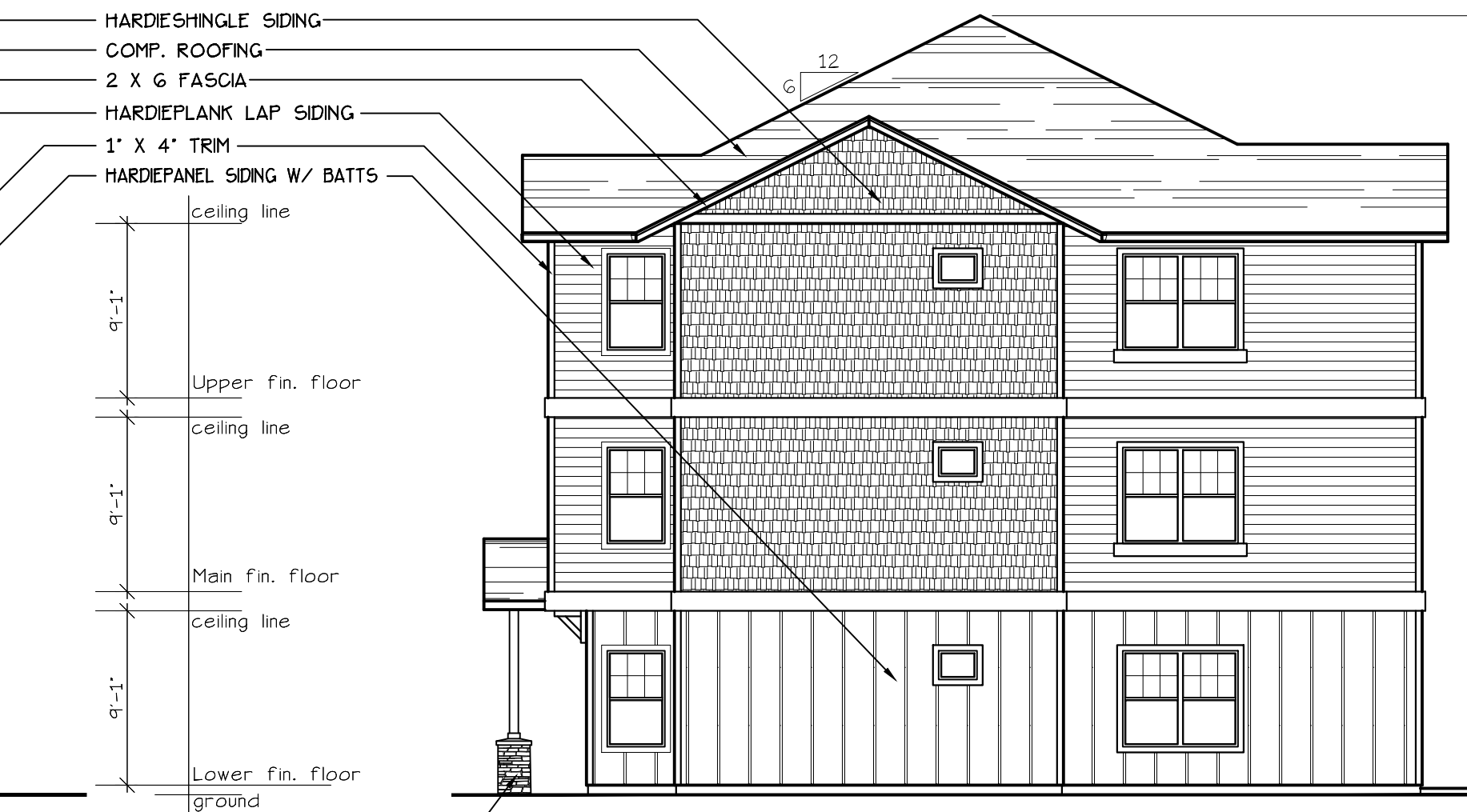
**SOUTH ELEVATION** (TYPE A UNITS)  
SCALE: 1/8" = 1'-0"  
**BLD. 28**



**EAST ELEVATION** (TYPE A UNITS)  
SCALE: 1/8" = 1'-0"  
**BLD. 28**



**NORTH ELEVATION** (TYPE A UNITS)  
SCALE: 1/8" = 1'-0"  
**BLD. 28**



**WEST ELEVATION** (TYPE A UNITS)  
SCALE: 1/8" = 1'-0"  
**BLD. 28**

NO CHANGES, MODIFICATIONS OR REVISIONS TO BE MADE TO THIS DRAWING WITHOUT THE AUTHORIZATION FROM THE DESIGN ENGINEER.  
DIMENSIONS & NOTES TAKE PRECEDENCE OVER GRAPHICAL REPRESENTATION.

DESIGN: P.L.M.  
DRAWN: G.L.D.  
CHECKED: M.D.G.  
DATE: Dec-20  
SCALE: AS SHOWN

JOB # 7111

REGISTERED PROFESSIONAL ENGINEER  
*Mark D. Grell*  
JULY 14, 1978  
MARK D. GRELL  
RENEW DATE: June 30, 2021

**A1.80**

**BUILDING ELEVATIONS**

**THE GROVE AT FAIRVIEW 2**

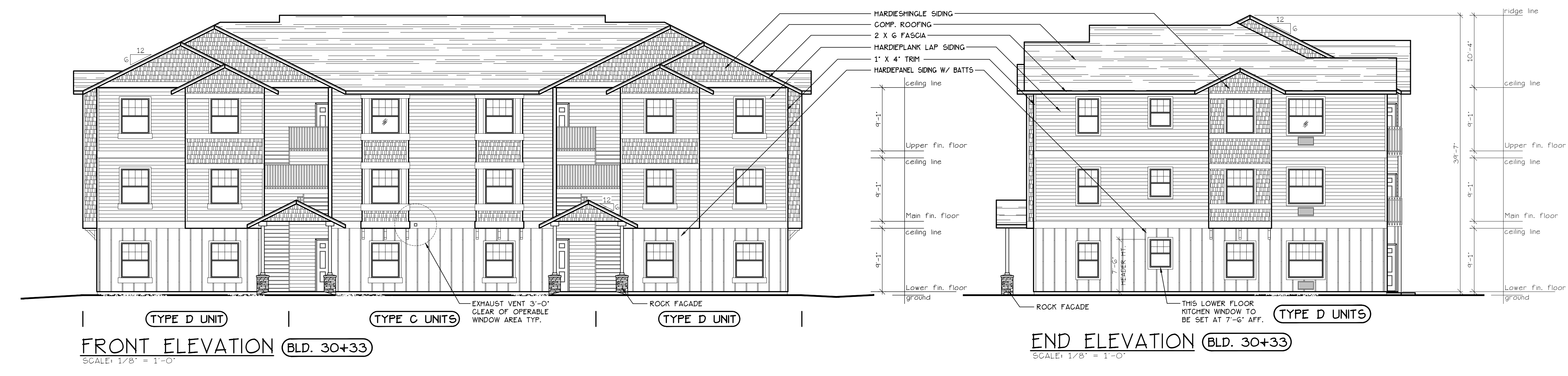
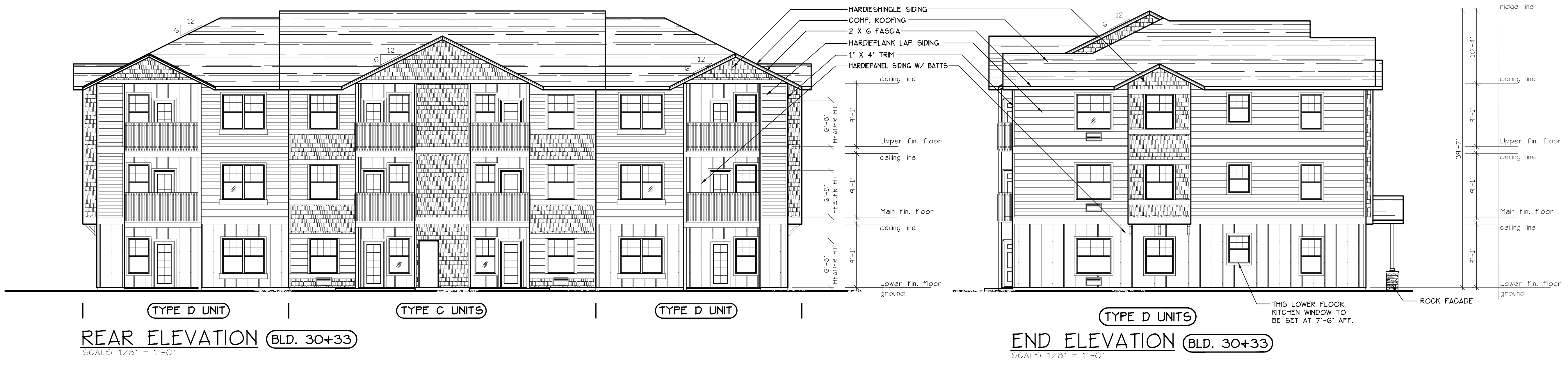
NO CHANGES, MODIFICATIONS OR REVISIONS TO BE MADE TO THIS DRAWING WITHOUT THE WRITTEN AUTHORIZATION FROM THE DESIGN ENGINEER.  
 DIMENSIONS & NOTES TAKE PRECEDENCE OVER GRAPHICAL REPRESENTATION.

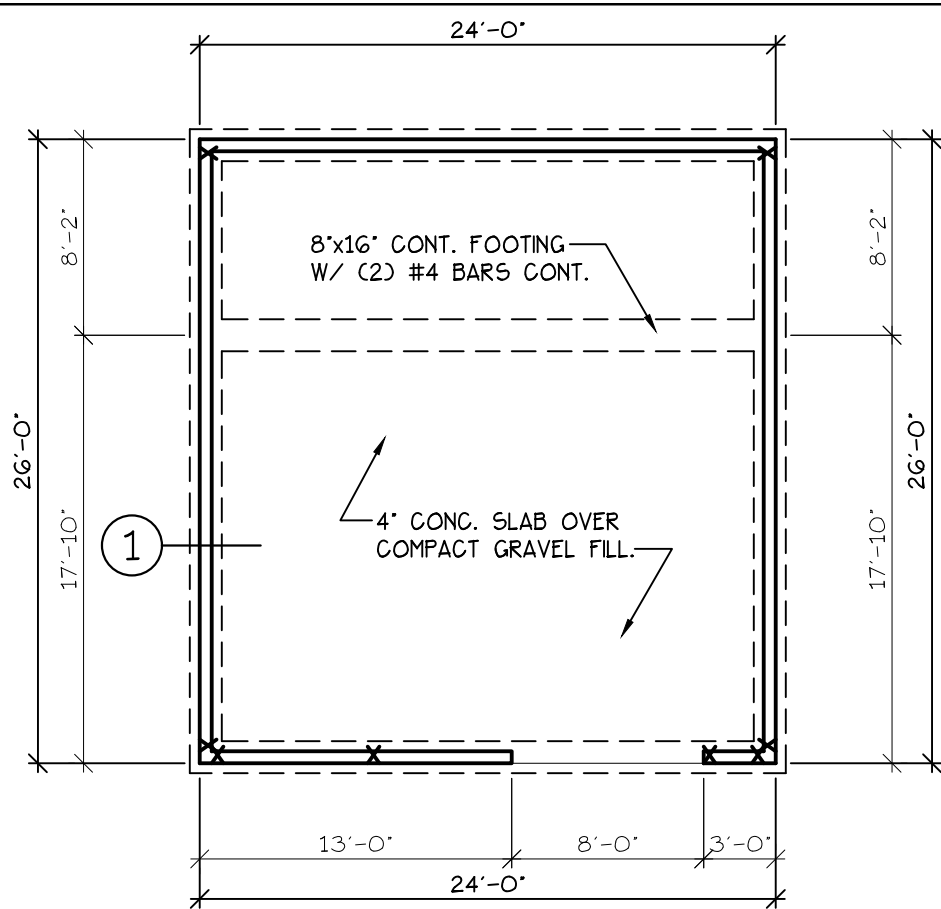
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 DRAWN: G.L.D.  
 CHECKED: M.D.G.  
 DATE: Dec-20  
 SCALE: AS SHOWN

JOB # 7111

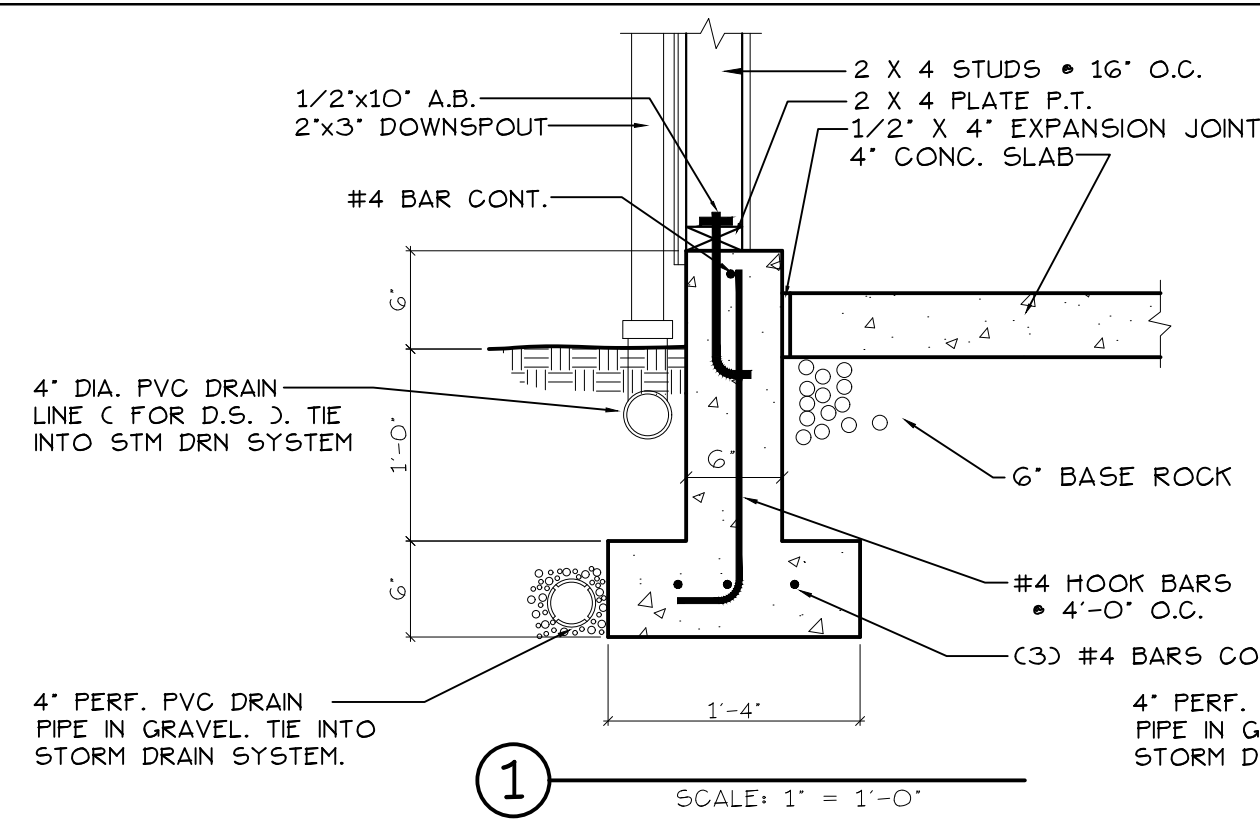
REGISTERED PROFESSIONAL ENGINEER  
 Mark D. Greer  
 JULY 14, 1978  
 RENEW DATE: June 30, 2021

**A7.80**



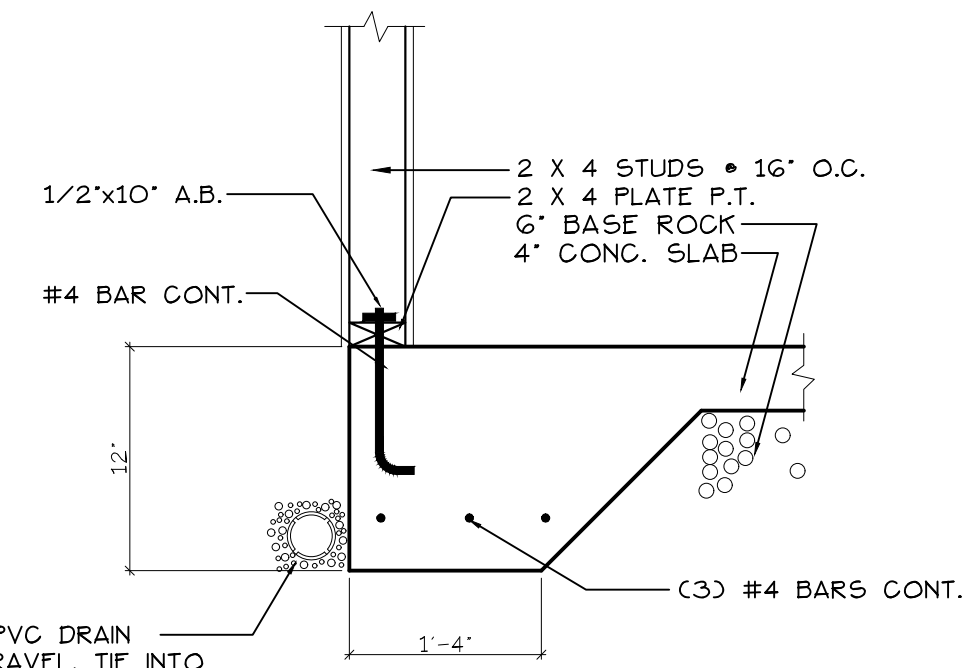
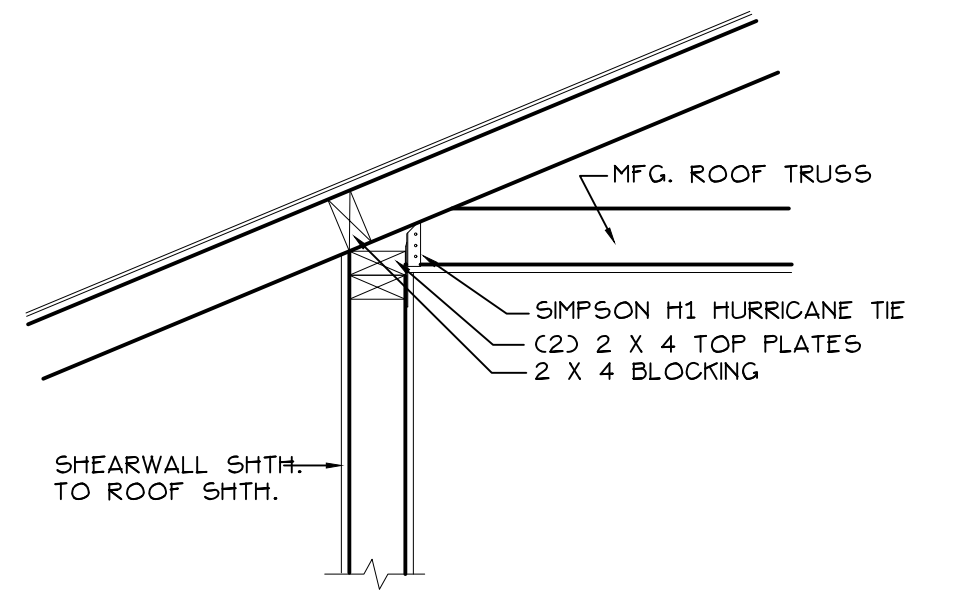


**FOUNDATION PLAN**  
SCALE: 1/8" = 1'-0"

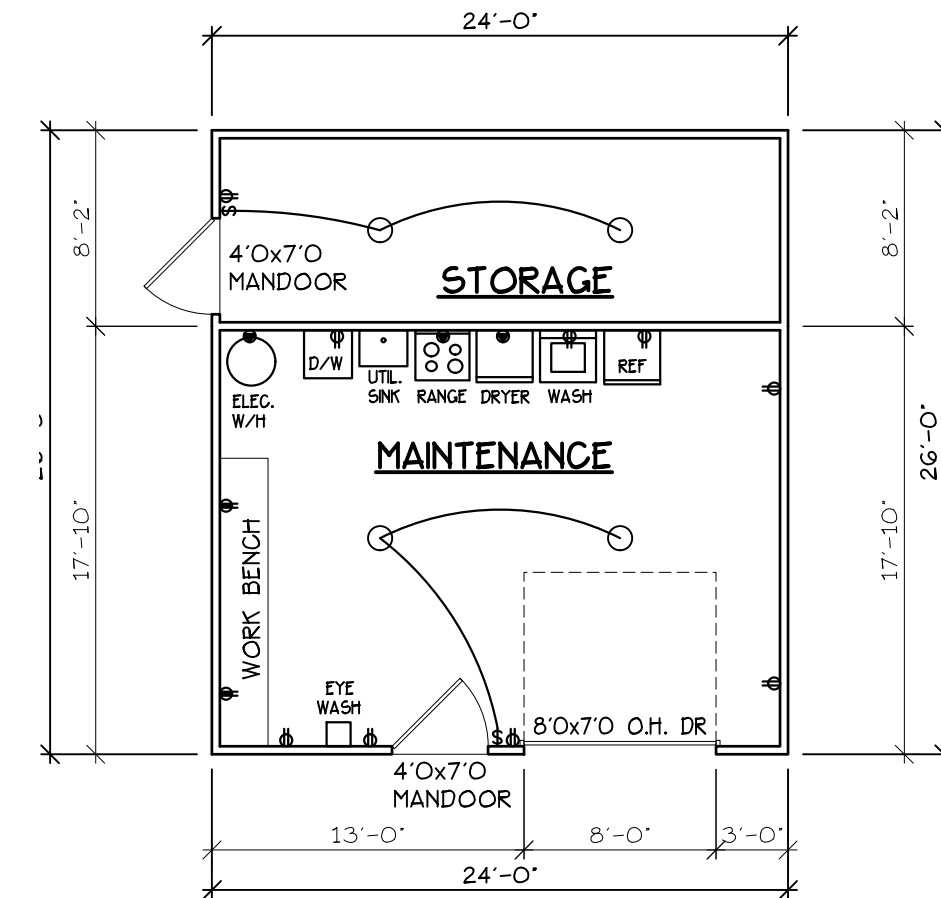


**ALTERNATE**  
SCALE: 1" = 1'-0"

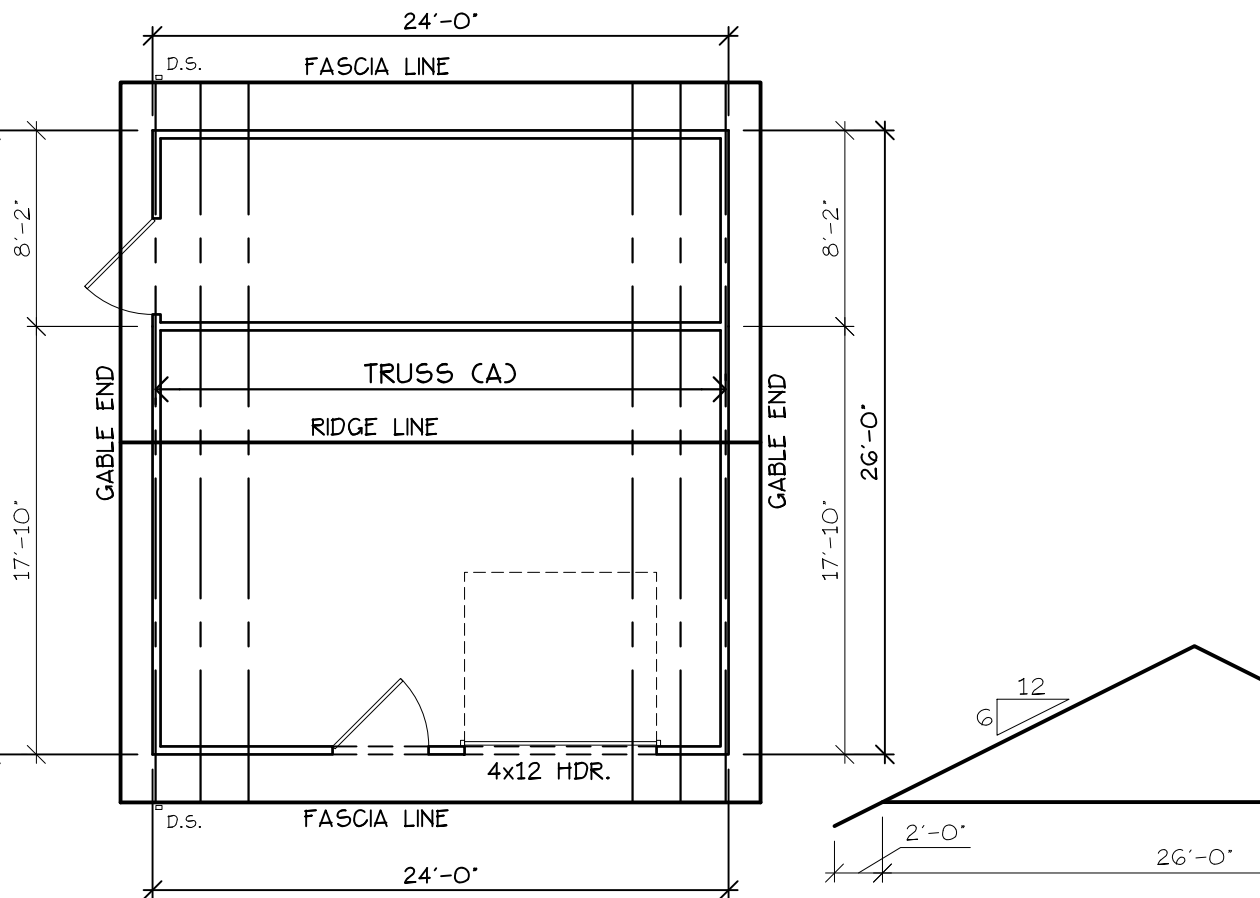
THICKEN FOOTING AT HOLDDOWN LOCATIONS AS NEEDED FOR EMBEDMENT DEPTH PLUS 3 INCHES.



