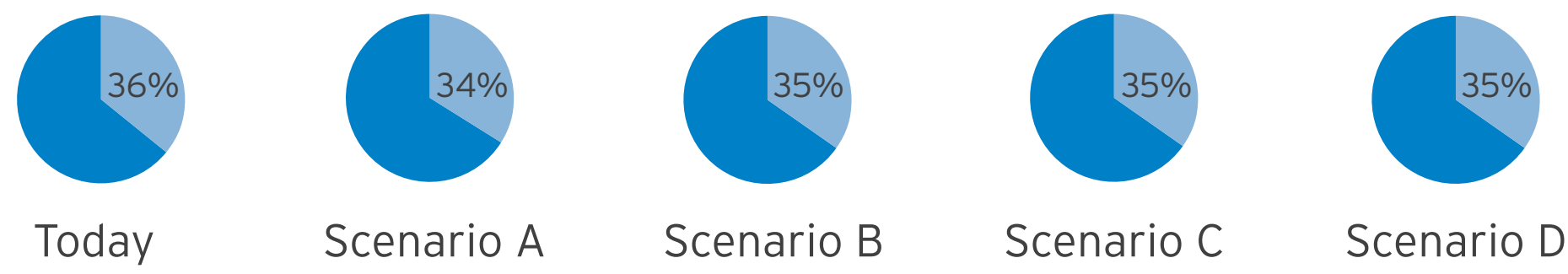


SCENARIO INDICATORS

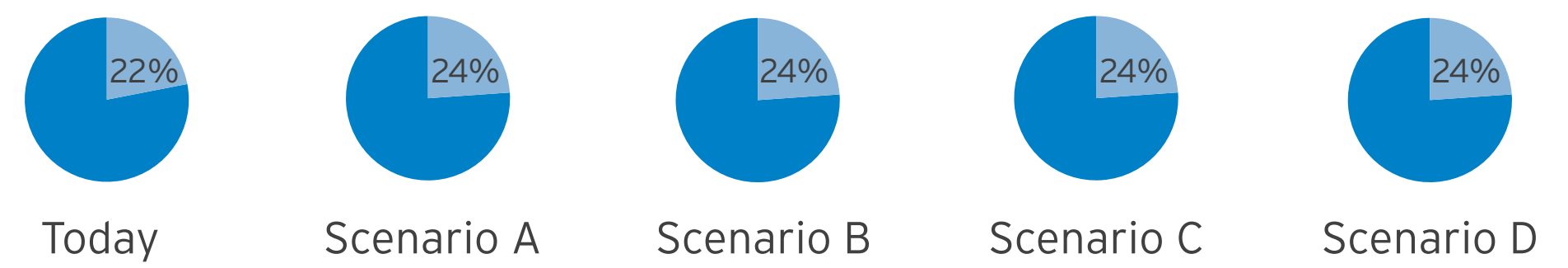
WELCOMING AND LIVABLE COMMUNITIES

Affordability (Housing + Transportation + Energy)