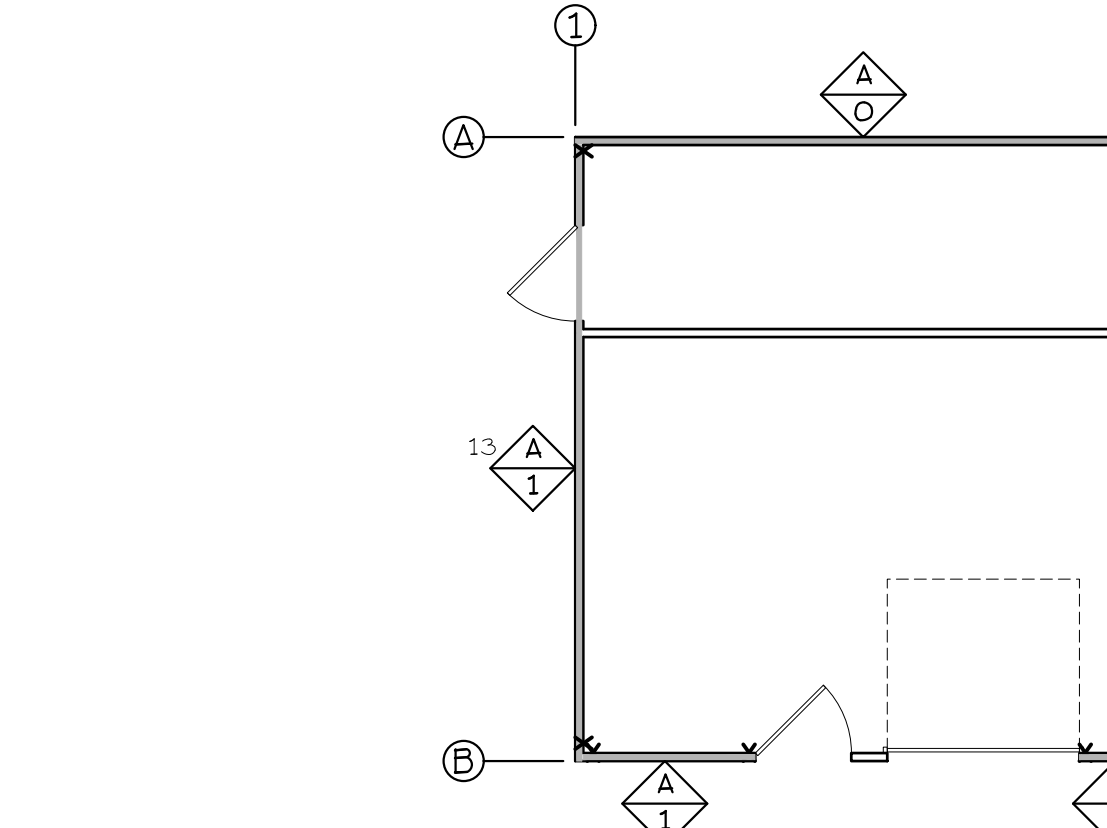
**TYPICAL WALL SECTION**  
SCALE: 1" = 1'-0"



**FLOOR PLAN**  
SCALE: 1/8" = 1'-0"



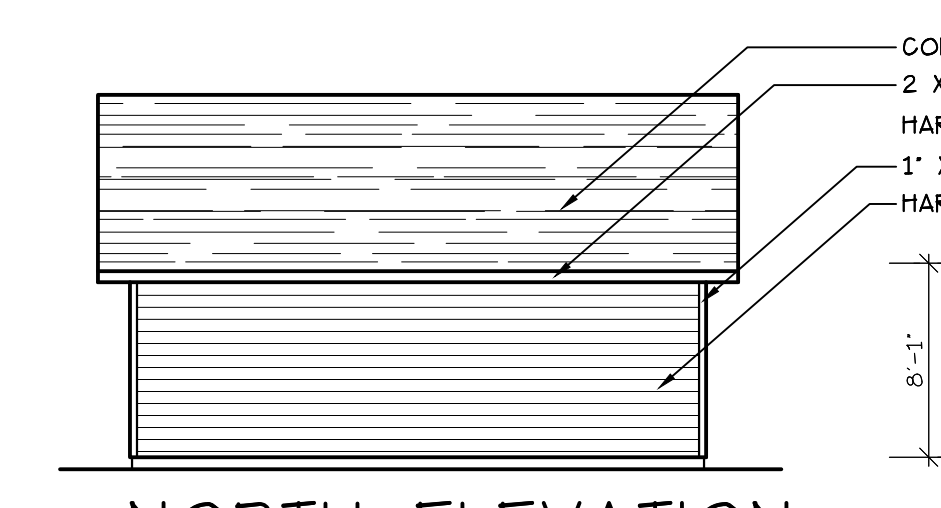
**ROOF + TRUSS PLAN**  
SCALE: 1/8" = 1'-0"



**FLOOR PLAN**  
SCALE: 1/8" = 1'-0"

**MAINTENANCE BLD.**

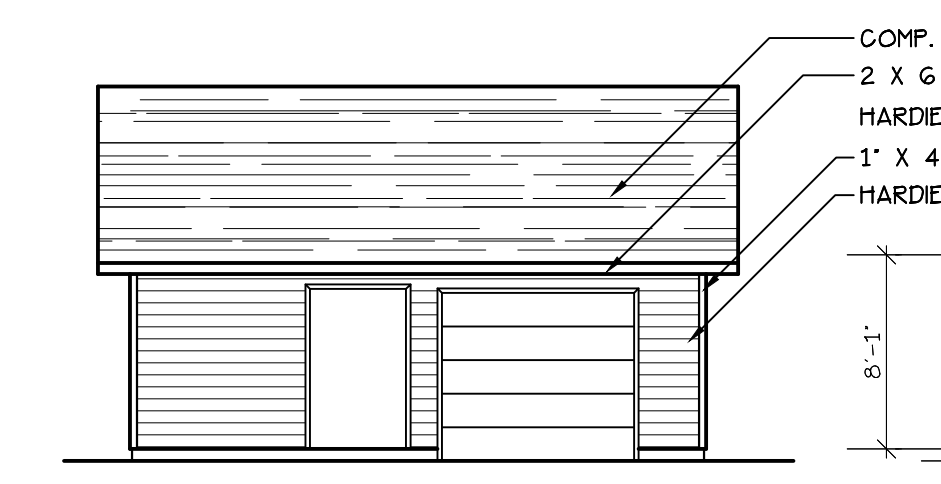
**MAINTENANCE BLD.**



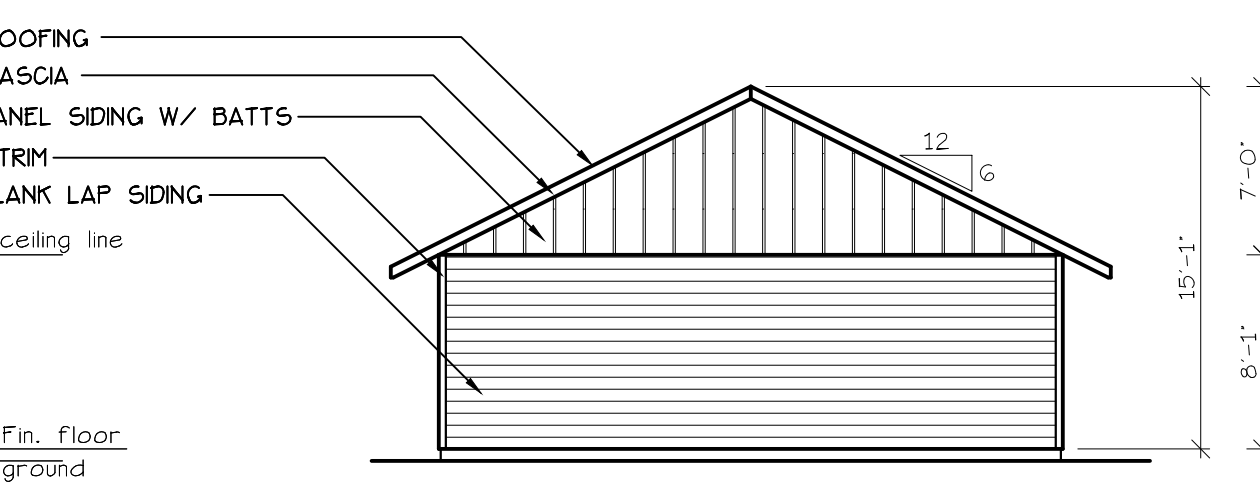
**NORTH ELEVATION**  
SCALE: 1/8" = 1'-0"



**WEST ELEVATION**  
SCALE: 1/8" = 1'-0"



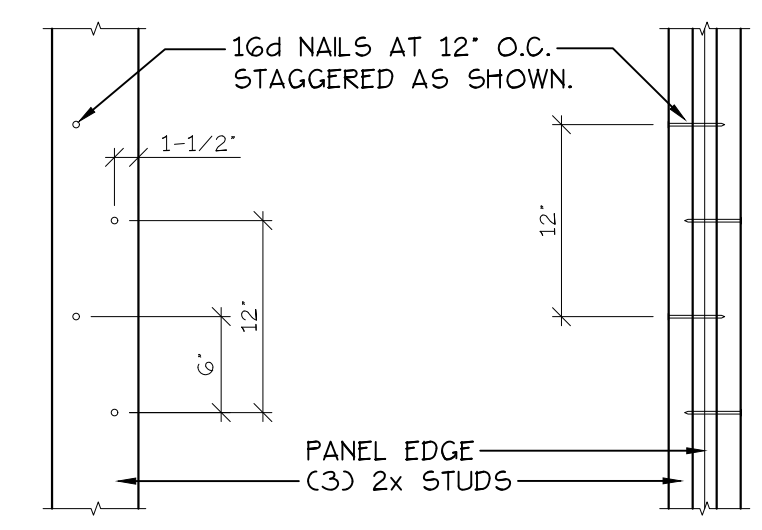
**SOUTH ELEVATION**  
SCALE: 1/8" = 1'-0"



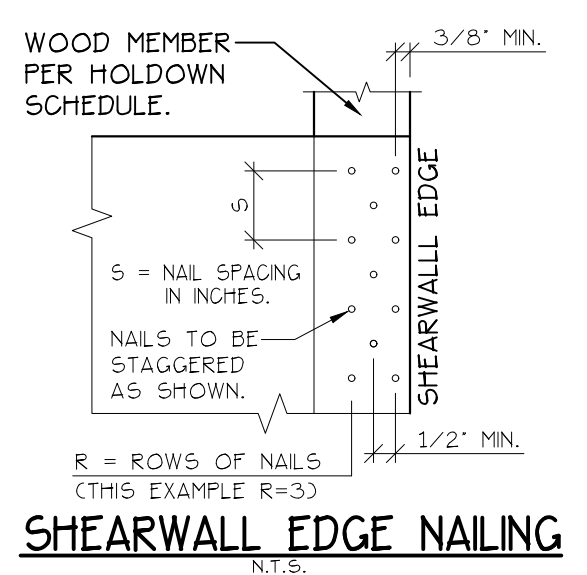
**EAST ELEVATION**  
SCALE: 1/8" = 1'-0"

**MAINTENANCE BLD.**

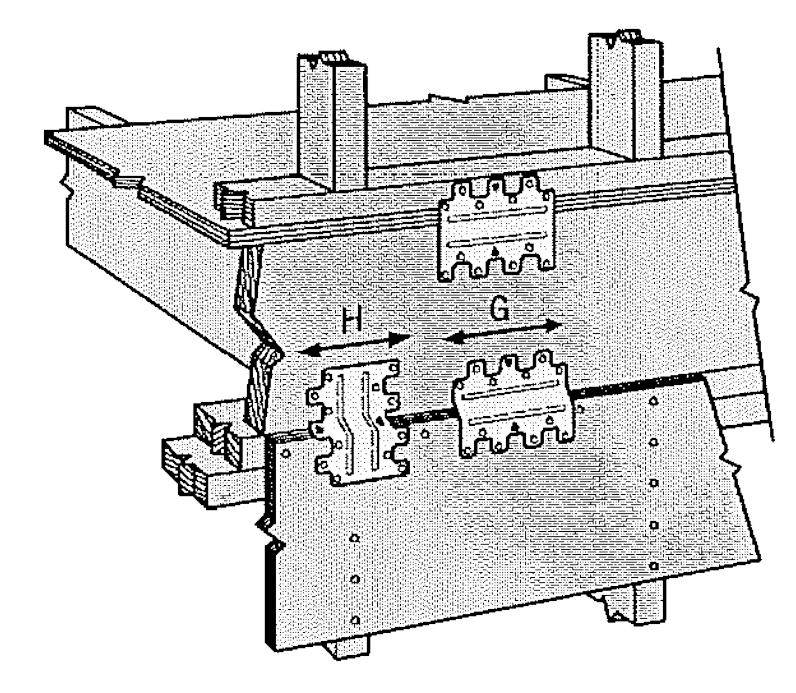
**MAINTENANCE BLD.**



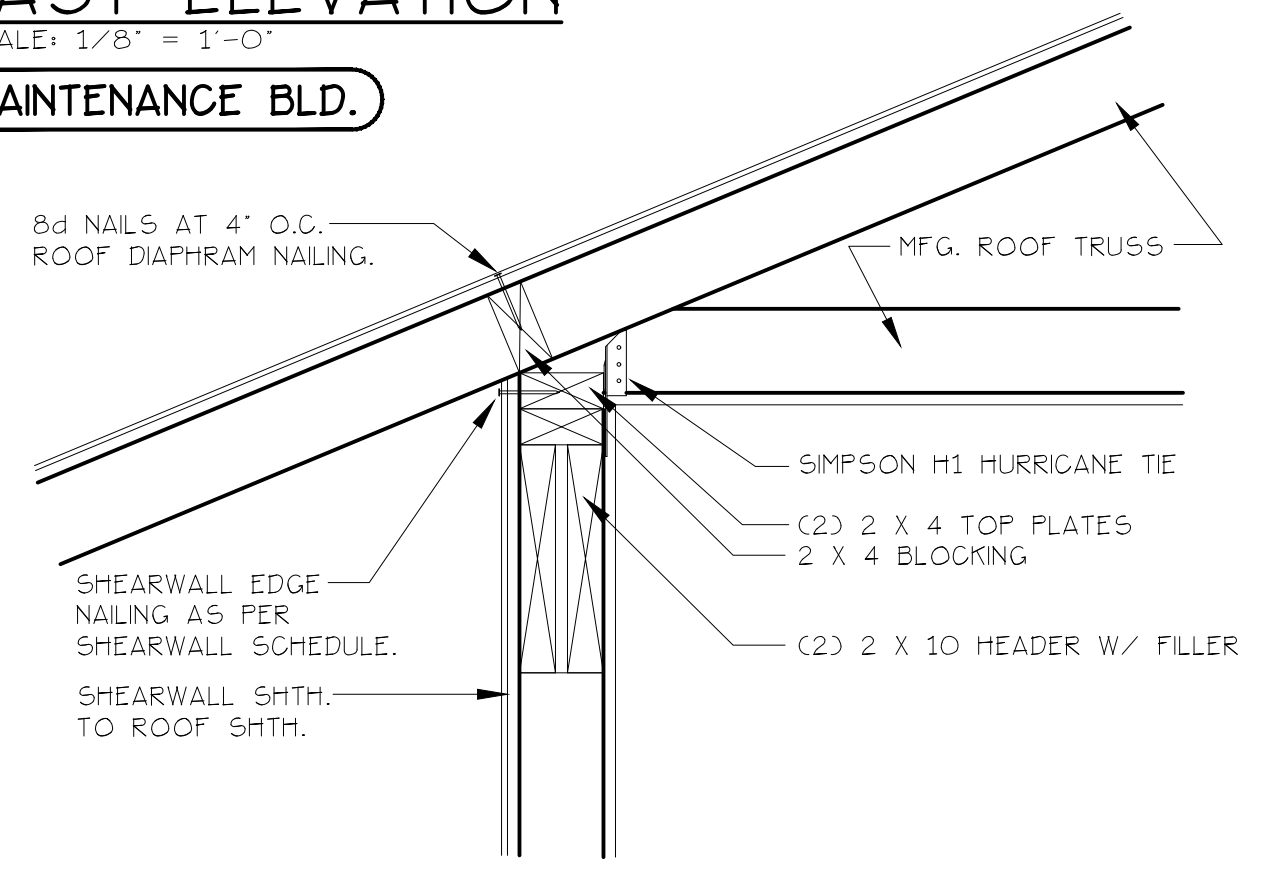
**(3) 2x STUDS LAMINATED**



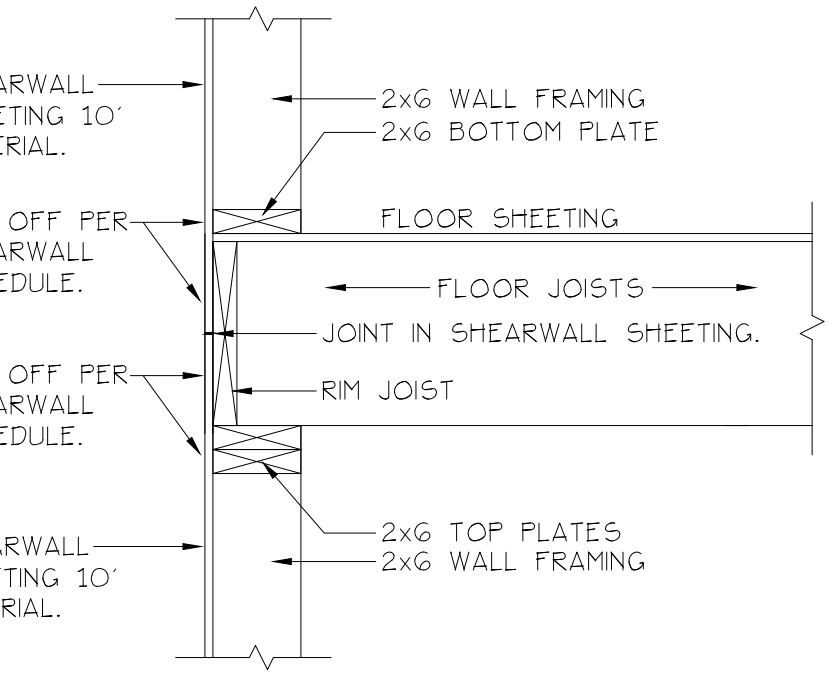
**SHEARWALL EDGE NAILING**



**LTP5 INSTALLATION**



**DOUBLE TOP PLATE CONNECTION WITH H1 HURRICANE TIE**



**ALTERNATE WALL NAILING TO ELIMINATE LTP-5 (Note 14)**

**HOLDOWN SCHEDULE**

HOLDOWN TYPE	HOLDOWN	ANCHOR BOLT	ANCHOR DIAMETER	EMBED LENGTH	MIN. STEM-WALL WIDTH	WOOD MEMBER	CONNECTION TO WOOD MEMBER	REMARKS	ALLOWABLE LOAD
0	No Holddown								
1	HDU2	S5TB24	5/8"	20 5/8"	6"	(2) 2x6 <sup>G</sup>	(6) 1/4"x2-1/2" <sup>7</sup>		3,075#
2	HDU4	S5B/8x24	5/8"	18"	6"	(2) 2x6 <sup>G</sup>	(10) 1/4"x2-1/2" <sup>7</sup>		4,565#
3	HDU5	S5B/8x24	5/8"	18"	6"	(2) 2x6 <sup>G</sup>	(14) 1/4"x2-1/2" <sup>7</sup>		5,645#
4	HDU8	S5B/8x24	7/8"	18"	8"	4x6	(20) 1/4"x2-1/2" <sup>7</sup>		7,870#
5	HQ8	PAB7-30	7/8"	26 3/4"	8"	4x6	(20) 1/4" x 3" <sup>7</sup>		9,230#
6	HD12	PAB8-30	1"	26 3/4"	8"	6x6	(4) 1" DIA. M.B.		14,220#
7	HDI14	PAB8-30	1"	26 3/4"	8"	(1) 6x6	(36) 1/4"x2-1/2" <sup>7</sup>		14,445#
8	MSTC28	N/A	N/A	N/A	N/A	(2) 2x6 <sup>G</sup>	(16) - 16d Sinker		1,540#
9	MSTC40	N/A	N/A	N/A	N/A	(2) 2x6 <sup>G</sup>	(32) - 16d Sinker		3,080#
10	MSTC52	N/A	N/A	N/A	N/A	(2) 2x6 <sup>G</sup>	(48) - 16d Sinker		4,620#
11	MSTC66	N/A	N/A	N/A	N/A	(2) 2x6 <sup>G</sup>	(64) - 16d Sinker		5,860#

Notes:  
 1. Holdowns by Simpson Strong-Tie Company, Inc. See Simpson catalog for proper installation.  
 2. Hardmount all holdown anchors prior to concrete pour.  
 3. Edge nail sheathing to all posts or boundary members at holdowns.  
 4. Locate Holddown within 6" of end of Shear Panel.  
 5. All wood members must be "Douglas fir".  
 6. Laminate studs with 16d Nails at 12' on center staggered. Clinch tips of nails.  
 7. Use Simpson SD5 1/4" dia. wood screws.  
 8. 2 Holddowns required (See 'Holdowns between floors' detail this sheet).  
 9. Use threaded rod + CNW coupler to attach to anchor bolt at foundation. Run threaded rod up to Holddown at lower floor and connect.  
 10. Shearwall Type  
 11. Holddown Type

**SHEAR WALL SCHEDULE**

WALL TYPE	STRUCTURAL PANEL SHEATHING	PANEL EDGE NAILING	SHEARWALL EDGE NAILING (SEE DETAIL)	FIELD NAILING	A 35 CLIP DOUBLE TOP PLATE CONN. <sup>5</sup>	SILL PLATE CONNECTION (A.B.) <sup>10</sup>	SOLE PLATE CONNECTION <sup>5</sup>	SHEAR VALUE (PLF) DOUGLAS FIR	
						1/2" DIA. x 10' LONG	5/8" DIA. x 10' LONG	WIND SEISMIC	
A	7/16" OSB or 15/32" Plywood	0.131" Dia. x 2.5" Nails at 6" o.c.	R=2 S=3	0.131" Dia. x 2.5" Nails at 12" o.c.	20' O.C.	3'-8" O.C. <sup>10</sup>	4'-0" O.C.	16d Nails at 6" o.c.	365 260
B	7/16" OSB or 15/32" Plywood	0.131" Dia. x 2.5" Nails at 4" o.c.	R=NA S=NA	0.131" Dia. x 2.5" Nails at 12" o.c.	15' O.C.	2'-9" O.C. <sup>10</sup>	4'-0" O.C.	16d Nails at 4-1/2" o.c.	530 380
C	7/16" OSB or 15/32" Plywood	0.131" Dia. x 2.5" Nails at 3" o.c.	R=NA S=NA	0.131" Dia. x 2.5" Nails at 12" o.c.	11' O.C.	N/A	2'-9" O.C. <sup>10</sup>	16d Nails at 3-1/2" o.c.	685 490
D	15/32" Plywood	0.148" Dia. x 3" Nails at 3" o.c.	R=NA S=NA	0.148" Dia. x 3" Nails at 12" o.c.	9' O.C.	N/A	2'-4" O.C. <sup>10</sup>	(2) Rows 16d Nails at 6" o.c.	840 600
E	15/32" Plywood	0.148" Dia. x 3" Nails at 2" o.c.	R=NA S=NA	0.148" Dia. x 3" Nails at 12" o.c.	7' O.C.	N/A	1'-9" O.C. <sup>12</sup>	N/A	1,075 770
F	1/2" GWB MIN.	5d cooler nails at 4" o.c.	R=NA S=NA	5d cooler nails at 12" o.c.	24' O.C.	4'-0" O.C.	4'-0" O.C.	16d Nails at 6" o.c.	125 125
G	1/2" GWB MIN.	#6x1-1/4" screws 4" o.c. (blocked)	R=NA S=NA	#6x1-1/4" screws 12" o.c.	24' O.C.	4'-0" O.C.	4'-0" O.C.	16d Nails at 6" o.c.	160 160

Notes:  
 1. Block all edges of sheathing.  
 2. Do not break sheathing skin by over driving nails.  
 3. Pre-drill as required to avoid splitting sills, etc.  
 4. Nails should be located 3/8" clear of panel edges.  
 5. Use Simpson A35 clips to attach blocking to top plate at floor line. At roof line use Simpson H-1 clips at each truss (U.O.N.).  
 6. Values of other standard construction fasteners will require spacing adjustments and must be approved by the engineer-of-record.  
 7. Use hot dipped galvanized nails at all exterior applications.  
 8. C-D, C-C sheathing, plywood panels siding, and other grades covered in APA Plywood Design Specification.  
 9. Sheathing face grain can be applied perpendicular or parallel to wall studs, provided studs are spaced a maximum of 16" o.c.  
 10. 3"x3"x1/4" washer required on A.B. with 2x sill plates.  
 11. Framing at adjoining panel edges shall be (3) 2x studs laminated (See detail this sheet).  
 12. Min. 3" nominal sill plate w/ 3"x3"x1/4" washers.  
 13. Sheath entire wall (above + below) windows and doors) according to shearwall schedule. Use of 3"x3"x1/4" washers are required on all anchor bolts for these walls.  
 14. Install LTP5 when required at 32" o.c. per detail.  
 15. All wood members must be "Douglas Fir".  
 16. All walls not designated as shearwalls to have 1/2"x10" anchor bolts at 4'-0" o.c. maximum.  
 17. Interior shearwall to extend to roof sheathing via truss or blocking panels.  
 18. Shearwall Type  
 19. Holddown Type

**GENERAL STRUCTURAL NOTES**

- CODE:  
 A. All materials and workmanship shall conform to the requirements of the State of Oregon 2019 Structural Specialty Code.
- DESIGN LOADS:  
 A. Roof Dead Load = 17 psf  
 Roof live load = 20 psf  
 B. Floor Dead Load = 20 psf  
 Floor live load = 40 psf  
 C. Flat roof snow Load = 12.6 psf.  
 D. Exposure factor = 0.9  
 E. Thermal factor = 1.0  
 F. Importance factor = 1.0

- GENERAL:  
 A. Contractor shall be responsible for all construction methods, techniques, sequencing and safety required to complete construction.  
 B. Contractor shall verify all dimensions and details prior to proceeding with construction. All discrepancies shall be approved by the Architect of Engineer of record.  
 C. Contractor shall verify all required openings on Architectural, Mechanical and Electrical plans.

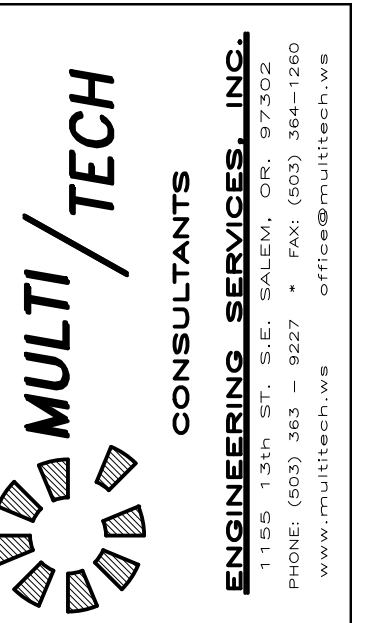
- FOUNDATION:  
 A. Footings have been designed for a maximum allowable soil bearing pressure of 2000 psf. (See Geotech report for soil condition and requirements).  
 B. All footings shall be poured against undisturbed soil unless otherwise noted. (See Geotech report for soil condition and requirements).

- CONCRETE:  
 A. All structural concrete shall develop a unit compressive stress of 2500 psi minimum at 28 days per O.S.C. section 1904.1.

- REINFORCED STEEL:  
 A. All reinforcing steel shall be ASTM A615, Grade 60 rail steel deformed bars conforming to ASTM A706, Grade 60 bars where welding is required.  
 B. Details of reinforcing steel shall conform to ASTM Manual of Standard Practice, Code of Standard Practice for Detailing Reinforcing Materials, by CRSI and WCRSI (latest Edition).  
 C. All concrete slab reinforcing steel shall be supported at the required heights by approved bolsters prior to pouring slab concrete.

- REINFORCING PROTECTION:  
 A. Concrete deposited against earth = 3 inches  
 B. Concrete formed surfaces exposed to ground or weather = #5 rebar and smaller = 1-1/2 inches  
 C. Slabs = 3/4 inches.

- STRUCTURAL WOOD:  
 A. All structural wood members shall conform to the National Design Specifications for Stress Grade lumber and its Fastenings.  
 Post and Beam 1st floor framing = Douglas Fir No. 2 or better.  
 Floor Joists and Headers = Douglas Fir No. 2 Unless noted otherwise.  
 Stud walls = Douglas Fir stud U.N.O.  
 (Use of Hem-Fir of Mx shall be acceptable only with approval from the engineer).  
 B. The Contractor shall furnish and install all bolts, and plates as required to complete the job.  
 C. Washers shall be used under all bolt heads and nuts bearing on wood.  
 D. All wood members in contact with concrete or masonry shall be pressure treated.  
 E. All 6x12 beams shall be Douglas fir No. 1.

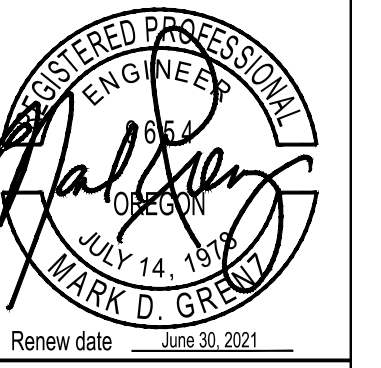


**MAINTENANCE BUILDING PLANS**

**THE GROVE AT FAIRVIEW 2**

NO CHANGES, MODIFICATIONS OR REPRODUCTIONS TO BE MADE TO THESE DRAWINGS WITHOUT WRITTEN AUTHORIZATION FROM THE DESIGN ENGINEER.  
 DIMENSIONS & NOTES TAKE PRECEDENCE OVER GRAPHICAL REPRESENTATION.

Design: P.L.M.  
 Drawn: G.L.D.  
 Checked: M.D.G.  
 Date: APR 2021  
 Scale: AS SHOWN  
 JOB # 6608



**A12.1**

## **The Grove Phase 2**

### ***Fairview Refinement Plan Amendment-Major***

Revised-May 28, 2021

#### **Background:**

The subject properties are about 10.26 acres in size, zoned Fairview Mixed-Use (FMU) and located in the Fairview Refinement Plan II (083W11A/Tax Lot 1002 and 083W02/Tax Lot 100).

The subject properties are part of the Fairview Refinement Plan II (FRP09-1 and FRPA16-01) and is identified as 083W11A/Tax Lot 1002 and 083W02/Tax Lot 100. The Fairview Refinement Plan II was approved in 2009 with development standards and guidelines. The Fairview Refinement Plan II was amended in April 2016.

On September 28, 2020, a Design Review Pre-Application Conference (PRE-AP20-85) was held with the applicant and City staff to discuss the development of the subject property. Per the pre-app meeting the following land-use application are required:

Class 3-Site Plan Review  
Class 1-Design Review  
Driveway Approach Permit  
Fairview Refinement Plan Major Amendment  
Class-2 Adjustments

On January 13, 2020, the applicant held an Open House with the Morningside Neighborhood Association and adjacent property owners to discuss the proposal and address any concerns.

#### **Proposal:**

The subject properties are about 10.26 acres in size, zoned Fairview Mixed-Use (FMU) and located in the Fairview Refinement Plan II (083W11A/Tax Lot 1002 and 083W02/Tax Lot 100).

The applicant is proposing a development consisting of 183 apartment units as shown on the site plans.

083W11A/Tax Lot 1002 (5.97 acres):  
AC (Adaptive Zone)-51 Units  
MI (Mixed-Intensity Zone)-27 Units

083W02/Tax Lot 100 (4.29 acres):  
VC (Village Center Zone)-105 Units

The applicant's proposal is a major amendment to the Fairview Refinement Plan II. The proposed major amendments are as follows:

- Density:** -To increase the 2000 maximum units allowed within the Refinement Plan II
- Lot Standards:** -Exceed the maximum lot depth of 200 feet in the AU zone  
-Exceed the maximum lot depth of 150 feet in the MI zone
- Lot Coverage:** -To exceed the maximum 1,000 square foot building footprint for carports
- Floor Area Ratio:** -To allow 54% FAR where 75% minimum is required
- Setbacks:** -Buildings 27, 28, 29, 30, 31, 32, 33, and 35 will exceed the maximum 20-foot setback within the VC zone along the street side  
-Buildings 20, 23, and 25 will exceed the maximum 20-foot setback within the AU and MI zone along the street side (Reed Road)  
-Building 23 will exceed the maximum 20-foot setback within the AU zone along the street side (north of Chapel Drive)  
-Buildings 22, 23, and 24 will exceed the maximum 20-foot setback within the AU and MI zone along the street side (Chapel Drive)  
-Building 26 will exceed the maximum 20-foot setback within the MI zone along the street side (Lindburg Road)  
-Buildings 20 and 21 do not meet the minimum 30-foot rear yard setback  
-The VC zoned development does not meet the minimum 70% of buildings along the buildable width  
-The AU and MI zoned development does not meet the minimum 50% of buildings along the buildable width
- Parking Setbacks:** -To allow a 16-foot setback along the street setback where 20 feet is required  
-To allow a 16-foot setback along the street setback where 20 feet is required adjacent Chapel Drive and future First Street  
To allow a 7 foot interior side setback where 10 feet is required
- Parking/Loading:** -To allow 139 parking spaces within the AU and MI zoned development, where 137 maximum are allowed



**Driveways/  
Curb Cuts:**

- Exceed the maximum 18-foot driveway width allowed within the VC zone
- Exceed the maximum 16-foot driveway width allowed within the AU and MI zones
- Exceed the maximum 14-foot driveway width allowed within the AU
- To allow 3 curb cuts in the AU zoned development where 2 are allowed

The applicant is proposing to modify the Fairview Refinement Plan II. The Fairview Refinement Plan II was originally adopted in 2009 and amended in 2016. The modification will allow the applicant to provide multi-family units where originally approved, by allowing some deviation from the development standards. See attached site plans.

Applicable criteria and development standards have been addressed below as well and will be reviewed as part of this application.

**Criteria:**

***Sec. 530.035(e)-Criteria:***

***(2)Major amendment. A major amendment shall be approved if all of the following criteria are met:***

***(A)The proposed amendment conforms to the applicable provisions of the Salem Area Comprehensive Plan.***

**Findings:** The subject property is identified as Mixed-Use on the Salem Area Comprehensive Plan (SACP). The proposed amendment is in compliance with the applicable provisions of the SACP. Per SACP-Land Use Plan Map (A)(3)(b) -Plan Map Designations (Pages 7 & 8):

***“b. Mixed-Use***

***The Mixed-Use designation encompasses the opportunity for a variety of housing opportunities, including but not limited to, single-family detached, single-family attached, garden apartments, apartments, communal living, and row houses. The location, density, and style of housing is governed by the zoning code. What differentiates this classification from Single-Family and Multi-Family Residential categories is an expansion of the types of uses deemed compatible with residential development. In addition to nonresidential uses (such as schools and other educational facilities, parks, open space, and religious organizations), this classification permits the mixing of***

*shopping and services, convenience stores, commercial office (including livework units) and compatible commercial and/or industrial uses within neighborhoods and structures.*

*The purpose of this designation is to:*

- 1. Allow a mixture of complementary land uses that may include housing of all types, retail, offices, commercial services, and civic uses to create economic and social vitality and the ability to meet multiple needs in compact areas;*
- 2. Develop mixed-use areas that are safe, comfortable and attractive to pedestrians;*
- 3. Provide flexibility in the siting and design of new development and redevelopment to optimize the use of natural resources, energy alternatives, improvements in development tools and techniques, and changes in the marketplace;*
- 4. Recognize streets as public places that encourage pedestrian, bicycle, and transit use;*
- 5. Facilitate efficient use of land by encouraging compact, high-density development and minimizing the amount of land that is needed for surface parking; and*
- 6. Encourage and facilitate development that supports public transit and the utilization of alternative modes of transportation.”*

The request is in conformance with intent of the mixed-use designation. Development of the subject property will provide a needed type of housing while facilitating a high-density development that is creative and pedestrian friendly. The proposal complies with the applicable intent statements of the SACP as addressed in this report. The applicant has presented evidence sufficient to prove compliance.

Mixed-Use Development Goal (Page 33): *To provide a mixture of complementary land uses that may include housing, retail, offices, services, industrial and civic uses, to create economic and social vitality.*

The Fairview Plans are setup to provide a mixture of uses throughout the Fairview area. The applicant’s proposal does not change the uses within the Fairview Refinement Plan. The amendment is to allow an increase in units allowed and deviated from some of the development standards. The proposed units will help to meet the Mixed-Use goals and housing needs, by providing a needed housing type like high density. The Fairview Plan is set up to allow a mixture of housing times to achieve its goal.

Mixed Use Development Policy 1 (Development/Page 34): *Encourage efficient use of land by facilitating compact, high-density development and minimizing the amount of land that is needed to accommodate automobile parking. Where appropriate, revise mixed-use design standards to make them simpler and easier to use, including making mixed-use design standards consistent across mixed-use areas.*

The amendment is to allow an increase in units allowed and deviated from some of the development standards. The proposed units will help to meet the Mixed-Use Goals and housing needs. Development of the subject property will provide a needed type of housing while facilitating a high-density development that is creative and pedestrian friendly.

Mixed Use Development Policy 2 (Development/Page 34): Encourage development that preserves private open space for residents and employees of mixed-use development.

The Fairview Refinement Plan identifies designated open space areas and requirements. The proposal is not deviating from open space Development Standards. Open space is provided through the proposed development as outline on the Open Space and Landscape Plans. Private open space via a balcony or patio will be provided for each unit per the Multi-Family Design Standards.

A tree plan has been provided as well to identify the preservation of trees within the development sites.

Mixed Use Development Policy 3 (Priorities for Mobility and Access/Page 34): Facilitate development (land use mix, density, connectivity, design, and orientation) that reduces the need for, and frequency of, SOV trips and supports public transit, where applicable.

The pedestrian paths and walkable areas provide a more pedestrian friendly development to potentially reduce vehicle trips. However, currently there is no public transit available directly to the site.

Mixed Use Development Policy 4 (Priorities for Mobility and Access/Page 34): Reinforce streets as public places that encourage pedestrian and bicycle travel.

The streets within the proposed development provided connections to adjacent developments via paved streets and/or paved pedestrian paths. The streets encourage pedestrian and bicycle travel throughout the development and surrounding developments.

Mixed Use Development Policy 5 (Priorities for Mobility and Access/Page 34): Provide roadway and pedestrian connections to residential areas.

Pedestrian connectivity is an important development aspect of the Fairview Refinement Plan II. The streets and pathways within the proposed development provided connections to adjacent residential developments as shown on the site plans.

Mixed Use Development Policy 6 (Design/Page 34): Develop commercial and mixed-use areas that are safe, comfortable, and attractive to pedestrians.

Development of the subject property will provide a needed type of housing while facilitating a high-density development that is attractive and pedestrian friendly.

Mixed Use Development Policy 7 (Design/Page 34): *Provide flexibility in the siting and design of new developments, facilities, and redevelopment to respond to changes in the marketplace and infrastructure systems.*

As shown on the site plan, the majority of the buildings are placed in close proximity to the street to provide a pedestrian friendly building.

Mixed Use Development Policy 8 (Design/Page 34): *Provide appropriate transitions between mixed-use areas and adjacent single-use neighborhoods.*

There are several adopted refinement plans within the Fairview area. All the plans work together to provide connectivity to adjacent uses.

The proposed amendment conforms to the applicable provisions of the Salem Area Comprehensive Plan, therefore, meeting this criteria.

***(B)The proposed amendment is compatible with adjoining land uses.***

Findings: The existing uses abutting the proposed development are as follows:

083W11A/Tax Lot 1002: AC (Adaptive Zone) and MI (Mixed-Intensity Zone)

- North: Existing Grove Apartments Phase 1, vacant land, and park land
- East: Vacant land proposed for single family dwellings and apartment development
- South: Vacant land (Simpson Hill)
- West: Existing Heritage School and Grove Apartments Phase 1

083W02/Tax Lot 100 (4.19 acres): VC (Village Center Zone)

- North: Vacant land within the Fairview Refinement Plan II
- East: Vacant land within the Fairview Refinement Plan II
- South: Vacant land (Simpson Hill)
- West: Vacant land (Fairview Addition)

The proposed residential uses are compatible with the adjoining land uses that are either existing or proposed for future development. Therefore, meeting this criteria.

***(C)The proposed amendment is physically feasible, given consideration of existing or proposed infrastructure and public services.***

Findings: The existing and required public facilities remain unchanged from the adopted 2009 Fairview Refinement Plan approvals. The applicant is required to provide the needed infrastructure for the development, which includes sewer, water, storm drainage and transportation facilities. The submitted plans show that the proposed buildings can be serviced by the infrastructure to support the development and are designed to City standards.

The proposal is for urban development in an area where future extensions of those services can be provided in the most feasible, efficient, and economical manner. The City's capital improvement program and its minimum code standards for public facilities provide a means for improving and updating public facilities systems (water and sewer). All necessary and appropriate public services and facilities essential for development will be provided to this property at levels that are adequate to serve the proposed use.

The City maintains an infrastructure of public services that includes sewer, water, and storm drainage facilities. The City will specify any needed changes to the existing service levels prior to development.

Other private service providers supply garbage, telephone, television, postal and internet services as needed by the development. The required public services and facilities to serve new development will be determined by the City at the time development permits are requested.

An updated Traffic Analysis dated January 12, 2021 has been provided to staff.

The proposed amendment is physically feasible and provides all required infrastructure and public services for development of the site. Therefore, this criteria has been met.

**(D) *The proposed amendment conforms to the following goals:***

***(i) Encourage mixed-use development, improved protection of open spaces and natural features, and greater housing and transportation options;***

Findings: The Refinement Plan encourages mix-use development and provides areas within the Plan for commercial and residential development. The proposed sites are within areas designated and zoned for residential development. The proposal will provide housing that is needed in this area, while protecting designated open space areas. The proposal will also provide common and private open space through the development.

The proposal conforms to this goal.

***(ii) Encourage the innovative integration of park and school uses;***

Findings: The proposed development is located directly to the south of the designated park land and located adjacent to an existing school (Heritage School). Leslie Middle school is located less than a mile from the site.

The proposal conforms to this goal.

***(iii) Encourage the principles of sustainable development and sustainable business practices;***

Findings: The proposal encourages sustainable development by providing 143,900 square feet of open space through the development. Also preserving any identified open space areas and trees within the site that have been adopted within the Fairview Refinement Plan II.

The proposal conforms to this goal.

***(iv) Support affordable housing options and mixed-income neighborhoods;***

Findings: The proposal is for 183-units total within both sites. Apartments are deemed to be an affordable housing option that will also help to achieve a mixed-income neighborhood with all the existing single-family dwellings within the Fairview development.

The proposal conforms to this goal.

***(v) Facilitate the resourceful use of land through the efficient arrangement of land uses, buildings, circulation systems, open space and infrastructure;***

Findings: The proposed development and amendments facilitate resourceful use of land by provided more than adequate open space throughout the site, an efficient layout of the buildings, and a safe and efficient circulation system throughout the development for both vehicles and pedestrians.

The proposal conforms to this goal.

***(vi) Encourage economic opportunities that comply with and support business practices;***

Findings: The proposal encourages economic opportunities by providing jobs via the development of the site.

The proposal conforms to this goal.

***(vii) Recognize the historic significance of buildings, structures, and sites, including archaeological sites, through appropriate means, including, but not limited to, obtaining official historic resource designation; and***

Findings: There are no historic buildings or structures located on the subject properties. All historic buildings within the Fairview Master Plan Areas have been documented.

The proposal conforms to this goal.

*(viii) Encourage energy conservation and improved air and water quality.*

Findings: The proposal is for 183-units within the 2 areas. There are no commercial uses proposed within these developments. However, the Fairview Plan does have areas designated for commercial use. Pedestrian paths and paved sidewalks are provided throughout to help minimize the use of vehicle transportation within the Fairview area.

The proposal conforms to this goal.

## Development Standards

Table 1: Page 21-Page 24 of the Amended Fairview Refinement Plan II

The applicant's proposal is a major amendment to the Fairview Refinement Plan II. The proposed major amendments are as follows:

- Density:** -To increase the 2000 maximum units allowed within the Refinement Plan II
- Lot Standards:** -Exceed the maximum lot depth of 200 feet in the AU zone  
-Exceed the maximum lot depth of 150 feet in the MI zone
- Lot Coverage:** -To exceed the maximum 1,000 square foot building footprint for carports
- Floor Area Ratio:** -To allow 54% FAR where 75% minimum is required
- Setbacks:** -Buildings 27, 28, 29, 30, 31, 32, 33, and 35 will exceed the maximum 20-foot setback within the VC zone along the street side  
-Buildings 20, 23, and 25 will exceed the maximum 20-foot setback within the AU and MI zone along the street side  
-Building 23 will exceed the maximum 20-foot setback within the AU zone along the street side (north of Chapel Drive)  
-Buildings 22, 23, and 24 will exceed the maximum 20-foot setback within the AU and MI zone along the street side (Chapel Drive)  
-Building 26 will exceed the maximum 20-foot setback within the MI zone along the street side (Lindburg Road)  
-Buildings 20 and 21 do not meet the minimum 30-foot rear yard setback  
-The VC zoned development does not meet the minimum 70% of buildings along the buildable width  
-The AU and MI zoned development does not meet the minimum 50% of buildings along the buildable width
- Parking Setbacks:** -To allow a 16-foot setback along the street setback where 20 feet is required  
-To allow a 16-foot setback along the street setback where 20 feet is required adjacent Chapel Drive and future First Street



To allow a 7 foot interior side setback where 10 feet is required

**Parking/Loading:** -To allow 139 parking spaces within the AU and MI zoned development, where 137 maximum are allowed

**Driveways/  
Curb Cuts:**

-Exceeds the maximum 18-foot driveway width allowed within the VC zone

-Exceeds the maximum 16-foot driveway width allowed within the AU and MI zones

-Exceed the maximum 14-foot driveway width allowed within the AU

-To allow 3 curb cuts in the AU zoned development where 2 are allowed

***Applicant Findings-***

**Density:**

Section 3 of the refinement plan (General Allocation and Identification of Major Proposed Land Uses), on page 12, includes the below provision discussing the number of dwelling units proposed:

There is no minimal amount of residential development in any area. However, residential development projects must meet the minimum and maximum density requirements for the area. The FMU zoning does not set a minimum number of residential units, but sets the maximum at 2000 units. The Fairview Plan states an expectation of 1600 units. It is anticipated that this refinement area will have units for about 280 families. Combined with those expected in the other approved refinement plans the total is about 1200. This leaves a wide range for the 14 acres of "Woods" and the 60+ acres owned by Simpson Hills that have not yet been planned.

The applicant is proposing a development consisting of 183 apartment units as shown on the site plans.

083W11A/Tax Lot 1002 (5.97 acres):  
AC (Adaptive Zone)-51 Units  
MI (Mixed-Intensity Zone)-27 Units

083W02/Tax Lot 100 (4.29 acres):  
VC (Village Center Zone)-105 Units

The proposal does increase the density within the Fairview Refinement Plan II. There are currently about 1005 units approved or constructed within approved plans within the Fairview Refinement Plan II.

Pringle Creek-	315 units
Fairview Additions West-	410 units
Fairview Refinement Plan II-	280 units
Total-	1005 units

**Current**

	Acres	Units	Units/Acre
<i>Approved Refinement Plans</i>			
Pringle Creek (2005)	32.5	315	9.7
Fairview Addition West (2014)	50.8	410	8.1
Fairview Refinement Plan II (2016)	74	280	3.8
<b>Total</b>	<b>157.3</b>	<b>1005</b>	<b>6.4</b>
<hr/>			
Total in Fairview Area	276.1	2000	
Remaining	118.8	995	8.4

This leaves a remainder of 995 units for the other properties for proposed future development. Our proposal is for 183 units within the Fairview Refinement Plan II. With our proposed 183 units, a remainder of 818 units will be available for other properties within the Refinement Plan II (Fairview Hills, The Woods Area, Simpson Hill).