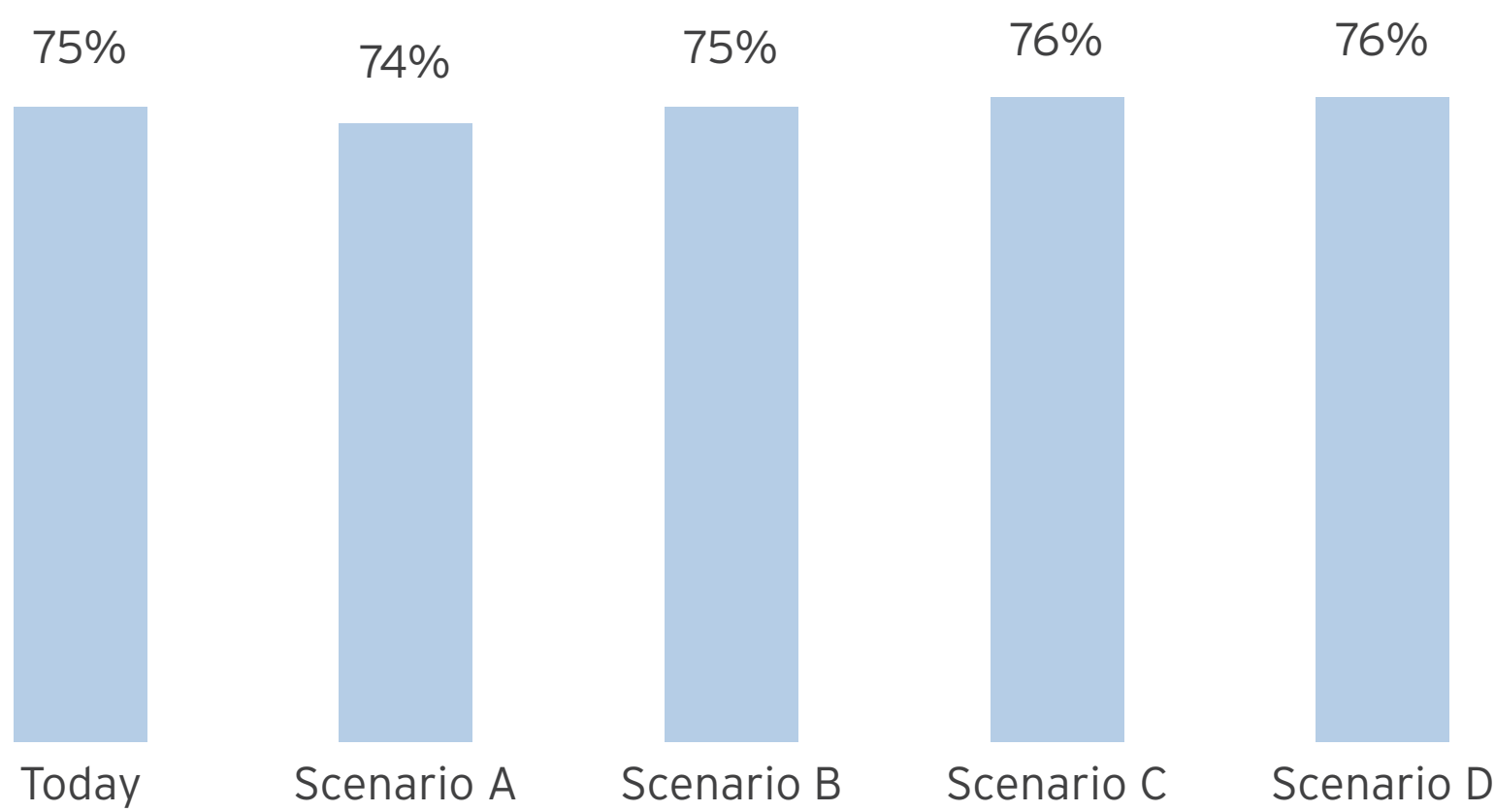
i The percentage of average monthly household income spent on housing, transportation, and energy expenses.
*Updated to align the graphics with the preferred scenario results.

Housing Affordability



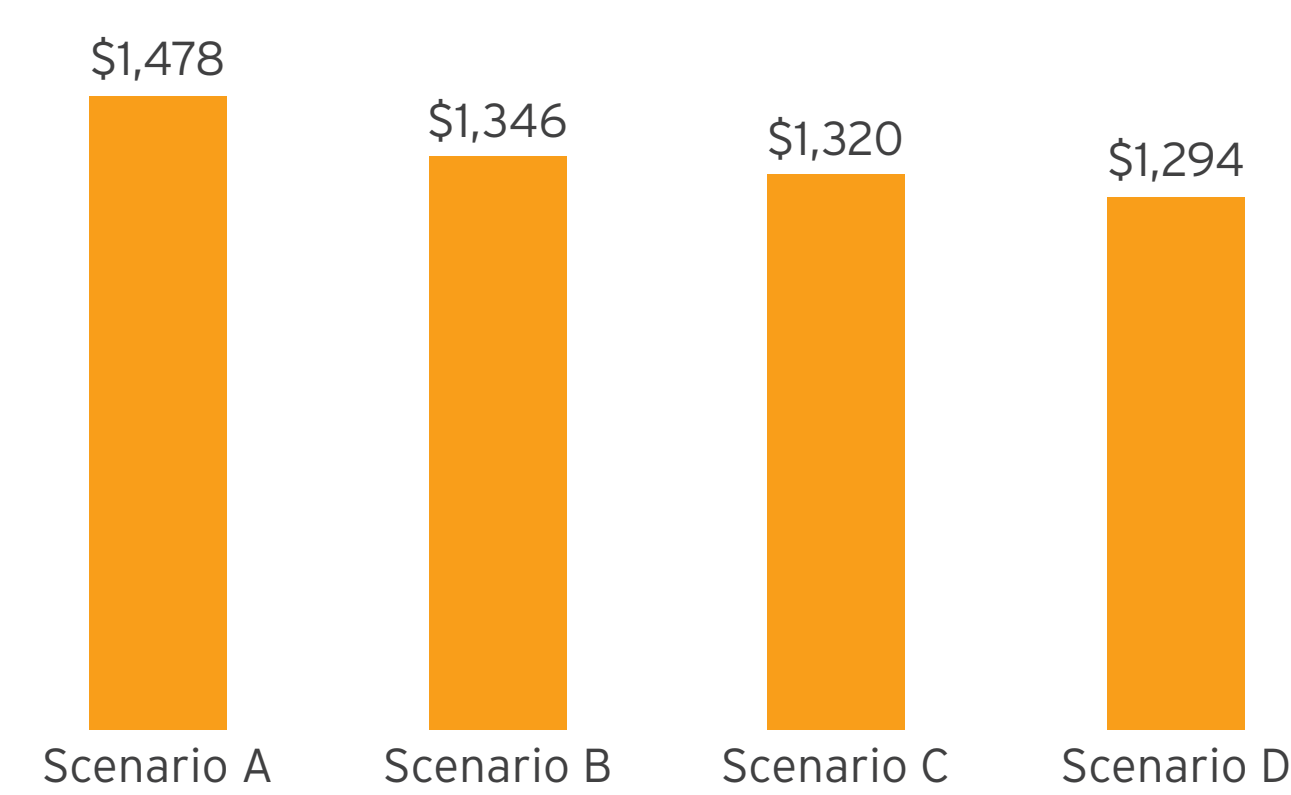
i The percentage of average monthly household income spent on housing.
*Updated to correct previously-inaccurate data and to align the graphics with the preferred scenario results.