**Proposed**

	Acres	Units	Units/Acre
<i>Approved Refinement Plans</i>			
Pringle Creek (2005)	32.5	315	9.7
Fairview Addition West (2014)	50.8	410	8.1
Fairview Refinement Plan II (2021)	74	457	6.2
<b>Total</b>	<b>157.3</b>	<b>1182</b>	<b>7.5</b>
<hr/>			
Total in Fairview Area	276.1	2000	
Remaining	118.8	818	6.9

Remaining Properties

The Woods ( MI)	12.04	84	7.0
The Woods (LI)	2	10	5.0
Fairview Hills (MI)	41.1	288	7.0
Simpson Hills (MI)	53.5	375	7.0
Simpson Hills (LI)	9.2	46	5.0
	<b>117.84</b>	<b>803</b>	<b>6.8</b>

The remaining properties to be developed will only require the need for 803 units as shown on the chart above. Therefore, with our proposal of 183 units, all remaining properties will be able to be developed and meet the density requirements of the Refinement Plan.

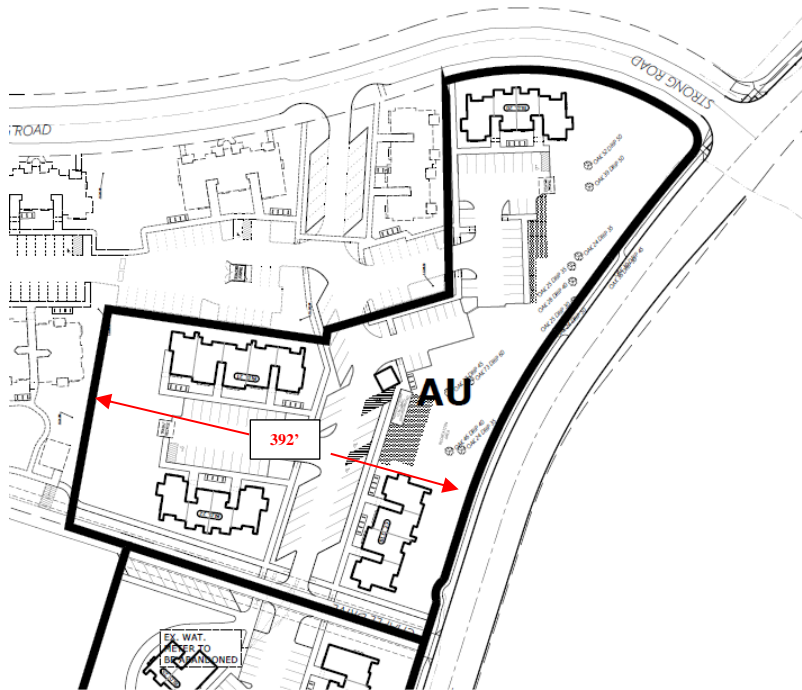
The approval of the subject property and the remaining properties may warrant the need for the maximum number of units within the Fairview Refinement Plan II to be increased by less than 10%. The increase in the maximum density will allow all properties within the Refinement Plan II to successfully develop within their allowed density.

**Lot Standards:**

AU Zone

The subject property is located on Strong Road and identified as 083W11A/Tax Lot 1002. Tax Lot 1002 requires an adjustment to the maximum lot depth requirements. The applicant is requesting an adjustment greater than 20% adjustment to the lot depth standards within the Fairview Refinement Plan:

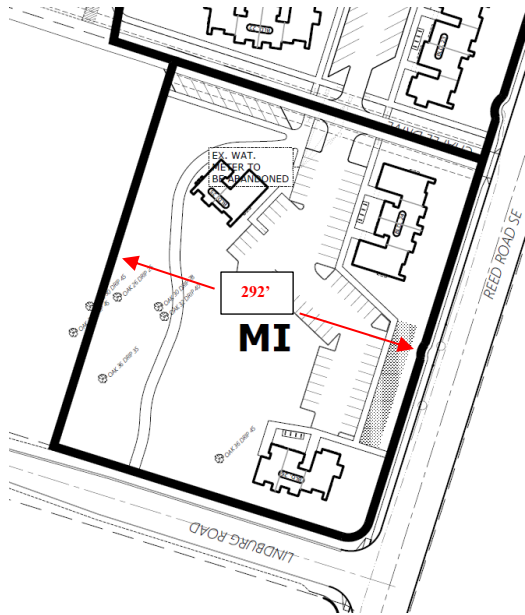
The subject property is located within the AU area. The maximum lot depth allowed in the AU zone is 200 feet. The northern portion of Tax Lot 1002 is about 392 feet in depth. Therefore, an adjustment to this standard is being requested.



MI Zone

The subject property is located on Strong Road and identified as 083W11A/Tax Lot 1002. Tax Lot 1002 requires an adjustment to the maximum lot depth requirements. The applicant is requesting an adjustment greater than 20% adjustment to the lot depth standards within the Fairview Refinement Plan:

The subject property is located within the MI area. The maximum lot depth allowed in the MI zone is 150 feet. The southern portion of Tax Lot 1002 is about 292 feet in depth. Therefore, an adjustment to this standard is being requested.



**Lot Coverage:**

The applicant is requesting to exceed the maximum 1,000 square foot building footprint for carports.

There are several carport structures located through the site that exceed the 1,000 square feet in size. The applicant is requesting an amendment to this standard to allow the proposed carport structures to range in size from 1,035 square feet to 1,727 square feet.

Carports are needed in order to provide covered parking for residents in the Oregon weather. Due to the number of parking stalls and the location of parking, providing smaller carports is not feasible because it would require breaking the parking areas into smaller pods. Which would then reduce the amount of parking on-site.

The carports being provided are standard carports as provided in The Grove Phase I.

**Floor Area Ratio:**

The applicant is requesting to deviate from the required minimum 75% FAR where 54% FAR has been provided.

The floor area of the buildings within the VC zoned development total 99,283 square feet. The VC lot is 4.29 acres in size, therefore, the FAR is 53%.

The VC site is narrow and odd in shape, making it difficult to add additional buildings. Furthermore, since maximum density within the Fairview Refinement Plan is an issue, additional buildings would only exceed density and create development issues with other properties for future development.

Therefore, the FAR within the VC zone cannot be met.

**Setbacks:**

(Front/Side)

Both sites are located zoned FMU (Fairview Mixed-Use) and located within the AU (Adaptive Use), MI (Mixed-Intensity), and VC (village Center) area within the Fairview Refinement Plan.

The applicant is requesting an adjustment greater than 20% adjustment to building setbacks within the Fairview Refinement Plan II:

All buildings abutting a street are required to have a minimum of a 10-foot setback with an allowed 20-foot maximum setback. The following buildings do not meet the setback requirements:

MI Zone

Building 24:	north side-	20' setback from Chapel Drive
Building 25:	north side-	65' setback from Chapel Drive
Building 26:	south side-	22' setback from Lindburg Road

AU Zone

Building 22:	south side-	25' setback from Chapel Drive
Building 23:	east side-	26.73' setback from Reed Road
	south side-	25' setback from Chapel Drive
Building 20:	east side-	110' setback from Reed Road

VC Zone:

Building 27:	north side-	110' setback from VC Loop (private right-of-way)
Building 28:	north side-	195' setback from VC Loop (private right-of-way)
Building 29:	north side-	35' setback from VC Loop (private right-of-way)
Building 30:	north side-	35' setback from VC Loop (private right-of-way)
Building 31:	east side-	74' setback from VC Loop (private right-of-way)
Building 32:	east side-	244' setback from VC Loop (private right-of-way)
Building 33:	east side-	240' setback from VC Loop (private right-of-way)
Building 35:	east side-	35' setback from VC Loop (private right-of-way)

The purpose of this requirement is to create developments that are pedestrian friendly by placing buildings closer to the streets and sidewalks. It creates a safe and convenient environment for residents. All buildings are within a close distance to a lit pathway or parking area, providing a safe environment for the residents when walking to and from the buildings. All buildings have pedestrian friendly access such as paved sidewalks or pedestrian pathways.

(Rear)

All rear yard setback for properties within the AU and MI zone shall have a minimum 30-foot setback. Buildings 20 and 21 do not meet the minimum 30-foot rear yard setback.

Building 20: west side- 15' setback

Building 21: north side- 10' setback

The purpose of this setback is to provide a buffer and privacy between uses. The property to the west and north of Buildings 20 and 21 is The Grove Apartments Phase 1. The proposed development flows into Phase 1 as shown on the site plans. The reduction in setbacks does not affect either development. Both Buildings 20 and 21 provide a buffer and landscaping that will help to provide privacy. However, since the proposed development (The Grove Phase 2) is an extension of The Grove Phase 1, having additional setbacks is not necessary.

(Lot Frontage)

Within the VC zone the Fairview Plan requires a minimum of 70% of the buildable width be occupied by buildings placed on the setback line. The development within the VC zone does not meet the minimum 70% of buildings along the buildable width.

Within the AU and MI zone the Fairview Plan requires a minimum of 50% of the buildable width be occupied by buildings placed on the setback line. The development within the AU and MI zone does not meet the minimum 50% of buildings along the buildable width.

Strong Road (AU and MI zone):

The subject property has 215 feet of buildable width (this excludes required side setbacks and driveway) along Strong Road. The Fairview Plan Code requires a minimum of 50% of the buildable width be occupied by buildings placed on the setback line. As shown on the site plan, the building totals 108 feet of the buildable width along the street frontage. However, the building is not located on the minimum 10-foot setback line. Therefore, occupying 0% of the buildable width of street frontage along Strong Road.

The applicant is requesting an adjustment to this standard.

Strong Road (VC zone):

The subject property has 275 feet of buildable width (this excludes required side setbacks and driveway) along Strong Road. The Fairview Plan Code requires a minimum of 70% of the buildable width be occupied by buildings placed on the setback line. As shown on the site plan, the buildings total 226 feet of the buildable width along the street frontage. However, the building is not located on the minimum 10-foot setback line. Therefore, occupying 0% of the buildable width of street frontage along Strong Road.

The applicant is requesting an adjustment to this standard

Chapel Drive (AU and MI zone):

North side-The subject property has 290 feet of buildable width (this excludes required side setbacks and driveway) along the east side of Chapel Drive. The Fairview Plan Code requires a minimum of 50% of the buildable width be occupied by buildings placed on the setback line. As shown on the site plan, the buildings total 145 feet of the buildable width along the street frontage. Therefore, occupying 50% of the buildable width of street frontage along the east side of Chapel Drive.

Chapel Drive (AU and MI zone):

South side-The subject property has 248 feet of buildable width (this excludes required side setbacks and driveway) along the west side of Chapel Drive. The Fairview Plan Code requires a minimum of 50% of the buildable width be occupied by buildings placed on the setback line. As shown on the site plan, the building totals 51 feet of the buildable width along the street frontage. However, the building is not located on the minimum 10-foot setback line. Therefore, occupying 0% of the buildable width of street frontage along the west side of Chapel Drive.

The applicant is requesting an adjustment to this standard.

Lindburg Road (AU and MI zone):

The subject property has 268 feet of buildable width (this excludes required side setbacks and driveway) along Lindburg Road. The Fairview Plan Code requires a minimum of 50% of the buildable width be occupied by buildings placed on the setback line. As shown on the site plan, the building totals 70 feet of the buildable width along the street frontage. However, the building is not located on the minimum 10-foot setback line. Therefore, occupying 0% of the buildable width of street frontage along Lindburg Road.

The applicant is requesting an adjustment to this standard.

Lindburg Road (VC zone):

The subject property has 182 feet of buildable width (this excludes required side setbacks and driveway) along Lindburg Road. The Fairview Plan Code requires a minimum of 70% of the buildable width be occupied by buildings placed on the setback line. As shown on the site plan, the building totals 95 feet of the buildable width along the street frontage. However, the building is not located on the minimum 10-foot setback line. Therefore, occupying 0% of the buildable width of street frontage along Lindburg Road.

The applicant is requesting an adjustment to this standard.

Reed Road (AU and MI zone):

The subject property has 908 feet of buildable width (this excludes required side setbacks and driveway) along Reed Road. The Fairview Plan Code requires a minimum of 50% of the buildable width be occupied by buildings placed on the setback line. As shown on the site plan, there are no buildings located on the required minimum setback line.

The applicant has requested an adjustment to this standard.

VC Loop (Private right-of-way) (VC zone):

The subject property has 447 feet of buildable width (this excludes required side setbacks and driveway) along VC Loop. The Fairview Plan Code requires a minimum of 70% of the buildable width be occupied by buildings placed on the setback line. As shown on the site plan, there are no buildings located on the required setback line.

The applicant has requested an adjustment to this standard.

The purpose of this requirement is to provide a pedestrian friendly development with building located close to the sidewalks instead of parking areas. The apartment development will provide landscaped areas throughout the site along with pedestrian paths/sidewalks throughout and visual appealing buildings. All of which will create a pedestrian friendly development. Therefore, the reduction of buildings along the setback line will have no effect on the proposed use or surrounding uses.

**Parking Setbacks:**

A minimum 20-foot setback is required along the street. There is a future street proposed along the south property line of the VC zoned development. Due to the odd shape of the lot and the layout, the applicant is only providing a 16-foot setback. Due to circulation, access, and the odd shape of the lot, the 20-foot setback cannot be provided. The applicant is also requesting a 7-foot interior side setback where 10 feet is required for parking.

**Parking/Loading:**

The development within the AU and MI zone is for 78-units. Code requires 1 vehicle parking space per every 1 dwelling unit, with an allowed maximum 1.75 times the minimum number of spaces required. The applicant is required to provide a minimum of 78 on-site vehicle parking spaces. As shown on the site plan, 139 on-site parking spaces are being provided where a maximum 137 on-site parking spaces are allowed.

Total:

60	Standard Parking Stalls
74	Compact Parking Stalls
5	Handicap Parking Stalls



## 139 Total Parking Stalls

The purpose of keeping within the minimum and maximum parking requirements is to avoid large parking lots within developments. In order to minimize large expanses of continuous pavement, 9-foot-wide planter islands have been provided a maximum of every 12 parking spaces.

All parking areas are within the requirements of the code and are separated by planter bays that are a minimum of 9 feet in width. The layout of the parking areas has been taken into consideration and provides for safe and efficient circulation throughout the development. All parking areas are landscaped as required and separated by landscaped bays that meet the minimum width as shown on the site plan. The parking areas and landscaped areas provide for visually appealing apartment grounds.

### VC Zone:

Parking and Driveway Area-61,716s.f.

Landscape w/in Parking-4,846s.f. (8% landscaping within parking areas)

### AU Zone:

Parking and Driveway Area-32,684s.f.

Landscape w/in Parking-5,700s.f. (17% landscaping within parking areas)

### MI Zone:

Parking and Driveway Area-21,255s.f.

Landscape w/in Parking-2,545s.f. (12% landscaping within parking areas)

## **Driveways/Curb Cuts**

AU and MI Zone (serving 15+ stalls)

12-foot minimum

16-foot maximum

AU, MI, and VC Zone (serving 4-14 stalls)

10-foot minimum

14-foot maximum

AU, MI, and VC Zone (serving 1-4 stalls)

8-foot minimum

12-foot maximum

VC Zone (serving 15+ stalls)

10-foot minimum

18-foot maximum

There are several curb cuts with the proposed development that do not meet the maximum 16-foot and 18-foot maximum width. All proposed curb cuts are 20 to 26 feet in width, with all internal driveways being 26 feet within width.

The proposed wider curb-cuts and driveway widths meet fire code or provide for safer and more efficient circulation onto the internal accessways.

The subject property and surrounding properties are zoned located within the Fairview Refinement Plan II. The additional driveway approach widths and 26-foot-wide internal driveways will provide better circulation and traffic flow that will have no effect on the surrounding uses.

The code only allows 2 curb cuts within the AU, MI, and VC zones. The development within the AU zone has 3 curb cuts. Therefore, the applicant is requesting an amendment to allow 3 curb cuts in the AU zoned development where 2 are allowed.

Due to the location of streets within the development and the need for efficient circulation, more than 2 curb cuts is necessary.

# **The Grove Phase 2-Apartments**

## **Design Review**

Revised-May 28, 2021

The following statement addresses the applicable Design Review Standards in the SRC Chapter 702 (Multiple Family Design Review) and the requirements within the Fairview Refinement Plan II. Information provided on the site plans for the Design Review application further address applicable code requirements and the requirements under the Fairview Refinement Plan II.

On September 28, 2020, a Design Review Pre-Application Conference (PRE-AP-20-85) was held with the applicant and City staff to discuss the development of the subject property. Per the pre-app meeting the following land-use application are required:

Class 3-Site Plan Review  
Class 1-Design Review  
Driveway Approach Permit  
Fairview Refinement Plan Major Amendment  
Class 4-Adjustments

On January 13, 2021, the applicant held an Open House with the Morningside Neighborhood Association and adjacent property owners to discuss the proposal and address any concerns.

### **Proposal (Sheet SDR3):**

The subject properties are about 10.29 acres in size, zoned Fairview Mixed-Use (FMU) and located in the Fairview Refinement Plan II (083W11A/Tax Lot 1002 and 083W02/Tax Lot 100).

The applicant is proposing a development consisting of 183 apartment units as shown on the site plans.

083W11A/Tax Lot 1002 (5.97 acres):  
AC (Adaptive Zone)-51 Units  
MI (Mixed-Intensity Zone)-27 Units

083W02/Tax Lot 100 (4.29 acres):  
VC (Village Center Zone)-105 Units

## Fairview Mixed-Use/Adaptive Use Area Chapter 530

*Adaptive Use (AU): The adaptive use area is intended to comprise the highest concentration of existing buildings and historic development patterns. Residential and nonresidential development within this area may occur within existing structures that have been rehabilitated or within new structures.*

Density: The site being developed is 3.36 acres in size. Development in an AU zone shall meet a minimum of 6 dwelling units and shall not exceed 30 dwelling units. Therefore, the site shall be developed with a minimum of 20 and allowed to have a maximum of 100 units. As shown on the site plan, there are 51 units proposed within the AU zone.

The development is in compliance with the minimum and maximum density requirements. See attached site plan.

Setbacks: All minimum setbacks to property lines, between buildings and distances to the entrances are met as shown on the tentative plan. Setbacks are shown on the tentative plan.

North:	Adjacent Strong Road:
	Building 20: 17.4' setback
Northwest:	Building 21: 10' setback
West:	Building 21: 60' setback
	Building 22: 80' setback
East:	Adjacent Reed Road:
	Building 20: 100' setback
	Building 23: 26.73' setback
South:	Adjacent Chapel Drive:
	Building 22: 10' setback
	Building 23: 10' setback

Maximum Height: Maximum building height allowed in the AU area is 45'. All proposed buildings are in compliance with the requirements of the Code.

\*Building 20 is 39.7 feet in height (measured to the highest point) and 34.5 feet in height (measured to the middle of the gable).

\*Building 21 is 40.1 feet in height (measured to the highest) and 34.6 feet in height (measured to the middle of the gable).

\*Building 22 is 39.7 feet in height (measured to the highest) and 34.5 feet in height (measured to the middle of the gable).

\*Building 23 is 40.3 feet in height (measured to the highest) and 34.8 feet in height (measured to the middle of the gable).

Parking: The development within the AU and MI zone is for 78-units. Code requires 1 vehicle parking space per every 1 dwelling unit, with an allowed maximum 1.75 times the minimum number of spaces required. The applicant is required to provide a minimum of 78 on-site vehicle parking spaces. As shown on the site plan, 139 on-site parking spaces are being provided where a maximum 137 on-site parking spaces are allowed.

Total:

60	Standard Parking Stalls
74	Compact Parking Stalls
5	Handicap Parking Stalls
139	Total Parking Stalls

Adequate parking has been provided throughout the development with 1.78 parking spaces per dwelling unit. One loading space has been provided as well.

All parking areas will be served by 26-foot wide internal two-way accessways that run through the development.

Bicycle Parking: The Code requires 0.1 bicycle parking space per dwelling unit. Bike racks will be provided on the site and located in a convenient location for the residents. A total of 78 bicycle parking spaces have been provided on-site via bike racks throughout the site.

Recycling: There are two trash/recycle area provided within the development. One is located on the north and one is located on the south side of the development. The trash receptacles are accessible for all residents via the paved internal sidewalk system in the development. The trash/recycle areas will be screened and enclosed with a sight-obscuring fence or wall. Detail plans for the trash receptacles have been provided.

Stormwater: The Fairview Refinement Plan calls for the stormwater to be infiltrated onsite for storms up through the 20-year storm event. This requirement was based on the assumption that the soil had an infiltration rate 3.1 inches per hour. The onsite soil for both Grove 2 sites have an infiltration rate of 0.7 inches per hour, considerably less than stipulated in the Fairview Refinement plan. Both sites also have considerable topography constraints, with portions of the sites having slopes of 6-10%. The Fairview Refinement Plan Section 23A states "Private on-site detention requirements will satisfy City of Salem standards only if infiltration of the 20-year storm cannot be achieved". Based on the measured infiltration rates of the sites and the topography, infiltration of the 20-year storm cannot be achieved and this section of the Fairview Refinement Plan would apply. The stormwater design for The Grove 2 will meet the current City of Salem Design Standards of treating the new hard surface and providing

detention for ½ the 2, the 10, the 25, and the 100 year storm events to the predeveloped rates.

A stormwater report dated February 22, 2021, has been provided as part of this application submittal.

Lot Coverage: The buildings (29,716sq.ft.) on the site (AU and MI zoned areas) cover 11% (259,974sq.ft.) of the lot. Therefore, lot coverage is under the 55% maximum allowed and in compliance with code.

**AU Zone:**

Area Size-146,345 square feet

Buildings-18,971 square feet

Total Lot Coverage-13%

**MI Zone:**

Area Size-113,629 square feet

Buildings-10,745 square feet

Total Lot Coverage-9%

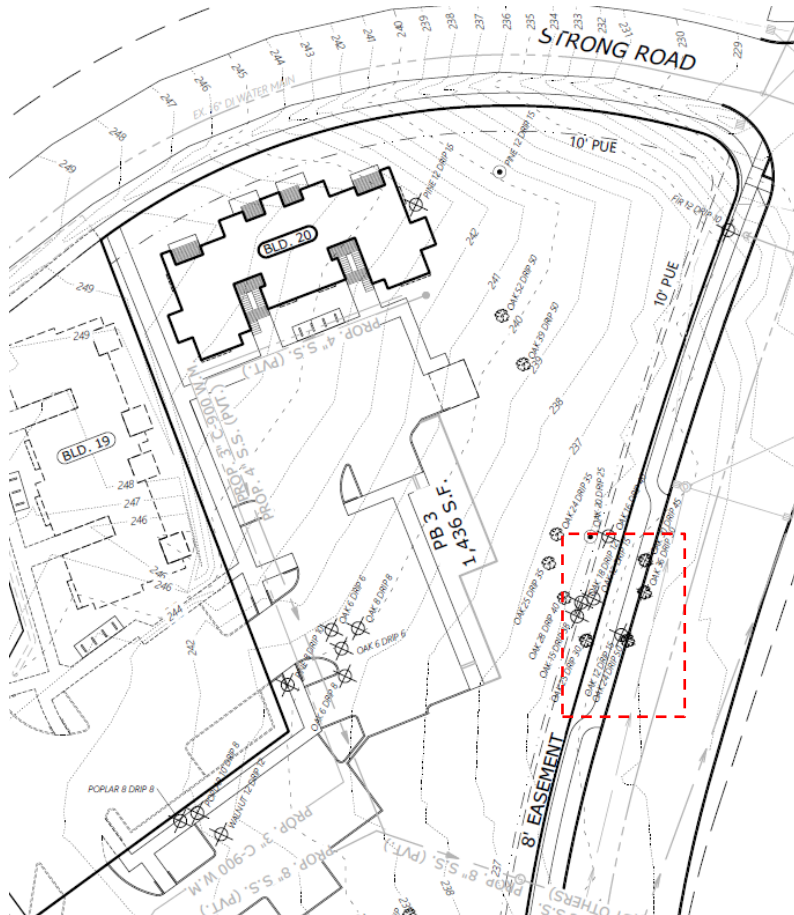
Landscaping: There is a total of 146,424 square feet of landscaped area throughout the development. The net developable area is 259,974 square feet in size. Therefore, the development provides 56% landscaping throughout the site.

<u>AU ZONE</u>	
SITE TOTALS:	
TOTAL AREA	146,345 S.F. (3.36 AC.)
OPEN SPACE:	
COMMON OPEN SPACE	86,428 S.F. (59.05%)
WITHIN PERIMETER BUILDING SETBACKS	21,018 S.F. (14.36%)
PERVIOUS AREA	
LANDSCAPE AREA	78,596 S.F. (53.71%)
LANDSCAPE W/IN PERIMETER SETBACKS	54,494 S.F.
LANDSCAPE W/IN PARKING	18,402 S.F.
5,700 S.F.	
IMPERVIOUS AREA	
PARKING & DRIVEWAY	64,320 S.F. (43.95%)
SIDEWALK	32,684 S.F.
BUILDINGS	12,669 S.F.
18,971 S.F.	
 <u>MI ZONE</u>	
SITE TOTALS:	
TOTAL AREA	113,629 S.F. (2.61 AC.)
OPEN SPACE:	
COMMON OPEN SPACE	74,089 S.F. (65.20%)
WITHIN PERIMETER BUILDING SETBACKS	12,894 S.F. (11.35%)
PERVIOUS AREA	
LANDSCAPE AREA	67,828 S.F. (59.69%)
LANDSCAPE W/IN PERIMETER SETBACKS	52,282 S.F.
LANDSCAPE W/IN PARKING	13,001 S.F.
2,545 S.F.	
IMPERVIOUS AREA	
PARKING & DRIVEWAY	42,545 S.F. (37.44%)
SIDEWALK	21,255 S.F.
BUILDINGS	10,545 S.F.
10,745 S.F.	

**Traffic:** An updated Traffic Impact Analysis (TIA) dated January 12, 2021 has been provided to staff.

**Geo:** A Geotechnical Report dated November 19, 2020, has been provide as part of this application.

**Trees:** There are 71 trees located within the AU, MI, and VC zoned portion of the development. Twenty-five (25) trees are designated for removal. There are eighteen (18) significant Oregon White Oaks on the site. Four (4) of the Oregon White Oaks are designated to be removed. Four (4) of the Oregon White Oaks proposed to be removed within the AU and MI zones are located within the right-of-way.



SRC 808.030(a)(2) state, "A tree and vegetation removal permit is not required for the removal of trees or native vegetation protected under SRC 808.015, SRC 808.020, or SRC 808.025 when the removal is: (B) Required by the City or a public utility for the installation, maintenance, or repair of roads or utilities, including water lines, sewer lines, gas lines, electric lines, and telecommunications lines. This exception does not apply to new development or construction in

*a riparian corridor;”*

In order to provide street connections and required improvements, Reed Road will be improved adjacent to the site. Reed Road provides circulation to and from the site and will require improvements to meet City standards. However, due to the required location of Reed road to the properties and the required improvements, significant trees will have to be removed. The significant trees proposed for removal are within the right-of-way of Reed Road as shown above. Therefore, the removal of the Oregon White Oak meets the exceptions of SRC 808.030(a)(2)(B).

**Fairview Mixed-Use/Mixed-Intensity Chapter 530**

Mixed-Intensity (MI): The Mixed Intensity area is described as comprising, “..primarily residential uses, along with a mix of small-scale neighborhood commercial, employment, and public services uses. Buildings will be a mix of one-story to three-story detached, attached, or stacked housing types sited on smaller individually-owned lots with private yards and street and/or alley access, or larger lots under multiple or separate ownership with shared street and/or alley access. Townhouse development is appropriate at the higher density range.”

Density: The site being developed is 2.61 acres in size. Development in an MI zone shall meet a minimum of 7 dwelling units and shall not exceed 30 dwelling units. Therefore, the site shall be developed with a minimum of 18 and allowed to have a maximum of 78 units. As shown on the site plan, there are 27 units proposed within the MI zone.

The development is in compliance with the minimum and maximum density requirements. See attached site plan.

Setbacks: All minimum setbacks to property lines, between buildings and distances to the entrances are met as shown on the tentative plan. Setbacks are shown on the tentative plan.

North:	Adjacent Chapel Drive:
	Building 24: 20’ setback
	Building 25: 65’ setback
	Parking: 20’ setback
East:	Adjacent Reed Road
	Building 24: 20’ setback
	Building 26: 20’ setback
	Parking: 35’ setback
West:	Building 25: 35’ setback
South:	Adjacent Lindburg Road:
	Building 26: 20’ setback



Maximum Height: Maximum building height allowed in the MI area is 45'. All proposed buildings are in compliance with the requirements of the Code.

\*Building 24 is 41.5 feet in height (measured to the highest point) and 35.4 feet in height (measured to the middle of the gable).

\*Building 25 is 39.7 feet in height (measured to the highest point) and 34.5 feet in height (measured to the middle of the gable).

\*Building 26 is 39.7 feet in height (measured to the highest point) and 34.5 feet in height (measured to the middle of the gable).

Parking: The development within the AU and MI zone is for 78-units. Code requires 1 vehicle parking space per every 1 dwelling unit, with an allowed maximum 1.75 times the minimum number of spaces required. The applicant is required to provide a minimum of 78 on-site vehicle parking spaces. As shown on the site plan, 139 on-site parking spaces are being provided where a maximum 137 on-site parking spaces are allowed.

Total:

60	Standard Parking Stalls
74	Compact Parking Stalls
5	Handicap Parking Stalls
139	Total Parking Stalls

Adequate parking has been provided throughout the development with 1.78 parking spaces per dwelling unit. One loading space has been provided as well.

All parking areas will be served by 26-foot wide internal two-way accessways that run through the development.

Bicycle Parking: The Code requires 0.1 bicycle parking space per dwelling unit. Bike racks will be provided on the site and located in a convenient location for the residents. A total of 78 bicycle parking spaces have been provided on-site via bike racks throughout the site.

Recycling: There are two trash/recycle area provided within the development. One is located on the north and one is located on the south side of the development. The trash receptacles are accessible for all residents via the paved internal sidewalk system in the development. The trash/recycle areas will be screened and enclosed with a sight-obscuring fence or wall. Detail plans for the trash receptacles have been provided.

Stormwater: The Fairview Refinement Plan calls for the stormwater to be infiltrated onsite for storms up through the 20-year storm event. This requirement was based on the assumption

that the soil had an infiltration rate 3.1 inches per hour. The onsite soil for both Grove 2 sites have an infiltration rate of 0.7 inches per hour, considerably less than stipulated in the Fairview Refinement plan. Both sites also have considerable topography constraints, with portions of the sites having slopes of 6-10%. The Fairview Refinement Plan Section 23A states “Private on-site detention requirements will satisfy City of Salem standards only if infiltration of the 20-year storm cannot be achieved”. Based on the measured infiltration rates of the sites and the topography, infiltration of the 20-year storm cannot be achieved and this section of the Fairview Refinement Plan would apply. The stormwater design for The Grove 2 will meet the current City of Salem Design Standards of treating the new hard surface and providing detention for ½ the 2, the 10, the 25, and the 100 year storm events to the predeveloped rates.

A stormwater report dated February 22, 2021, has been provided as part of this application submittal.

Lot Coverage: The buildings (29,716sq.ft.) on the site (AU and MI zoned areas) cover 11% (259,974sq.ft.) of the lot. Therefore, lot coverage is under the 55% maximum allowed and in compliance with code.

**AU Zone:**

Area Size-146,345 square feet  
 Buildings-18,971 square feet  
 Total Lot Coverage-13%

**MI Zone:**

Area Size-113,629 square feet  
 Buildings-10,745 square feet  
 Total Lot Coverage-9%

Landscaping: There is a total of 146,424 square feet of landscaped area throughout the development. The net developable area is 259,974 square feet in size. Therefore, the development provides 56% landscaping throughout the site.

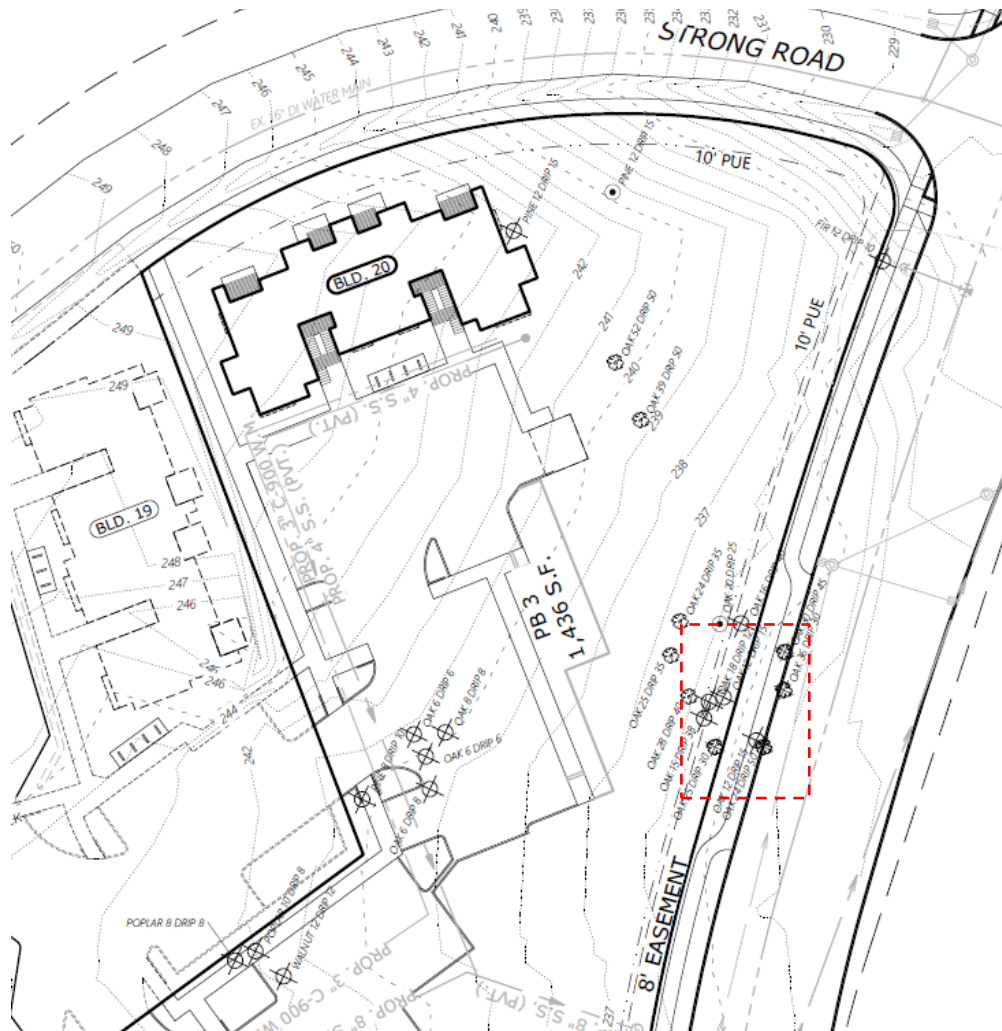
<u>AU ZONE</u>	
SITE TOTALS:	
TOTAL AREA	146,345 S.F. (3.36 AC.)
OPEN SPACE:	
COMMON OPEN SPACE	86,428 S.F. (59.05%)
WITHIN PERIMETER BUILDING SETBACKS	21,018 S.F. (14.36%)
PERVIOUS AREA	
LANDSCAPE AREA	78,596 S.F. (53.71%)
LANDSCAPE W/IN PERIMETER SETBACKS	54,494 S.F.
LANDSCAPE W/IN PARKING	18,402 S.F.
LANDSCAPE W/IN PARKING	5,700 S.F.
IMPERVIOUS AREA	
PARKING & DRIVEWAY	64,320 S.F. (43.95%)
SIDEWALK	32,684 S.F.
BUILDINGS	12,669 S.F.
BUILDINGS	18,971 S.F.

MI ZONE	
SITE TOTALS:	
TOTAL AREA	113,629 S.F. (2.61 AC.)
OPEN SPACE:	
COMMON OPEN SPACE	74,089 S.F. (65.20%)
WITHIN PERIMETER BUILDING SETBACKS	12,894 S.F. (11.35%)
PERVIOUS AREA	
LANDSCAPE AREA	67,828 S.F. (59.69%)
LANDSCAPE W/IN PERIMETER SETBACKS	52,282 S.F.
LANDSCAPE W/IN PARKING	13,001 S.F.
	2,545 S.F.
IMPERVIOUS AREA	
PARKING & DRIVEWAY	42,545 S.F. (37.44%)
SIDEWALK	21,255 S.F.
BUILDINGS	10,545 S.F.
	10,745 S.F.

**Traffic:** An updated Traffic Impact Analysis (TIA) dated January 12, 2021, has been provided to staff.

**Geo:** A Geotechnical Report dated November 19, 2020, has been provide as part of this application.

**Trees:** There are 71 trees located within the AU, MI, and VC zoned portion of the development. Twenty-five (25) trees are designated for removal. There are eighteen (18) significant Oregon White Oaks on the site. Four (4) of the Oregon White Oaks are designated to be removed. Four (4) of the Oregon White Oaks proposed to be removed within the AU and MI zones are located within the right-of-way.



SRC 808.030(a)(2) state, “A tree and vegetation removal permit is not required for the removal of trees or native vegetation protected under SRC 808.015, SRC 808.020, or SRC 808.025 when the removal is: (B) Required by the City or a public utility for the installation, maintenance, or repair of roads or utilities, including water lines, sewer lines, gas lines, electric lines, and telecommunications lines. This exception does not apply to new development or construction in a riparian corridor;”

In order to provide street connections and required improvements, Reed Road will be improved adjacent to the site. Reed Road provides circulation to and from the site and will require improvements to meet City standards. However, due to the required location of Reed road to the properties and the required improvements, significant trees will have to be removed. The significant trees proposed for removal are within the right-of-way of Reed Road as shown above. Therefore, the removal of the Oregon White Oak meets the exceptions of SRC 808.030(a)(2)(B).

**Fairview Mixed-Use/Village Center Chapter 530**

*Village Center (VC): The village center area comprises the most intense and pedestrian-oriented residential, commercial, employment, and public services uses. Residential uses will have densities of no less than 16 dwelling units per net acre. Nonresidential uses include a mix of large and small scale commercial establishments, which cumulatively will be limited to not more than approximately 80,000 square feet of pedestrian-oriented retail. Office uses are encouraged.*

**Density:** The site being developed is 4.29 acres in size. Development in an VC zone shall meet a minimum of 13 dwelling units and shall not exceed 35 dwelling units. Therefore, the site shall be developed with a minimum of 55 and allowed to have a maximum of 150 units. As shown on the site plan, there are 105 units proposed within the VC zone.

The development is in compliance with the minimum and maximum density requirements. See attached site plan.

**Setbacks:** All minimum setbacks to property lines, between buildings and distances to the entrances are met as shown on the tentative plan. Setbacks are shown on the tentative plan.

- Northwest: Adjacent Strong Road
  - Building 34: 10' setback
  - Building 35: 13' setback
  - Parking: 20' setback
- Northeast: Adjacent VC Loop
  - Building 27: 100' setback
  - Building 29: 30' setback
  - Building 30: 30' setback
- East: Adjacent Lindburg Road
  - Building 27: 20' setback
- South: Building 32: 10' setback
  - Parking: 16' setback
- West: Building 32: 17' setback
  - Building 33: 14' setback
  - Building 34: 14' setback

**Maximum Height:** Maximum building height allowed in the VC area is 45'. All proposed buildings are in compliance with the requirements of the Code.

\*Building 27 is 40.5 feet in height (measured to the highest point) and 34.3 feet in height (measured to the middle of the gable).

\*Building 28 is 40 feet in height (measured to the highest point) and 34.5 feet in height (measured to the middle of the gable).

\*Building 29 is 40.1 feet in height (measured to the highest point) and 34.6 feet in height (measured to the middle of the gable).

\*Building 30 is 39.7 feet in height (measured to the highest point) and 34.5 feet in height (measured to the middle of the gable).

\*Building 31 is 40.5 feet in height (measured to the highest point) and 34.3 feet in height (measured to the middle of the gable).

\*Building 32 is 39.7 feet in height (measured to the highest point) and 34.5 feet in height (measured to the middle of the gable).

\*Building 33 is 39.7 feet in height (measured to the highest point) and 34.5 feet in height (measured to the middle of the gable).

\*Building 34 is 39.7 feet in height (measured to the highest point) and 34.5 feet in height (measured to the middle of the gable).

\*Building 35 is 40.3 feet in height (measured to the highest point) and 34.8 feet in height (measured to the middle of the gable).

Parking: The development within the VC zone is for a 105-unit apartment complex. The Refinement Plan Code requires 1 vehicle parking spaces per every 1 dwelling unit. The applicant is required to provide a minimum of 105 on-site vehicle parking spaces. As shown on the site plan, 173 on-site parking spaces are being provided. The maximum parking spaces allowed is 184.

173 parking spaces provided:

- 82 Standard Parking Stalls
- 81 Compact
- 4 Compact (off-site)
- 6 Handicap Parking Stalls

Adequate parking has been provided throughout this portion of the development with 1.65 parking spaces per dwelling unit. Loading spaces have been provided as well.

All parking areas will be served by 26-foot wide internal two-way accessways that run through the development.

**Bicycle Parking:** The Code requires 0.1 bicycle parking space per dwelling unit. Bike racks will be provided on the site and located in a convenient location for the residents. A total of 106 bicycle parking spaces have been provided on-site.

**Recycling:** There are two trash/recycle area provided within the development. One is located on the east and one is located on the west side of the development. The trash receptacles are accessible for all residents via the paved internal sidewalk system in the development. The trash/recycle areas will be screened and enclosed with a sight-obscuring fence or wall. Detail plans for the trash receptacles have been provided.

**Stormwater:** The Fairview Refinement Plan calls for the stormwater to be infiltrated onsite for storms up through the 20-year storm event. This requirement was based on the assumption that the soil had an infiltration rate 3.1 inches per hour. The onsite soil for both Grove 2 sites have an infiltration rate of 0.7 inches per hour, considerably less than stipulated in the Fairview Refinement plan. Both sites also have considerable topography constraints, with portions of the sites having slopes of 6-10%. The Fairview Refinement Plan Section 23A states "Private on-site detention requirements will satisfy City of Salem standards only if infiltration of the 20-year storm cannot be achieved". Based on the measured infiltration rates of the sites and the topography, infiltration of the 20-year storm cannot be achieved and this section of the Fairview Refinement Plan would apply. The stormwater design for The Grove 2 will meet the current City of Salem Design Standards of treating the new hard surface and providing detention for ½ the 2, the 10, the 25, and the 100 year storm events to the predeveloped rates.

A stormwater report dated February 22, 2021, has been provided as part of this application submittal.

**Lot Coverage:** The buildings on the site cover 22% (42,013sq.ft.) of the lot. Therefore, lot coverage is under the 55% maximum allowed and in compliance with code.

**Landscaping:** The net developable area is 187,066 (4.29 acres) square feet in size with 64,337 square feet of landscaping throughout. Therefore, the development provides 34% landscaping throughout the site.

<u>VC ZONE</u>	
SITE TOTALS:	
TOTAL AREA	187,066 S.F. (4.29 AC.)
OPEN SPACE:	
COMMON OPEN SPACE	69,631 S.F. (37.22%)
WITHIN PERIMETER BUILDING SETBACKS	20,250 S.F. (10.83%)
PERVIOUS AREA	
LANDSCAPE AREA	64,337 S.F. (34.39%)
LANDSCAPE W/IN PERIMETER SETBACKS	42,394 S.F.
LANDSCAPE W/IN PARKING	17,097 S.F.
	4,846 S.F.
IMPERVIOUS AREA	
PARKING & DRIVEWAY	122,729 S.F. (65.61%)
SIDEWALK	61,716 S.F.
BUILDINGS	19,000 S.F.
	42,013 S.F.

**Traffic:** An updated Traffic Impact Analysis (TIA) dated January 14, 2021, has been provide to

staff.

Geo: A Geotechnical Report dated November 19, 2020, has been provide as part of this application.

Trees: There are 71 trees located within the AU, MI, and VC zoned portion of the development. Twenty-five (25) trees are designated for removal. There are eighteen (18) significant Oregon White Oaks on the site. Four (4) of the Oregon White Oaks are designated to be removed. Four (4) of the Oregon White Oaks proposed to be removed within the AU and MI zones are located within the right-of-way.

**Multiple Family Design Review Standards- Chapter 702**

**702.020(a):**

702.020(a)(1) Open Space (Sheet SDR8 & SDR9): In multi-family developments, a portion of the land not covered by buildings and parking shall be of adequate size and shape and in the proper location to be functional for outdoor recreation and relaxation. The standards are also intended to ensure that open space is an integral part of the overall development design. Per the code the proposed development shall provide a minimum 30% open space. The minimum open space area required for this development is 132,117 square feet of the net developable area.

AU/MI Zone: The minimum open space are required for this portion of the development is 76,056 square feet. The portion of the subject property being developed in the AU and MI zone is 260,053 (5.97 acres) square feet in size with 158,517 square feet of open space. Therefore, totaling 61% open space.

<u>AU_ZONE</u>	
SITE TOTALS:	
TOTAL AREA	146,345 S.F. (3.36 AC.)
OPEN SPACE:	
COMMON OPEN SPACE	86,428 S.F. (59.05%)
WITHIN PERIMETER BUILDING SETBACKS	21,018 S.F. (14.36%)
 <u>MI_ZONE</u>	
SITE TOTALS:	
TOTAL AREA	113,629 S.F. (2.61 AC.)
OPEN SPACE:	
COMMON OPEN SPACE	74,089 S.F. (65.20%)
WITHIN PERIMETER BUILDING SETBACKS	12,894 S.F. (11.35%)

VC Zone: The minimum open space are required for this portion of the development is 56,119 square feet. The portion of the subject property being developed in the VC zone is 187,066 (4.29 acres) square feet in size with 69,631 square feet of open space. Therefore, totaling 37% open space.



VC ZONE  
SITE TOTALS:  
TOTAL AREA \_\_\_\_\_ 187,066 S.F. (4.29 AC.)  
  
OPEN SPACE:  
COMMON OPEN SPACE \_\_\_\_\_ 69,631 S.F. (37.22%)  
WITHIN PERIMETER BUILDING SETBACKS \_\_\_\_\_ 20,250 S.F. (10.83%)

The open space areas within both developed areas included open recreational landscaped areas, along with tot lots, and a pedestrian pathway. The pedestrian system connects the buildings to the public sidewalk system and adjacent properties as required by the Fairview Refinement Plan II.

Therefore, this standard has been met. See attached site plans and open space plan.

702.020(a)(1)(A) Common Open Space Area Size and Dimensions (Sheet SDR4):

The applicant is proposing a development consisting of 183-apartment units as shown on the site plans. The apartments will be developed with the AU, MI, and VC zoned portions of the Fairview Refinement Plan II and consists of 10.29 acres.

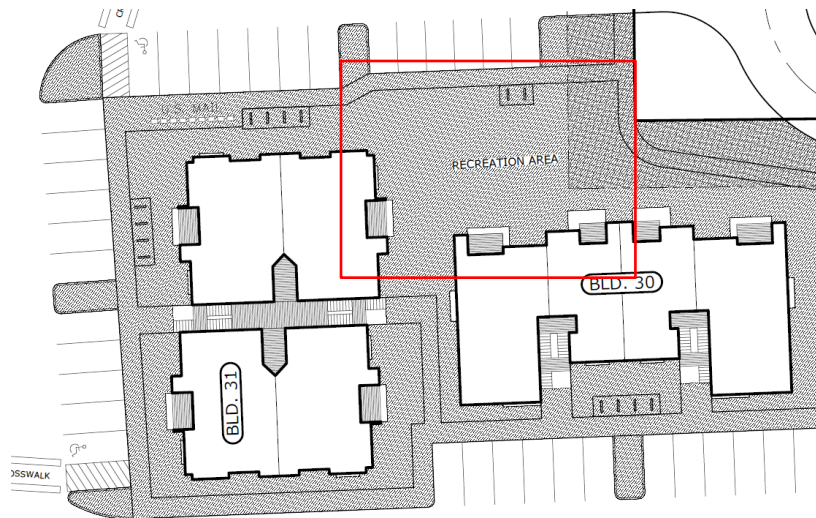
Per Table 702-3, the minimum usable common open space area required for this development is 40,250 square feet of the site. The portion of the subject property being developed is zoned AU, MI, VC and is 448,232 (10.29 acres) square feet in size with 175,986 square feet (does not include open space within the perimeter setbacks) of common open space, including landscaped areas, a tot lot, and pedestrian paths. Therefore, totaling 39% open space.

AU/MI Zone: (78-units): 1,725 square feet of open space required  
Site Area-260,053 square feet (5.97 acres)  
Open Space-129,605s.f. (49%/excludes open space within setbacks)  
Open Space w/in Perimeter Setbacks-33,912s.f.  
Landscaped Areas-146,424s.f. (56%)  
Parking and Driveway Area-53,939s.f.  
Landscape w/in Parking-8,254s.f. (15% landscaping within parking areas)



Common open space areas have been provided through the development that meet size and dimension requirements.

- VC Zone: (105-units): 1,988 square feet of open space required
- Site Area-187,066 square feet (4.29 acres)
- Open Space-49,381s.f. (26%/excludes open space within setbacks)
- Open Space w/in Perimeter Setbacks-20,250s.f.
- Landscaped Areas-64,337s.f. (34%)
- Parking and Driveway Area-61,716s.f.
- Landscape w/in Parking-4,846s.f. (8% landscaping within parking areas)



Common open space areas have been provided through the development that meet size and dimension requirements.

702.020(a)(1)(B): As shown on the site plan all open space is usable open space. The existing conditions plan, and grading plan identify all the slopes throughout the site.

702.020(a)(1)(C) and (D): Private Open Space: Each unit will have private open space as required by code. Ground floor units will have patio areas that are 96 square feet in size, with no dimension less than 6 feet. All second and third story units will have balconies/decks that are a minimum 60 square feet in size. All private open space areas are located contiguous to the dwelling unit and will be screened with 5-foot-high landscaping. This private open space includes the patios and balconies/decks. Therefore, this standard has been met.

702.020(a)(1)(E): In multi-family developments, a portion of the land not covered by buildings and parking shall be of adequate size and shape and in the proper location to be functional for outdoor recreation and relaxation. The standards are also intended to ensure that usable open space is an integral part of the overall development design. As shown on the site plan, there is a usable open space area located within the development. The proposed development within the Refinement Plan II provided more than adequate usable open space areas that are landscaped. A 6- to 10-foot-wide pedestrian path through the development is also provided for an open space amenity. The pedestrian system connects the buildings to the public sidewalk system and adjacent properties as required by the Fairview Refinement Plan II.

702.020(a)(1)(F): The subject properties are located within the Fairview Refinement Plan II, where parks and open space areas have been identified. There is a proposed community parking located within a ½ mile from both developments. Therefore, due to the location of the development within the Refinement Plan, there is a park located within ½ a mile of the

subject properties.



**702.020(b)(1) and 702.020(b)(2) Landscaping Standards:** The subject property does not abut RA or RS zoned properties. Landscaping is being provided adjacent all property lines and within the interior of the development. Landscaping has been provided throughout the site as identified on the landscape plans. A minimum of 1 tree will be planted for every 2,000 square feet of the site. New trees and vegetation have been provided throughout the development as shown on the landscape plans. There is 210,761 square feet of landscaped area throughout the site. Therefore, 47% of the site is landscaped. Landscape plans have been provided and demonstrate how the landscape standards have been met.

**AU and MI zone Landscaping:** There is a total of 146,424 square feet of landscaped area throughout the development. The net developable area is 260,053 square feet in size. Therefore, the development provides 56% landscaping throughout the site.

**VC zone Landscaping:** The net developable area is 187,066 (4.29 acres) square feet in size with 64,337 square feet of landscaping throughout. Therefore, the development provides 34% landscaping throughout the site.

A permanent underground irrigation system will be provided when development plans are final.

702.020(b)(3), (4), (5): Landscaping has been provided around the proposed buildings as shown on the plans.

702.020(b)(6): All private ground level open space areas are located contiguous to the dwelling unit and will be screened with 5-foot-high landscaping. This will help to ensure privacy for the patio areas designated as private open space for residents.

702.020(b)(7) and (8): All parking areas are within the requirements of the code and are separated by planter bays that are a minimum of 9 feet in width. All parking areas are landscaped as required and separated by landscaped bays that meet the minimum width as shown on the site plan. The parking areas and landscaped areas provide for visually appealing apartment grounds. There is 13,100 square feet of landscaping within the parking areas. This does not include the landscaped areas surrounding the parking areas.

AU and MI Zone:

Parking and Driveway Area-53,939s.f.

Landscape w/in Parking-8,245s.f. (15% landscaping within parking areas)

VC Zone:

Parking and Driveway Area-61,716s.f.

Landscape w/in Parking-4,846s.f. (8% landscaping within parking areas)

Interior Parking Lot Landscaping: The proposal is for 183-units; therefore, the development is exempt from the requirements of SRC 806.035(d)(2).

702.020(c)(1) and (2) Site Safety and Security Standards: All buildings have windows provided in habitable rooms and windows that face the parking lots and open space areas. This helps provide an eye on the development. Lighting on the buildings and along the sidewalks will be provided as well.

702.020(c)(3) and (4): Safety of the residents is very important, and all requirements are met to assure safety and compliance with code. There are no fences or plant materials located in areas within the development that obstruct visibility. All landscaping adjacent to open space areas will not exceed 3 feet in height.

702.020(d)(1), (2), and (4) Parking and Site Design Standards: The subject properties have street frontage on Strong Road, Lindburg Road, Reed Road, and Chapel Drive. Internal accessways are proposed within the development and meet City standards.

As shown on Sheet SDR4, some of the parking areas are greater than 6,700 square feet in area. In order to minimize large expanses of continuous pavement, 9-foot wide planter islands have

been provided a maximum of every 12 parking spaces. Therefore, meeting the code.

All parking areas are within the requirements of the code and are separated by planter bays that are a minimum of 9 feet in width. The layout of the parking areas has been taken into consideration and provides for safe and efficient circulation throughout the development. In order to take into consideration circulation, pedestrian access, landscaping, and the requirements of the code, the parking areas have been carefully designed. All parking areas are landscaped as required and separated by landscaped bays that meet the minimum width as shown on the site plan. The parking areas and landscaped areas provide for visually appealing apartment grounds. There is 13,091 square feet of landscaping within the parking areas. This does not include the landscaped areas surrounding the parking areas.

AU and MI Zone:

Parking and Driveway Area-53,939s.f.

Landscape w/in Parking-8,245s.f. (15% landscaping within parking areas)

VC Zone:

Parking and Driveway Area-61,716s.f.

Landscape w/in Parking-4,846s.f. (8% landscaping within parking areas)

The development does provide setbacks between the buildings and pathways. All the pathways connect the buildings, open space, parking areas, and surrounding uses.

Therefore, meeting this standard.

702.020(e)(1) through (8) Façade and Building Design: These guidelines are intended to promote building and site design that contributes positively to a sense of neighborhood and to the overall streetscape by carefully relating building mass, entries and yards to public streets.

All buildings are under the 150 maximum building length allowed. The height and length of the buildings and structures conform to the measuring requirements in code.

The building design does not have long flat walls or roof lines. The buildings will have an offset that breaks up the front of the buildings and the roof lines. Balconies (decks) and dormers are incorporated in the building design to add some visual element to the buildings. Therefore, breaking up the long building length.

All buildings face the interior of the lot. The rear side of Buildings 20, 34, and 35 face Strong Road; the rear side of Building 22 faces Chapel Drive; the rear side of Buildings 23 and 24 face Reed Road; the rear side of Building 26 faces Lindburg Road; and the rear side of Buildings 29 and 30 face VC Loop (private right-of-way). The street side of these buildings (rear) will be designed to be visually appealing, by providing similar design as is being provided for the front

building facade for all buildings. In order to be consistent with the front facade of the building; windows, offsets, and architectural features will be incorporated in the portions of the building facing the right-of-way. There are no balconies facing the RA zoned properties. All of which will help to provide privacy.

In order to provide safety and convenience for the residents, all buildings will face the interior of the lot. This provides more eyes on the site, which provides greater safety for residents. This also provides convenience since all parking is located within the interior of the site.

The subject property does not abut any RA zoned properties.

**Strong Road (AU and MI zone):**

The subject property has 215 feet of buildable width (this excludes required side setbacks and driveway) along Strong Road. The Fairview Plan Code requires a minimum of 50% of the buildable width be occupied by buildings placed on the setback line. As shown on the site plan, the building totals 108 feet of the buildable width along the street frontage. Therefore, occupying 50% of the buildable width of street frontage along Strong Road.

**Strong Road (VC zone):**

The subject property has 275 feet of buildable width (this excludes required side setbacks and driveway) along Strong Road. The Fairview Plan Code requires a minimum of 70% of the buildable width be occupied by buildings placed on the setback line. As shown on the site plan, the buildings total 226 feet of the buildable width along the street frontage. Therefore, occupying 82% of the buildable width of street frontage along Strong Road.

**Chapel Drive (AU and MI zone):**

East side-The subject property has 290 feet of buildable width (this excludes required side setbacks and driveway) along the east side of Chapel Drive. The Fairview Plan Code requires a minimum of 50% of the buildable width be occupied by buildings placed on the setback line. As shown on the site plan, the buildings total 145 feet of the buildable width along the street frontage. Therefore, occupying 50% of the buildable width of street frontage along the east side of Chapel Drive.

**Chapel Drive (AU and MI zone):**

West side-The subject property has 248 feet of buildable width (this excludes required side setbacks and driveway) along the west side of Chapel Drive. The Fairview Plan Code requires a minimum of 50% of the buildable width be occupied by buildings placed on the setback line. As shown on the site plan, the building totals 51 feet of the buildable width along the street frontage. Therefore, occupying 21% of the buildable width of street frontage along the west side of Chapel Drive. The applicant is requesting an amendment to this standard.

**Lindburg Road (AU and MI zone):**

The subject property has 268 feet of buildable width (this excludes required side setbacks and driveway) along Lindburg Road. The Fairview Plan Code requires a minimum of 50% of the buildable width be occupied by buildings placed on the setback line. As shown on the site plan, the building totals 70 feet of the buildable width along the street frontage. Therefore, occupying 26% of the buildable width of street frontage along Lindburg Road. The applicant is requesting an amendment to this standard.

**Lindburg Road (VC zone):**

The subject property has 182 feet of buildable width (this excludes required side setbacks and driveway) along Lindburg Road. The Fairview Plan Code requires a minimum of 70% of the buildable width be occupied by buildings placed on the setback line. As shown on the site plan, the building totals 95 feet of the buildable width along the street frontage. Therefore, occupying 52% of the buildable width of street frontage along Lindburg Road. The applicant is requesting an amendment to this standard.

**Reed Road (AU and MI zone):**

The subject property has 908 feet of buildable width (this excludes required side setbacks and driveway) along Reed Road. The Fairview Plan Code requires a minimum of 50% of the buildable width be occupied by buildings placed on the setback line. As shown on the site plan, there are no buildings located on the required setback line. The applicant has requested an adjustment to this standard.

**VC Loop (Private right-of-way) (VC zone):**

The subject property has 447 feet of buildable width (this excludes required side setbacks and driveway) along VC Loop. The Fairview Plan Code requires a minimum of 70% of the buildable width be occupied by buildings placed on the setback line. As shown on the site plan, there are no buildings located on the required setback line. The applicant has requested an adjustment to this standard.

All roof-mounted equipment will be screened and integrated into the building design. Further review of this requirement will take place at the time of building permits.

All buildings have entrances physically and visually connected to the internal public sidewalk system and the parking lots. All external stairways are recessed into the buildings. Therefore, physically and visually incorporating them into the building's architecture design. The primary entrances for each individual unit are provided through a covered entry way. All building entries are clearly defined and easily accessible. The design of the building with the use of roofline offsets and covered entry ways, promote a positive sense of neighborhood.



702.020(e)(9) and (10) (See Building Elevations): Varied materials, textures, and colors are being used on the buildings. The applicant has provided building elevations to show how this is being complied with. The materials used on the front, rear, and sides of the apartments are the same; shake siding, trim board, and lap siding. See attached building elevations.

The proposed buildings proved the required offsets and design elements as shown on the building elevations. The subject property does not abut any RA or RS zoned property. All properties surrounding the site are within the Fairview Refinement area and zoned FMU.

The primary entrances for each individual unit is provided through a covered entry way. All building entries are clearly defined and easily accessible. The design of the building with the use of roofline offsets and covered entry ways, promote a positive sense of neighborhood. All building entrances face the internal street/parking system.

The Fairview Refinement Plan II (Page 24, Table 2, Building mass and Façade/Compatibility), requires the following: “Vertical to 28 feet in height, plus 45 degrees additional setback above 28 feet”. The intent of this compatibility requirement is to provide a well scaled and inviting building. As stated throughout, the design of the building provides offsets, windows, and a variety of materials to create a well scaled and inviting building, without meeting this requirement.

**The proposed buildings provide offsets:**





**Covered Decks and Recessed Balconies:**



**Covered Entrance:**



## **The Grove Phase 2**

### **Class 3-Site Plan Review**

*Revised-May 28, 2021*

#### **SRC 220.005(f)(3) Class 3 Site Plan Review Criteria:**

##### ***(A) The application meets all applicable standards of the UDC;***

**Applicant Findings:** The applicant has addressed all applicable standards and guidelines per the UDC as shown on the site plan. The applicant is proposing the development of 10.26 acres of Fairview Mixed-Use (FMU) zoned property within the Fairview Refinement Plan II. The subject properties are within the AU (Adaptive Use), MI (Mixed-Intensity), and the VC (Village Center) overlay areas within the FMU zone. The applicant is proposing to develop the site with 183 apartment units as shown on the site plans.

The applicant is meeting the standards and guidelines of the Multi-Family Design Handbook (SRC Chapter 702) and the Fairview Refinement Plan II. All standards and guidelines have been addressed and met as outlined within the narrative and on the site plans.

All applicable standards have been outlined below and on the attached site plans.

#### **Fairview Mixed-Use/Adaptive Use Area Chapter 530**

**Adaptive Use (AU):** The adaptive use area is intended to comprise the highest concentration of existing buildings and historic development patterns. Residential and nonresidential development within this area may occur within existing structures that have been rehabilitated or within new structures.

**Density:** The site being developed is 3.36 acres in size. Development in an AU zone shall meet a minimum of 6 dwelling units and shall not exceed 30 dwelling units. Therefore, the site shall be developed with a minimum of 20 and allowed to have a maximum of 101 units. As shown on the site plan, there are 51 units proposed within the AU zone.

The development is in compliance with the minimum and maximum density requirements. See attached site plan.

**Setbacks:** All minimum setbacks to property lines, between buildings and distances to the entrances are met as shown on the tentative plan. Setbacks are shown on the tentative plan.

North:	Adjacent Strong Road:
	Building 20: 17.4' setback
Northwest:	Building 21: 10' setback
West:	Building 21: 60' setback
	Building 22: 80' setback
East:	Adjacent Reed Road:
	Building 20: 100' setback
	Building 23: 26.73' setback
South:	Adjacent Chapel Drive:
	Building 22: 10' setback
	Building 23: 10' setback

Maximum Height: Maximum building height allowed in the AU area is 45'. All proposed buildings are in compliance with the requirements of the Code.

\*Building 20 is 39.7 feet in height (measured to the highest point) and 34.5 feet in height (measured to the middle of the gable).

\*Building 21 is 40.1 feet in height (measured to the highest) and 34.6 feet in height (measured to the middle of the gable).

\*Building 22 is 39.7 feet in height (measured to the highest) and 34.5 feet in height (measured to the middle of the gable).

\*Building 23 is 40.3 feet in height (measured to the highest) and 34.8 feet in height (measured to the middle of the gable).

Parking: The development within the AU and MI zone is for 78-units. Code requires 1 vehicle parking space per every 1 dwelling unit, with an allowed maximum 1.75 times the minimum number of spaces required. The applicant is required to provide a minimum of 78 on-site vehicle parking spaces. As shown on the site plan, 139 on-site parking spaces are being provided where a maximum 137 on-site parking spaces are allowed.

Total:

60	Standard Parking Stalls
74	Compact Parking Stalls
5	Handicap Parking Stalls
139	Total Parking Stalls

Adequate parking has been provided throughout the development with 1.78 parking spaces per dwelling unit. One loading space has been provided as well.

All parking areas will be served by 26-foot wide internal two-way accessways that run through the development.

Bicycle Parking: The Code requires 0.1 bicycle parking space per dwelling unit. Bike racks will be provided on the site and located in a convenient location for the residents. A total of 78 bicycle parking spaces have been provided on-site via bike racks throughout the site.

Recycling: There are two trash/recycle area provided within the development. One is located on the north and one is located on the south side of the development. The trash receptacles are accessible for all residents via the paved internal sidewalk system in the development. The trash/recycle areas will be screened and enclosed with a sight-obscuring fence or wall. Detail plans for the trash receptacles have been provided.

Stormwater: The Fairview Refinement Plan calls for the stormwater to be infiltrated onsite for storms up through the 20-year storm event. This requirement was based on the assumption that the soil had an infiltration rate 3.1 inches per hour. The onsite soil for both Grove 2 sites have an infiltration rate of 0.7 inches per hour, considerably less than stipulated in the Fairview Refinement plan. Both sites also have considerable topography constraints, with portions of the sites having slopes of 6-10%. The

Fairview Refinement Plan Section 23A states “Private on-site detention requirements will satisfy City of Salem standards only if infiltration of the 20-year storm cannot be achieved”. Based on the measured infiltration rates of the sites and the topography, infiltration of the 20-year storm cannot be achieved and this section of the Fairview Refinement Plan would apply. The stormwater design for The Grove 2 will meet the current City of Salem Design Standards of treating the new hard surface and providing detention for ½ the 2, the 10, the 25, and the 100-year storm events to the predeveloped rates.

A stormwater report dated February 22, 2021, has been provided as part of this application submittal.

Lot Coverage: The buildings (29,716sq.ft.) on the site (AU and MI zoned areas) cover 11% (259,974sq.ft.) of the lot. Therefore, lot coverage is under the 55% maximum allowed and in compliance with code.

AU Zone:

Area Size-146,345 square feet

Buildings-18,971 square feet

Total Lot Coverage-13%

MI Zone:

Area Size-113,629 square feet

Buildings-10,745 square feet

Total Lot Coverage-9%

Landscaping: There is a total of 146,424 square feet of landscaped area throughout the development. The net developable area is 259,974 square feet in size. Therefore, the development provides 56% landscaping throughout the site.

Traffic: An updated Traffic Impact Analysis (TIA) dated January 12, 2021, has been provided to staff.

Geo: A Geotechnical Report dated November 19, 2020, has been provide as part of this application.

Trees: There are 71 trees located within the AU, MI, and VC zoned portion of the development. Twenty-five (25) trees are designated for removal. There are eighteen (18) significant Oregon White Oaks on the site. Four (4) of the Oregon White Oaks are designated to be removed. Four (4) of the Oregon White Oaks proposed to be removed within the AU and MI zones are located within the right-of-way.

### **Fairview Mixed-Use/Mixed-Intensity Chapter 530**

Mixed-Intensity (MI): The Mixed Intensity area is described as comprising, “..primarily residential uses, along with a mix of small-scale neighborhood commercial, employment, and public services uses. Buildings will be a mix of one-story to three-story detached, attached, or stacked housing types sited on smaller individually owned lots with private yards and street and/or alley access, or larger lots under multiple or separate ownership with shared street and/or alley access. Townhouse development is appropriate at the higher density range.”

Density: The site being developed is 2.61 acres in size. Development in an MI zone shall meet a minimum of 7 dwelling units and shall not exceed 30 dwelling units. Therefore, the site shall be

developed with a minimum of 18 and allowed to have a maximum of 78 units. As shown on the site plan, there are 27 units proposed within the MI zone.

The development is in compliance with the minimum and maximum density requirements. See attached site plan.

Setbacks: All minimum setbacks to property lines, between buildings and distances to the entrances are met as shown on the tentative plan. Setbacks are shown on the tentative plan.

North:           Adjacent Chapel Drive:  
                  Building 24:    20' setback  
                  Building 25:    65' setback  
                  Parking:        20' setback

East:            Adjacent Reed Road  
                  Building 24:    20' setback  
                  Building 26:    20' setback  
                  Parking:        35' setback

West:           Building 25:    35' setback

South:           Adjacent Lindburg Road:  
                  Building 26:    20' setback

Maximum Height: Maximum building height allowed in the MI area is 45'. All proposed buildings are in compliance with the requirements of the Code.

\*Building 24 is 41.5 feet in height (measured to the highest point) and 35.4 feet in height (measured to the middle of the gable).

\*Building 25 is 39.7 feet in height (measured to the highest point) and 34.5 feet in height (measured to the middle of the gable).

\*Building 26 is 39.7 feet in height (measured to the highest point) and 34.5 feet in height (measured to the middle of the gable).

Parking: The development within the AU and MI zone is for 78-units. Code requires 1 vehicle parking space per every 1 dwelling unit, with an allowed maximum 1.75 times the minimum number of spaces required. The applicant is required to provide a minimum of 78 on-site vehicle parking spaces. As shown on the site plan, 139 on-site parking spaces are being provided where a maximum 137 on-site parking spaces are allowed.

Total:  
60     Standard Parking Stalls  
74     Compact Parking Stalls  
5      Handicap Parking Stalls  
139    Total Parking Stalls

Adequate parking has been provided throughout the development with 1.78 parking spaces per dwelling unit. One loading space has been provided as well.

All parking areas will be served by 26-foot wide internal two-way accessways that run through the development.

Bicycle Parking: The Code requires 0.1 bicycle parking space per dwelling unit. Bike racks will be provided on the site and located in a convenient location for the residents. A total of 78 bicycle parking spaces have been provided on-site via bike racks throughout the site.

Recycling: There are two trash/recycle area provided within the development. One is located on the north and one is located on the south side of the development. The trash receptacles are accessible for all residents via the paved internal sidewalk system in the development. The trash/recycle areas will be screened and enclosed with a sight-obscuring fence or wall. Detail plans for the trash receptacles have been provided.

Stormwater: The Fairview Refinement Plan calls for the stormwater to be infiltrated onsite for storms up through the 20-year storm event. This requirement was based on the assumption that the soil had an infiltration rate 3.1 inches per hour. The onsite soil for both Grove 2 sites have an infiltration rate of 0.7 inches per hour, considerably less than stipulated in the Fairview Refinement plan. Both sites also have considerable topography constraints, with portions of the sites having slopes of 6-10%. The Fairview Refinement Plan Section 23A states "Private on-site detention requirements will satisfy City of Salem standards only if infiltration of the 20-year storm cannot be achieved". Based on the measured infiltration rates of the sites and the topography, infiltration of the 20-year storm cannot be achieved and this section of the Fairview Refinement Plan would apply. The stormwater design for The Grove 2 will meet the current City of Salem Design Standards of treating the new hard surface and providing detention for ½ the 2, the 10, the 25, and the 100-year storm events to the predeveloped rates.

A stormwater report dated February 22, 2021, has been provided as part of this application submittal.

Lot Coverage: The buildings (29,716sq.ft.) on the site (AU and MI zoned areas) cover 11% (259,974sq.ft.) of the lot. Therefore, lot coverage is under the 55% maximum allowed and in compliance with code.

AU Zone:

Area Size-146,345 square feet

Buildings-18,971 square feet

Total Lot Coverage-13%

MI Zone:

Area Size-113,629 square feet

Buildings-10,745 square feet

Total Lot Coverage-9%

Landscaping: There is a total of 146,424 square feet of landscaped area throughout the development. The net developable area is 259,974 square feet in size. Therefore, the development provides 56% landscaping throughout the site.

Traffic: An updated Traffic Impact Analysis (TIA) dated January 12, 2021, has been submitted to staff for review and approval.

Geo: A Geotechnical Report dated November 19, 2020, has been provide as part of this application.



Trees: There are 71 trees located within the AU, MI, and VC zoned portion of the development. Twenty-five (25) trees are designated for removal. There are eighteen (18) significant Oregon White Oaks on the site. Four (4) of the Oregon White Oaks are designated to be removed. Four (4) of the Oregon White Oaks proposed to be removed within the AU and MI zones are located within the right-of-way.

### **Fairview Mixed-Use/Village Center Chapter 530**

Village Center (VC): The village center area comprises the most intense and pedestrian-oriented residential, commercial, employment, and public services uses. Residential uses will have densities of no less than 16 dwelling units per net acre. Nonresidential uses include a mix of large and small scale commercial establishments, which cumulatively will be limited to not more than approximately 80,000 square feet of pedestrian-oriented retail. Office uses are encouraged.

Density: The site being developed is 4.29 acres in size. Development in an VC zone shall meet a minimum of 13 dwelling units and shall not exceed 35 dwelling units. Therefore, the site shall be developed with a minimum of 55 and allowed to have a maximum of 150 units. As shown on the site plan, there are 105 units proposed within the VC zone.

The development is in compliance with the minimum and maximum density requirements. See attached site plan.

Setbacks: All minimum setbacks to property lines, between buildings and distances to the entrances are met as shown on the tentative plan. Setbacks are shown on the tentative plan.

Northwest:	Adjacent Strong Road
	Building 34: 10' setback
	Building 35: 13' setback
	Parking: 20' setback
Northeast:	Adjacent VC Loop
	Building 27: 100' setback
	Building 29: 30' setback
	Building 30: 30' setback
East:	Adjacent Lindburg Road
	Building 27: 20' setback
South:	Building 32: 10' setback
	Parking: 16' setback
West:	Building 32: 17' setback
	Building 33: 14' setback
	Building 34: 14' setback

Maximum Height: Maximum building height allowed in the VC area is 45'. All proposed buildings are in compliance with the requirements of the Code.

\*Building 27 is 40.5 feet in height (measured to the highest point) and 34.3 feet in height (measured to the middle of the gable).

\*Building 28 is 40 feet in height (measured to the highest point) and 34.5 feet in height (measured to the middle of the gable).

\*Building 29 is 40.1 feet in height (measured to the highest point) and 34.6 feet in height (measured to the middle of the gable).

\*Building 30 is 39.7 feet in height (measured to the highest point) and 34.5 feet in height (measured to the middle of the gable).

\*Building 31 is 40.5 feet in height (measured to the highest point) and 34.3 feet in height (measured to the middle of the gable).

\*Building 32 is 39.7 feet in height (measured to the highest point) and 34.5 feet in height (measured to the middle of the gable).

\*Building 33 is 39.7 feet in height (measured to the highest point) and 34.5 feet in height (measured to the middle of the gable).

\*Building 34 is 39.7 feet in height (measured to the highest point) and 34.5 feet in height (measured to the middle of the gable).

\*Building 35 is 40.3 feet in height (measured to the highest point) and 34.8 feet in height (measured to the middle of the gable).

Parking: The development within the VC zone is for a 105-unit apartment complex. The Refinement Plan Code requires 1 vehicle parking spaces per every 1 dwelling unit. The applicant is required to provide a minimum of 105 on-site vehicle parking spaces. As shown on the site plan, 173 on-site parking spaces are being provided. The maximum parking spaces allowed is 184.

173 parking spaces provided:

82	Standard Parking Stalls
81	Compact
4	Compact (off-site)
6	Handicap Parking Stalls

Adequate parking has been provided throughout this portion of the development with 1.65 parking spaces per dwelling unit. Loading spaces have been provided as well.

All parking areas will be served by 26-foot wide internal two-way accessways that run through the development.

Bicycle Parking: The Code requires 0.1 bicycle parking space per dwelling unit. Bike racks will be provided on the site and located in a convenient location for the residents. A total of 106 bicycle parking spaces have been provided on-site.

Recycling: There are two trash/recycle area provided within the development. One is located on the east and one is located on the west side of the development. The trash receptacles are accessible for all residents via the paved internal sidewalk system in the development. The trash/recycle areas will be

screened and enclosed with a sight-obscuring fence or wall. Detail plans for the trash receptacles have been provided.

Stormwater: The Fairview Refinement Plan calls for the stormwater to be infiltrated onsite for storms up through the 20-year storm event. This requirement was based on the assumption that the soil had an infiltration rate 3.1 inches per hour. The onsite soil for both Grove 2 sites have an infiltration rate of 0.7 inches per hour, considerably less than stipulated in the Fairview Refinement plan. Both sites also have considerable topography constraints, with portions of the sites having slopes of 6-10%. The Fairview Refinement Plan Section 23A states “Private on-site detention requirements will satisfy City of Salem standards only if infiltration of the 20-year storm cannot be achieved”. Based on the measured infiltration rates of the sites and the topography, infiltration of the 20-year storm cannot be achieved and this section of the Fairview Refinement Plan would apply. The stormwater design for The Grove 2 will meet the current City of Salem Design Standards of treating the new hard surface and providing detention for ½ the 2, the 10, the 25, and the 100-year storm events to the predeveloped rates.

A stormwater report dated February 22, 2021, has been provided as part of this application submittal.

Lot Coverage: Lot Coverage: The buildings on the site cover 22% (42,013sq.ft.) of the lot. Therefore, lot coverage is under the 55% maximum allowed and in compliance with code.

Landscaping: The net developable area is 187,066 (4.29 acres) square feet in size with 64,337 square feet of landscaping throughout. Therefore, the development provides 34% landscaping throughout the site.

Traffic: An updated Traffic Impact Analysis (TIA) dated January 12, 2021 has been provided to staff.

Geo: A Geotechnical Report dated November 19, 2020, has been provide as part of this application.

Trees: There are 71 trees located within the AU, MI, and VC zoned portion of the development. Twenty-five (25) trees are designated for removal. There are eighteen (18) significant Oregon White Oaks on the site. Four (4) of the Oregon White Oaks are designated to be removed. Four (4) of the Oregon White Oaks proposed to be removed within the AU and MI zones are located within the right-of-way.

**(B) *The transportation system provides for the safe, orderly, and efficient circulation of traffic into and out of the proposed development, and negative impacts to the transportation system are mitigated adequately;***

**Applicant Findings:** An updated Traffic Analysis dated January 12, 2021, has been provided to staff.

The subject property has street frontage on Strong Road, Reed Road, Chapel Drive, Lindburg Road, and VC Loop (a private street).

Strong Road is designated as a ‘collector’ street within the City of Salem Transportation Plan. A Driveway Approach Permit has been provided as well.

Reed Road is designated as a ‘minor arterial’ within the City of Salem Transportation Plan. No access to

Reed Road will be taken from the development.

Chapel Drive is designated as a 'local' street within the City of Salem Transportation Plan. A Driveway Approach Permit has been provided as well.

Lindburg Road is designated as a 'local' within the City of Salem Transportation Plan. No access to Reed Road will be taken from the development.

As shown on the site plan, safe and efficient access and circulation has been provided into and throughout the development.

**(C) *Parking areas and driveways are designed to facilitate safe and efficient movement of vehicles, bicycles, and pedestrians; and***

**Applicant Findings:**

Parking: The development within the AU and MI zone is for 78-units. Code requires 1 vehicle parking space per every 1 dwelling unit, with an allowed maximum 1.75 times the minimum number of spaces required. The applicant is required to provide a minimum of 78 on-site vehicle parking spaces. As shown on the site plan, 139 on-site parking spaces are being provided where a maximum 137 on-site parking spaces are allowed.

Total:

60 Standard Parking Stalls  
74 Compact Parking Stalls  
5 Handicap Parking Stalls  
139 Total Parking Stalls

Adequate parking has been provided throughout the development with 1.78 parking spaces per dwelling unit. One loading space has been provided as well.

Parking: The development within the VC zone is for a 105-unit apartment complex. The Refinement Plan Code requires 1 vehicle parking spaces per every 1 dwelling unit. The applicant is required to provide a minimum of 105 on-site vehicle parking spaces. As shown on the site plan, 173 on-site parking spaces are being provided. The maximum parking spaces allowed is 184.

173 parking spaces provided:

82 Standard Parking Stalls  
81 Compact  
4 Compact (off-site)  
6 Handicap Parking Stalls

Adequate parking has been provided throughout this portion of the development with 1.65 parking spaces per dwelling unit. Loading spaces have been provided as well.

Bicycle Parking: The Code requires 0.1 bicycle parking space per dwelling unit. Bike racks will be provided on the site and located in a convenient location for the residents. A total of 78 bicycle parking spaces have been provided on-site via bike racks throughout the site. All parking areas will be served by 26-foot wide internal two-way accessways that run through the development.

Bicycle Parking: The Code requires 0.1 bicycle parking space per dwelling unit. Bike racks will be provided on the site and located in a convenient location for the residents. A total of 106 bicycle parking spaces have been provided on-site via bike racks throughout the site.

All parking areas will be served by 26-foot wide two-way accessways that run through the development.

Bicycle parking is also required on site. The Code requires 1 bicycle parking space per dwelling unit. One hundred-eighty (183) bicycle parking spaces are required for this development. Bike racks have been provided on the site and located in a convenient location for the residents.

Therefore, this standard has been met.

**(D)        *The proposed development will be adequately served with City water, sewer, stormwater facilities, and other utilities appropriate to the nature of the development.***

***Applicant Findings:*** Utility plans have been provided that show how the site will be served with City water, sewer, storm water facilities, and other utilities appropriate to the development.

# The Grove Phase 2

## Adjustment Class-2 Application

Revised-May 28, 2021

### Proposal:

The subject properties are about 10.26 acres in size, zoned Fairview Mixed-Use (FMU) and located in the Fairview Refinement Plan II (083W11A/Tax Lot 1002 and 083W02/Tax Lot 100).

The subject properties are part of the Fairview Refinement Plan II (FRP09-1 and FRPA16-01) and is identified as 083W11A/Tax Lot 1002 and 083W02/Tax Lot 100. The Fairview Refinement Plan II was approved in 2009 with development standards and guidelines. The Fairview Refinement Plan II was amended in April 2016.

The applicant is requesting an adjustment greater than 20% adjustment to Sec. 702.020(e)(5) Façade and building design:

**“(5) To orient buildings to the street, any ground-level unit, cluster of units, interior lobbies, or portions thereof, located within 25 feet of the property line abutting a street shall have a building entrance facing that street, with direct pedestrian access to adjacent sidewalks.”**

### Adjustment Criteria-SRC 250.005(d)(2) Criteria:

- (A) The purpose underlying the specific development standard proposed for adjustment is:**
- (i) Clearly inapplicable to the proposed development; or**
  - (ii) Equally or better met by the proposed development.**
- (B) If located within a residential zone, the proposed development will not detract from the livability or appearance of the residential area.**
- (C) If more than one adjustment has been requested, the cumulative effect of all the adjustments result in a project which is still consistent with the overall purpose of the zone.**

### Applicant Findings:

- (A) The purpose of this requirement is to provide a pedestrian friendly development with buildings entrances facing the street.**

All buildings face the interior of the lot. The rear side of Buildings 20, 34, and 35 face Strong Road. The rear side of Buildings 23 and 24 faces Reed Road. The rear side of Building 22 faces Chapel Drive. The side of Buildings 23 and 24 face Chapel Drive. The side of Building 26 faces Reed Road and Lindburg Road. The rear side of Buildings 29 and 30 face VC Loop. The street side of these buildings (rear/side) will be designed to be visually appealing, by providing similar design as is being provided for the front building facade for all buildings. In order to be consistent with the front facade of the building; windows, offsets, and architectural features will be incorporated in the portions

of the building facing the right-of-way.

All buildings within the development have direct pedestrian access onto sidewalks. The internal pedestrian circulation system consists of hard 6-foot wide surfaced sidewalks that provide easily identifiable and safe connections between the residential units, parking, recreation areas, manager's apartment, and the trash disposal area. The pedestrian system connects the buildings to the public sidewalk system within Strong Road, Reed Road, Lindburg Road, VC Loop, and Chapel Drive via the proposed internal sidewalk system.

Proposed and existing sidewalks will further enhance the pedestrian connections and circulation to and from the site. The proposed sidewalks to and from the site will provide pedestrian circulation to the entrances of the buildings.

Proposed pedestrian sidewalk connections are illustrated on the tentative site plan. The proposed development provides safe and convenient bicycle and pedestrian access from within the development to adjacent residential areas.

Due to the slope of the site, providing additional pedestrian paths is not feasible due to ADA requirements. Therefore, due to the slope, and not being able to meet ADA requirements for paths to the street, this standard is better met by providing the paths as outlined on the site plan. Please see revised site plans and grading plans.

- (B) The apartment development will provide landscaped areas throughout the site along with pedestrian paths/sidewalks throughout, along with visual appealing buildings. All of which will create a pedestrian friendly development. Buildings not facing the street will have no effect on the proposed use or surrounding uses.
- (C) There are several adjustments being requested for this proposal. The adjustment do not have any effect on the project.

# The Grove II-Apartments

## Adjustment Class-2 Application

May 28, 2021

### Proposal:

The subject properties are about 10.26 acres in size, zoned Fairview Mixed-Use (FMU) and located in the Fairview Refinement Plan II (083W11A/Tax Lot 1002 and 083W02/Tax Lot 100).

The subject properties are part of the Fairview Refinement Plan II (FRP09-1 and FRPA16-01) and is identified as 083W11A/Tax Lot 1002 and 083W02/Tax Lot 100. The Fairview Refinement Plan II was approved in 2009 with development standards and guidelines. The Fairview Refinement Plan II was amended in April 2016.

The applicant is requesting an adjustment greater than 20% adjustment to Sec. 702.020(e)(4) Façade and building design:

**“(4) On sites with 75 feet or more of buildable width, a minimum of 40 percent of the buildable width shall be occupied by building placed at the setback line to enhance visual interest and activity along the street. Accessory structures shall not apply towards meeting the required percentage.”**

### Adjustment Criteria-SRC 250.005(d)(2) Criteria:

- (A) **The purpose underlying the specific development standard proposed for adjustment is:**
- (i) **Clearly inapplicable to the proposed development; or**
  - (ii) **Equally or better met by the proposed development.**
- (B) **If located within a residential zone, the proposed development will not detract from the livability or appearance of the residential area.**
- (C) **If more than one adjustment has been requested, the cumulative effect of all the adjustments result in a project which is still consistent with the overall purpose of the zone.**

### Applicant Findings:

- A) Findings: The purpose of this requirement is to provide a pedestrian friendly development with building located close to the sidewalks instead of parking areas.

Within the VC zone the Fairview Plan requires a minimum of 70% of the buildable width be occupied by buildings placed on the setback line. The development within the VC zone does not meet the minimum 70% of buildings along the buildable width.

Within the AU and MI zone the Fairview Plan requires a minimum of 50% of the buildable width be occupied by buildings placed on the setback line. The development within the AU and MI zone does not meet the minimum 50% of buildings along the buildable width.



Strong Road (AU and MI zone):

The subject property has 215 feet of buildable width (this excludes required side setbacks and driveway) along Strong Road. The Fairview Plan Code requires a minimum of 50% of the buildable width be occupied by buildings placed on the setback line. As shown on the site plan, the building totals 108 feet of the buildable width along the street frontage. However, the building is not located on the minimum 10-foot setback line. Therefore, occupying 0% of the buildable width of street frontage along Strong Road.

Strong Road (VC zone):

The subject property has 275 feet of buildable width (this excludes required side setbacks and driveway) along Strong Road. The Fairview Plan Code requires a minimum of 70% of the buildable width be occupied by buildings placed on the setback line. As shown on the site plan, the buildings total 226 feet of the buildable width along the street frontage. However, the building is not located on the minimum 10-foot setback line. Therefore, occupying 0% of the buildable width of street frontage along Strong Road.

Chapel Drive (AU and MI zone):

South side-The subject property has 248 feet of buildable width (this excludes required side setbacks and driveway) along the west side of Chapel Drive. The Fairview Plan Code requires a minimum of 50% of the buildable width be occupied by buildings placed on the setback line. As shown on the site plan, the building totals 51 feet of the buildable width along the street frontage. However, the building is not located on the minimum 10-foot setback line. Therefore, occupying 0% of the buildable width of street frontage along the west side of Chapel Drive.

Lindburg Road (AU and MI zone):

The subject property has 268 feet of buildable width (this excludes required side setbacks and driveway) along Lindburg Road. The Fairview Plan Code requires a minimum of 50% of the buildable width be occupied by buildings placed on the setback line. As shown on the site plan, the building totals 70 feet of the buildable width along the street frontage. However, the building is not located on the minimum 10-foot setback line. Therefore, occupying 0% of the buildable width of street frontage along Lindburg Road.

Lindburg Road (VC zone):

The subject property has 182 feet of buildable width (this excludes required side setbacks and driveway) along Lindburg Road. The Fairview Plan Code requires a minimum of 70% of the buildable width be occupied by buildings placed on the setback line. As shown on the site plan, the building totals 95 feet of the buildable width along the street frontage. However, the building is not located on the minimum 10-foot setback line. Therefore, occupying 0% of the buildable width of street frontage along Lindburg Road.

Reed Road (AU and MI zone):

The subject property has 908 feet of buildable width (this excludes required side setbacks and driveway) along Reed Road. The Fairview Plan Code requires a minimum of 50% of the buildable width be occupied by buildings placed on the setback line. As shown on the site plan, there are no buildings located on the required minimum setback line.

VC Loop (Private right-of-way) (VC zone):

The subject property has 447 feet of buildable width (this excludes required side setbacks and driveway) along VC Loop. The Fairview Plan Code requires a minimum of 70% of the buildable width be occupied by buildings placed on the setback line. As shown on the site plan, there are no buildings located on the required setback line.

The purpose of this requirement is to provide a pedestrian friendly development with building located close to the sidewalks instead of parking areas. The apartment development will provide landscaped areas throughout the site along with pedestrian paths/sidewalks throughout and visual appealing buildings. All of which will create a pedestrian friendly development. Therefore, the reduction of buildings along the setback line will have no effect on the proposed use or surrounding uses.

- (B) The apartment development will provide additional landscaped areas throughout the site along with pedestrian paths/sidewalks through, along with visual appealing buildings. All of which will create a pedestrian friendly development. The reduction of buildings along the setback line will have no effect on the proposed use or surrounding uses.
- (C) There are several adjustments being requested for this proposal. The adjustments do not have any effect on the project.

# The Grove II-Apartments

## Adjustment Class-2 Application

May 28, 2021

### Proposal:

The subject properties are about 10.26 acres in size, zoned Fairview Mixed-Use (FMU) and located in the Fairview Refinement Plan II (083W11A/Tax Lot 1002 and 083W02/Tax Lot 100).

The subject properties are part of the Fairview Refinement Plan II (FRP09-1 and FRPA16-01) and is identified as 083W11A/Tax Lot 1002 and 083W02/Tax Lot 100. The Fairview Refinement Plan II was approved in 2009 with development standards and guidelines. The Fairview Refinement Plan II was amended in April 2016.

The applicant is requesting an adjustment greater than 20% adjustment to Sec. 702.020(b)(7) Landscaping Standards:

***“To provide protection from winter wind and summer sun and to ensure trees are distributed throughout a site and along parking areas, a minimum of one canopy tree shall be planted along every 50 feet of the perimeter of parking areas. Trunks of the trees shall be located within ten feet of the edge of the parking area (see Figure 702-3).***

***(A) A minimum of one canopy tree shall be planted within each planter bay.***

***(B) A landscaped planter bay a minimum of nine feet in width shall be provided at a minimum spacing of one for every 12 spaces. (see Figure 702-3).”***

***Adjustment Criteria-SRC 250.005(d)(2) Criteria:***

***(A) The purpose underlying the specific development standard proposed for adjustment is:***

***(i) Clearly inapplicable to the proposed development; or***

***(ii) Equally or better met by the proposed development.***

***(B) If located within a residential zone, the proposed development will not detract from the livability or appearance of the residential area.***

***(C) If more than one adjustment has been requested, the cumulative effect of all the adjustments result in a project which is still consistent with the overall purpose of the zone.***

Applicant Findings:

- A) Findings: The purpose of this requirement is to provide shade and break up all the paved areas. This helps to provide a pedestrian friendly development.

The subject property is located with in the Fairview Refinement Plan. The canopy tree planting requirement is outlined in SRC 702 (per SRC 702.020(b)(7)) and in the Fairview Refinement Plan under the alternative multiple family design review standards section of the standards (Section 7 – Table 2 on page 24).

SRC 702.020(b)(7) requires a minimum 50-foot spacing requirement around the perimeter of parking lots. However, the Refinement Plan, requires a 30-foot spacing around the perimeter of parking lots. Therefore, the applicant is requesting an adjustment to the 30-foot spacing requirement for the planting of canopy trees around the perimeter of the parking lots.

As shown on the landscape plan, there are large canopy trees provided through out the parking bays and the development. Some canopy trees are provided every 30 feet. However, due to the size and shape of the site, a canopy tree can not feasible be provided every 30 feet. This would result in the loss of landscaped areas, pedestrian paths, and parking.

There are 252 trees being planted throughout the perimeter of the parking lots and the site. Therefore, the purpose of this requirement is to provide shaded areas for a pedestrian friendly development has been met. The apartment development will provide landscaped areas throughout the site, which include the planting of trees along with pedestrian paths/sidewalks and buildings. All of which will create a pedestrian friendly development with shaded areas for the residents.

- (B) The apartment development will provide landscaped areas throughout the site, which include the planting of trees along with pedestrian paths/sidewalks and buildings. All of which will create a pedestrian friendly development with shaded areas for the residents. The reduction of canopy trees within the perimeter of the parking areas will have no effect on the proposed use or surrounding uses.
- (C) There are several adjustments being requested for this proposal. The adjustments do not have any effect on the project.

# The Grove Phase 2

## Adjustment Class-2 Application

Revised-May 28, 2021

### Proposal:

The subject properties are about 10.26 acres in size, zoned Fairview Mixed-Use (FMU) and located in the Fairview Refinement Plan II (083W11A/Tax Lot 1002 and 083W02/Tax Lot 100).

The subject properties are part of the Fairview Refinement Plan II (FRP09-1 and FRPA16-01) and is identified as 083W11A/Tax Lot 1002 and 083W02/Tax Lot 100. The Fairview Refinement Plan II was approved in 2009 with development standards and guidelines. The Fairview Refinement Plan II was amended in April 2016.

The applicant is requesting an adjustment greater than 20% adjustment to the Fairview Alternative Multi-Family Design Standards-Building Mass and Façade-Side Yard Façade Setback Plane:

***Requires those portions of a building facade above 28 in feet in height to be setback from the side yard an additional distance based on a 45-degree plane.***

### Adjustment Criteria-SRC 250.005(d)(2) Criteria:

- (A) ***The purpose underlying the specific development standard proposed for adjustment is:***
- (i) Clearly inapplicable to the proposed development; or***
  - (ii) Equally or better met by the proposed development.***
- (B) ***If located within a residential zone, the proposed development will not detract from the livability or appearance of the residential area.***
- (C) ***If more than one adjustment has been requested, the cumulative effect of all the adjustments result in a project which is still consistent with the overall purpose of the zone.***

### Applicant Findings:

- (A) The purpose of this requirement is to provide additional side setbacks along the boundary for the development. Buildings 20 and 21 do not meet this requirement.
- Building 20 is 39.7 feet in height (measured to the highest point) and 34.5 feet in height (measured to the middle of the gable).
- The side of Building 20 is located along the boundary of the development. The east side of Building 20 is located 13.4' from the boundary property line.
- There will be landscaping and adequate setbacks adjacent all property lines, that will help before a privacy buffer for adjacent uses.

- (B) The apartment development will provide landscaped areas throughout the site, along with visual appealing buildings. All of which will create privacy for residents and adjacent uses.
- (C) There are several adjustments being requested for this proposal. The adjustment do not have any effect on the project.

**The Grove II**  
Adjustment Class-2 Application  
May 28, 2021

**PROPOSAL:**

The subject properties are about 10.26 acres in size, zoned Fairview Mixed-Use (FMU) and located in the Fairview Refinement Plan II (083W11A/Tax Lot 1002 and 083W02/Tax Lot 100).

The applicant is proposing a development consisting of 183 apartment units as shown on the site plans.

083W11A/Tax Lot 1002 (5.97 acres):  
AC (Adaptive Zone)-51 Units  
MI (Mixed-Intensity Zone)-27 Units

**Request:** The applicant is requesting a Class-2 Adjustment to allow 139 parking spaces within the AU and MI zones where 137 maximum is allowed.

**Adjustment Criteria-SRC 250.005(d)(2) Criteria**

***(A) The purpose underlying the specific development standard proposed for adjustment is:***

- (i) Clearly inapplicable to the proposed development; or***
- (ii) Equally or better met by the proposed development.***

***(B) If located within a residential zone, the proposed development will not detract from the livability or appearance of the residential area.***

***(C) If more than one adjustment has been requested, the cumulative effect of all the adjustments result in a project which is still consistent with the overall purpose of the zone.***

Applicant's Reasons:

(A) The development within the AU and MI zone is for 78-units. Code requires 1 vehicle parking space per every 1 dwelling unit, with an allowed maximum 1.75 times the minimum number of spaces required. The applicant is required to provide a minimum of 78 on-site vehicle parking spaces. As shown on the site plan, 139 on-site parking spaces are being provided where a maximum 137 on-site parking spaces are allowed.

Total:  
60 Standard Parking Stalls  
74 Compact Parking Stalls  
5 Handicap Parking Stalls  
139 Total Parking Stalls

The purpose of keeping within the minimum and maximum parking requirements is to avoid large parking lots within developments. In order to minimize large expanses of

continuous pavement, 9-foot-wide planter islands have been provided a maximum of every 12 parking spaces.

All parking areas are within the requirements of the code and are separated by planter bays that are a minimum of 9 feet in width. The layout of the parking areas has been taken into consideration and provides for safe and efficient circulation throughout the development. All parking areas are landscaped as required and separated by landscaped bays that meet the minimum width as shown on the site plan.

The adjustment will allow the applicant to provide 2 more parking spaces than allowed. The increase in parking allows the applicant to provide additional parking on-site.

The increase in parking helps serve the property in a better way by providing parking for the residents which is better for this development.

- (B) The subject properties are about 10.26 acres in size, zoned Fairview Mixed-Use (FMU) and located in the Fairview Refinement Plan II (083W11A/Tax Lot 1002 and 083W02/Tax Lot 100). The subject property is within a residential development and will be compatible with the existing uses.

The applicant is proposing a development consisting of 183 apartment units as shown on the site plans. The applicant is going through the required land use process to ensure that Code requirements are met, and that the proposal has little to no impact on the surrounding neighborhood.

Any conditions placed on the application will require Code compliance, which will help ensure minimal to no impacts on the neighborhood.

- (C) The proposed adjustment will not affect surrounding existing or proposed development. The adjustments do not have any effect on the project.



**Class 2-Driveway Approach Permit**  
**Revised-May 28, 2021**

***SRC 804.025 (d) Criteria. A Class 2 Driveway Approach Permit shall be granted if:***

***(1) The proposed driveway approach meets the standards of this Chapter and the Public Works Design Standards;***

Applicant Response: The subject properties are about 10.26 acres in size, zoned Fairview Mixed-Use (FMU) and located in the Fairview Refinement Plan II (083W11A/Tax Lot 1002 and 083W02/Tax Lot 100).

The applicant is proposing a development consisting of 183 apartment units as shown on the site plans.

083W11A/Tax Lot 1002 (5.97 acres):  
AC (Adaptive Zone)-51 Units  
MI (Mixed-Intensity Zone)-27 Units

083W02/Tax Lot 100 (4.29 acres):  
VC (Village Center Zone)-105 Units

The proposed development will have access onto Strong Road, Chapel Drive, VC Loop, Heritage Street, Strong Road, and Lindburg Road. VC Loop and Heritage Street are both private streets. Reed Road is designated as a 'arterial' street on the Salem Transportation System Plan. Strong Road is designated as a 'collector' street on the Salem Transportation System Plan. Both Chapel Drive, Heritage Street, and Lindburg Road are designated as 'local' streets on the Salem Transportation System Plan. All proposed driveways will meet Public Works design standards. As shown on the site plan the driveways are required for access to the site and are in compliance with design standards.

***(2) No site conditions prevent placing the driveway approach in the required location;***

Applicant Response: The location of the driveways was taken into consideration prior to laying the site out. The location of the proposed driveways take into consideration the location of internal streets adjacent to the site, access onto the local streets, and existing development. Therefore, all factors were taken into consideration and there are no conditions on the site that prevent the driveway approach.

***(3) The number of driveway approaches onto an arterial are minimized;***

Applicant Response: There are no direct driveways proposed onto an arterial. Therefore, this criterion is not applicable.

***(4) The proposed driveway approach, where possible:***

***(A) Is shared with an adjacent property; or***

***(B) Takes access from the lowest classification of street abutting the property;***

Applicant Response to (4)(B): The proposed development is sharing access within the existing development within the Refinement Plan (The Grove Phase 1). All direct access is onto local streets, access onto an arterial or collector is via a shared accessway or private street. Therefore, this criterion has been met.

***(5) The proposed driveway approach meets vision clearance standards;***

Applicant Response: Through the pre-app process, the applicant has been working with Public Works to ensure that the driveway approach is in the required location and meets vision clearance standards. As shown on the site plan, this criterion has been met.

***(6) The proposed driveway approach does not create traffic hazards and provides for safe turning movements and access;***

Applicant Response: The driveway approach does not create traffic hazards. As shown on the site plan, this criterion has been met.

***(7) The proposed driveway approach does not result in significant adverse impacts to the vicinity;***

Applicant Response: Public Works has had the opportunity to review the site plan for any adverse impacts. No adverse impacts to the vicinity have been identified. As shown on the site plan, the location of the driveway will not have any impacts on the subject property or adjacent properties. This criterion has been met.

***(8) The proposed driveway approach minimizes impact to the functionality of adjacent streets and intersections; and***

Applicant Response: The applicant has been working with Public Works to ensure that the driveway approach is in the required locations to minimize impacts to adjacent streets and intersections. As shown on the site plan, this criterion has been met.


***(9) The proposed driveway approach balances the adverse impacts to residentially zoned property and the functionality of adjacent streets.***

Applicant Response: The applicant has been working with Public Works to ensure that the driveway approach is in the required location to help balance the adverse impacts to residentially zoned property. The location of the proposed driveways takes into consideration the location of the streets adjacent to the site and existing development. As shown on the site plan, this criterion has been met.



# MEMO

**TO:** Bryce Bishop, Planner III  
Community Development Department

**FROM:** Glenn J. Davis, PE, CFM, Chief Development Engineer   
Public Works Department

**DATE:** June 8, 2021

**SUBJECT: PUBLIC WORKS RECOMMENDATIONS  
FRPA-DR-SPR-ADJ-DAP21-02 (21-101926-ZO)  
2110 STRONG ROAD SE & 4000-4100 BLOCKS OF REED ROAD SE  
183-UNIT MULTIPLE FAMILY DEVELOPMENT**

## PROPOSAL

A consolidated application for a proposed 183-unit multiple family development on two portions of the former Fairview Training Center site totaling approximately 10.74 acres in size. The application includes a Class 1 Design Review and Class 3 Site Plan review for the proposed multiple family development, a Major Amendment to the Fairview Refinement Plan II refinement plan, a Class 1 Adjustment, a Class 2 Adjustment, and two Class 2 Driveway Approach Permits. The subject property is located at 2110 Strong Road SE and in the 4000 to 4100 Blocks of Reed Road SE (Marion County Assessor's Tax Map and Lots: 083W02 00100 and 083W11A 01002).

## RECOMMENDED CONDITIONS OF APPROVAL

1. Design and construct all City infrastructure, including streets, according to the refinement plan and PWDS.
2. Convey land for dedication to equal a half-width right-of-way of 36 feet along the entire frontage of the subject property measured from the newly established offset centerline of Reed Road SE, and as shown on public construction plans under permit number 21-101403-PC.
3. Construct a full-street improvement along the frontage of Reed Road SE to Minor Arterial street standards from Strong Road SE to approximately 500 feet south of Lindberg Road SE, and as shown on public construction plans under permit number 21-101403-PC.
4. Complete incomplete portions of the boundary street improvements along the property frontages of Strong Road SE and Lindberg Road SE.

Code authority references are abbreviated in this document as follows: *Salem Revised Code* (SRC); *Public Works Design Standards* (PWDS); *Salem Transportation System Plan* (Salem TSP); and *Stormwater Management Plan* (SMP).

5. Construct Village Center Loop SE (private) from Lindburg Road SE to Strong Road SE. The private/public street intersections shall be constructed pursuant to PWDS.
6. Construct an extension of Mountain Street SE (private) from its existing terminus at the northern boundary of the eastern property to Chapel Drive SE (private).
7. Construct incomplete portions of the boundary street improvements along the property frontages of Chapel Drive SE (private).
8. Dedicate public access easements for the proposed private streets within the development.
9. Construct an 18-inch S-1 water main within Reed Road SE from Strong Road SE to approximately 500 feet south of Lindberg Road SE.
10. Design and construct a storm drainage system at the time of development in compliance with SRC Chapter 71 and PWDS.
11. Obtain all state and federal permits as indicated by the Oregon Department of State Lands (DSL).

## **FACTS**

### **Streets**

1. Strong Road SE
  - a. Standard—This street is designated as a Collector street in the Salem TSP and Fairview Refinement Plan II. The standard for this street classification is a 28- to 36-foot-wide improvement within a 60-foot-wide right-of-way.
  - b. Existing Conditions—This street has an approximate 28- to 36-foot improvement within a 60-foot-wide right-of-way abutting the subject property.
2. Lindberg Road SE
  - a. Standard—This street is designated as a Collector street in the Salem TSP and Fairview Refinement Plan II. The standard for this street classification is a 28- to 36-foot-wide improvement within a 60-foot-wide right-of-way.
  - b. Existing Conditions—This street has an approximate 36-foot improvement within a 60-foot-wide right-of-way abutting the subject property.

3. Reed Road SE

- a. Standard—This street is designated as a Minor Arterial street in the Salem TSP. The standard for this street classification is a 46-foot-wide improvement within a 72-foot-wide right-of-way.
- b. Existing Conditions—This street has an approximate 20-foot turnpike improvement within a 40-foot-wide right-of-way abutting the subject property.

4. Chapel Lane SE (private)

- a. Standard—This street is designated as a Private street in the Fairview Refinement Plan II. The standard for this street classification is a minimum 20-foot-wide improvement within a variable-width easement.
- b. Existing Conditions—This street has an approximate 20-foot turnpike improvement within a 24-foot-wide easement on the subject property.

**Storm Drainage**

1. Existing Conditions

- a. A 10-inch storm main is located in Strong Road SE at Reed Road SE.
- b. An 18-inch piped creek main is located at Chapel Lane SE (private) and Reed Road SE.
- c. A 24-inch piped creek main is located at Lindberg Road SE and Reed Road SE.
- d. An 18-inch storm main is located in Strong Road SE along the western development frontage.
- e. A 12-inch storm main is located in Lindberg Road SE along the western development frontage.

**Water**

1. Existing Conditions

- a. The subject property is located in the S-1 water service level.
- b. A 16-inch S-1 water main is located in Strong Road SE along the eastern development frontage. Mains of this size generally convey flows of 1,900 to 4,400 gallons per minute.

- c. A 10-inch S-1 water main is located in Strong Road SE and Lindberg Road SE along the western development frontage. Mains of this size generally convey flows of 1,500 to 3,400 gallons per minute.
- d. 8-inch S-1 water mains are located in Lindberg Road SE along the eastern development frontage, and in Chapel Lane SE (private). Mains of this size generally convey flows of 500 to 1,100 gallons per minute.

## **Sanitary Sewer**

### **1. Existing Conditions**

- a. An 8-inch sewer main is located in Strong Road SE.
- b. A 10-inch sewer main is located in Lindberg Road SE.
- c. A 15-inch sewer main is located in Reed Road SE.
- d. An 8-inch sewer main is located in Chapel Lane SE (private).

## **CRITERIA AND FINDINGS**

Analysis of the development based on relevant criteria in SRC 220.005(f)(3) is as follows:

**Criteria: SRC 220.005(f)(3)(A) The application meets all applicable standards of the UDC (*Unified Development Code*)**

**Finding**—With completion of the conditions above, the subject property meets all applicable standards of the following chapters of the UDC: 601 – Floodplain; 802 – Public Improvements; 803 – Streets and Right-of-Way Improvements; 804 – Driveway Approaches; 805 – Vision Clearance; 809 – Wetlands; and 810 - Landslides.

Public Works staff has reviewed the Flood Insurance Study and Flood Insurance Rate Maps and has determined that no floodplain or floodway areas exist on the subject property.

The Salem-Keizer Local Wetland Inventory shows that there are wetland channels and/or hydric soils mapped on the property. The applicant should contact the DSL to verify if any permits are required for development or construction in the vicinity of the mapped wetland area(s). Wetland notice was sent to the DSL pursuant to SRC 809.025. Pursuant to PWDS, the application shall obtain all state and federal permits as indicated by DSL.

According to the City's adopted landslide hazard susceptibility maps and SRC Chapter 810 (Landslide Hazards), there are mapped 2- and 3-point landslide hazard areas on the subject property. The proposed activity of a multi-family development adds 2 activity points to the proposal, which results in a total of 5 points. Therefore, the proposed development is classified as a moderate landslide risk and requires a geological assessment. A Geotechnical Investigation, prepared by Multi-Tech Engineering Services, Inc. and dated November 19, 2020, was submitted to the City of Salem. This assessment demonstrates the subject property can be developed with the recommendations contained in the report.

**Criteria: SRC 220.005(f)(3)(B) The transportation system provides for the safe, orderly, and efficient circulation of traffic into and out of the proposed development, and negative impacts to the transportation system are mitigated adequately**

**Finding**—Strong Road SE meets the right-of-way width and pavement width standards pursuant to the Salem TSP and Fairview Refinement Plan II; however, sections along the property frontage are lacking sidewalks and street trees. The applicant shall construct sidewalk and provide street trees to the maximum extent feasible along the Strong Road SE frontage pursuant to SRC Chapter 803.

Lindberg Road SE meets the right-of-way width and pavement width standards pursuant to the Salem TSP and Fairview Refinement Plan II; however, sections along the property frontage are lacking adequate sidewalk and street trees. The applicant shall construct sidewalk and provide street trees to the maximum extent feasible along the Lindberg Road SE frontage pursuant to SRC Chapter 803.

The existing condition of Reed Road SE does not meet current standards for its classification of street pursuant to the Salem TSP. The applicant shall convey for dedication a half-width right-of-way of 36 feet to Reed Road SE street standards as specified in the PWDS, and as shown on public construction plans under permit number 21-101403-PC. The applicant shall also construct a full-street improvement to Minor Arterial street standards from Strong Road SE to approximately 500 feet south of Lindberg Road SE as specified in the PWDS and as shown on public construction plans under permit number 21-101403-PC. Sidewalks, street trees, and streetlights are not required on the opposite side of the street.

The location of the sidewalk along the frontage of Reed Road SE may meander between property-line and curb-line sidewalk to accommodate the preservation of existing trees pursuant to SRC 803.035(l)(2)(B). Removal of trees located within the right-of-way requires a street tree removal permit pursuant to SRC Chapter 86.

Internal private streets shall be constructed in compliance with the refinement plan and PWDS. The applicant shall dedicate public access easements for the proposed private streets within the development. The private/public street intersections shall be constructed pursuant to PWDS, and private streets shall be differentiated in appearance

from public streets by installation of a commercial driveway. The applicant shall construct Village Center Loop SE (private) from Lindburg Road SE to Strong Road SE. The private/public street intersections shall be constructed pursuant to PWDS. The applicant shall construct an extension of Mountain Street SE (private) from its existing terminus at the northern boundary of the eastern property to Chapel Drive SE (private). The applicant shall construct incomplete portions of the boundary street improvements along the property frontages of Chapel Drive SE (private). The applicant shall dedicate public access easements for the proposed private streets within the development.

**Criteria: SRC 220.005(f)(3)(C) Parking areas and driveways are designed to facilitate safe and efficient movement of vehicles, bicycles, and pedestrians**

**Finding**—The driveway accesses onto Strong Road SE and Lindberg Road SE provides for safe turning movements into and out of the property.

**Criteria: SRC 220.005(f)(3)(D) The proposed development will be adequately served with City water, sewer, storm drainage, and other utilities appropriate to the nature of the development**

**Finding**—The Public Works Department has reviewed the applicant's preliminary plan for this site. The water, sewer, and storm infrastructure are available within surrounding streets/areas and are adequate to serve the proposed development. In order to serve upstream parcels, the applicant shall construct an 18-inch S-1 water main in Reed Road SE from Strong Road SE to approximately 500 feet south of Lindberg Road SE.

The applicant's engineer submitted a statement demonstrating compliance with Stormwater PWDS Appendix 004-E(4)(b) and SRC Chapter 71. The preliminary stormwater design demonstrates the use of green stormwater infrastructure to the maximum extent feasible. At the time of development, the applicant's engineer shall design and construct a storm drainage system at the time of development in compliance with SRC Chapter 71 and PWDS.

The applicant shall design and construct all utilities (sewer, water, and storm drainage) according to the PWDS and to the satisfaction of the Public Works Director.

**Criteria—A Class 2 Driveway Approach Permit shall be granted if:**

**(1) The proposed driveway approach meets the standards of this Chapter and the Public Works Design Standards;**

**Finding**—The proposed driveways meet the standards for SRC Chapter 804 and PWDS.

**(2) No site conditions prevent placing the driveway approach in the required location;**



**Finding**—There are no site conditions prohibiting the location of the proposed driveways.

**(3) The number of driveway approaches onto an arterial are minimized;**

**Finding**—The proposed driveways are not accessing onto an Arterial street.

**(4) The proposed driveway approach, where possible:**

- i. **Is shared with an adjacent property; or**
- ii. **Takes access from the lowest classification of street abutting the property;**

**Finding**—The proposed driveways are located with access to the lowest classification of street abutting the subject property.

**(5) Proposed driveway approach meets vision clearance standards;**

**Finding**—The proposed driveway meets the PWDS vision clearance standards set forth in SRC Chapter 805.

**(6) The proposed driveway approach does not create traffic hazards and provides for safe turning movements and access;**

**Finding**—No evidence has been submitted to indicate that the proposed driveways will create traffic hazards or unsafe turning movements. Staff analysis of the proposed driveways indicate that they will not create a traffic hazard and will provide for safe turning movements for access to the subject property.

**(7) The proposed driveway approach does not result in significant adverse impacts to the vicinity;**

**Finding**—Staff analysis of the proposed driveways indicate that the location of the proposed driveways will not have any adverse impact to the adjacent properties or streets.

**(8) The proposed driveway approach minimizes impact to the functionality of adjacent streets and intersections; and**

**Finding**—The proposed driveway approaches are located on Collector streets and do not create a significant impact to adjacent streets and intersections.

**(9) The proposed driveway approach balances the adverse impacts to residentially zoned property and the functionality of adjacent streets.**

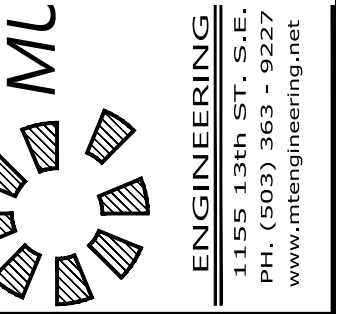
**Finding**—The proposed driveway approaches are located in a mixed-use area. The driveways will not have an adverse impact to residentially zoned property and the functionality of the adjacent streets.

Prepared by: Jennifer Scott, Program Manager  
cc: File

# Attachment G



**ZONE KEY**  
 VC = VILLAGE CENTER  
 AU = ADAPTIVE USE  
 MI = MIXED INTENSITY

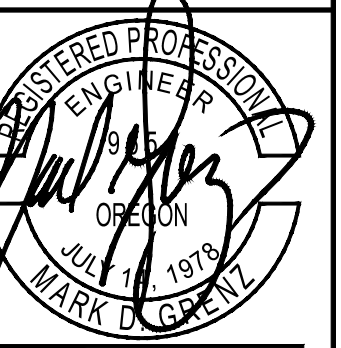


## ZONING MAP

## THE GROVE AT FAIRVIEW 2

NO CHANGES, MODIFICATIONS  
 OR REPRODUCTIONS TO BE  
 MADE TO THESE DRAWINGS  
 WITHOUT WRITTEN  
 AUTHORIZATION FROM THE  
 DESIGN ENGINEER.  
 DIMENSIONS & NOTES TAKE  
 PRECEDENCE OVER  
 GRAPHICAL REPRESENTATION.

7111P SDR4-ZONE  
 Design: M.D.G.  
 Drawn: C.D.S.  
 Checked: B.M.G.  
 Date: \_\_\_\_\_  
 Scale: AS SHOWN

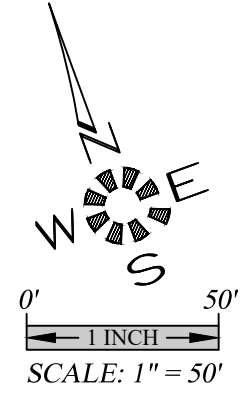


EXPIRES: 06-30-2021  
 JOB # 7111

## SDR4

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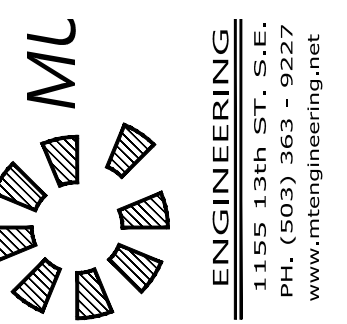
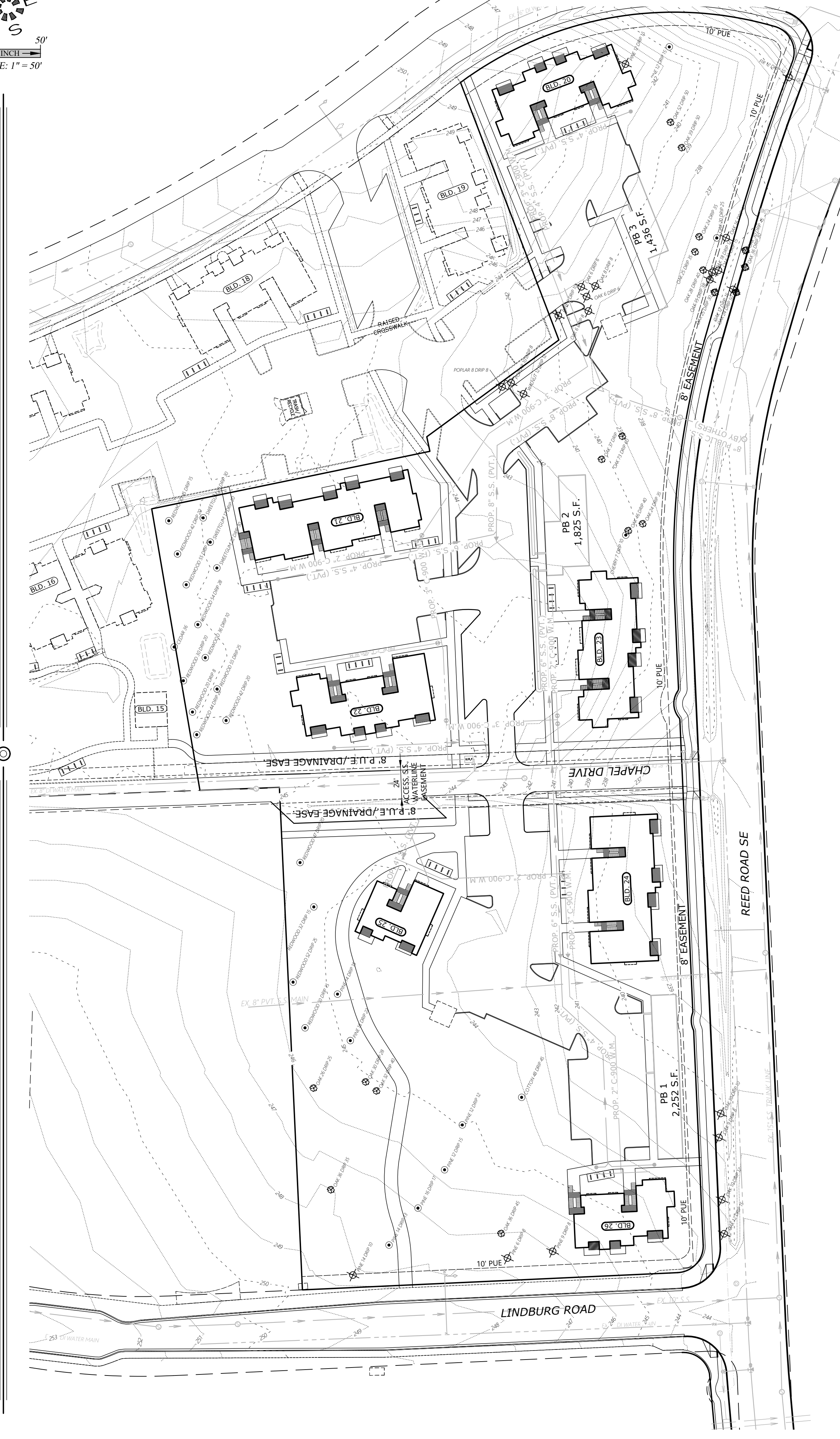
# Attachment H



**TREES WITHIN WORK AREAS**

- ⊙ 28 = EXISTING TREE TO REMAIN
- ⊗ 25 = EXISTING TREE TO BE REMOVED
- ⊗ 14 = EXISTING OAK TREE 24"+ DIA. TO REMAIN
- ⊗ 4 = EXISTING OAK TREE 24"+ DIA. TO BE REMOVED

TREE 00 DRIP 00  
 | DRIP LINE DIA. (FEET)  
 | TRUNK DIA. (INCHES)



## TREE CONSERVATION MAP

## THE GROVE AT FAIRVIEW 2

NO CHANGES, MODIFICATIONS OR REPRODUCTIONS TO BE MADE TO THESE DRAWINGS WITHOUT WRITTEN AUTHORIZATION FROM THE DESIGN ENGINEER.  
 DIMENSIONS & NOTES TAKE PRECEDENCE OVER GRAPHICAL REPRESENTATION.

7111P SDR3 TREE  
 Design: M.D.G.  
 Drawn: C.D.S.  
 Checked: B.M.G.  
 Date: 02/01/20  
 Scale: AS SHOWN



EXPIRES 06-30-2021  
 JOB # 7111

## SDR3

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