Proximity to Parks



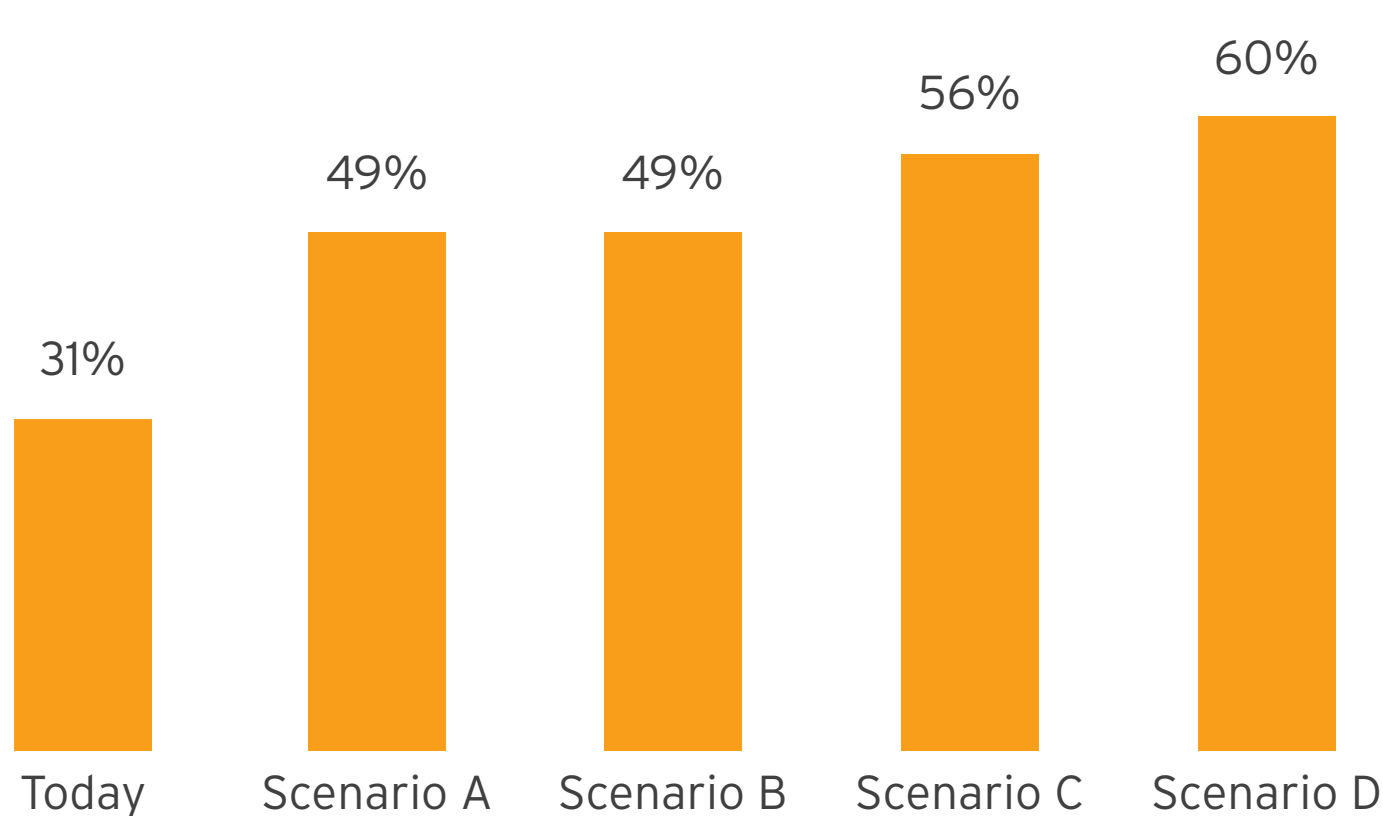
i The percentage of households living within a half-mile of a park.

Monthly Housing Cost



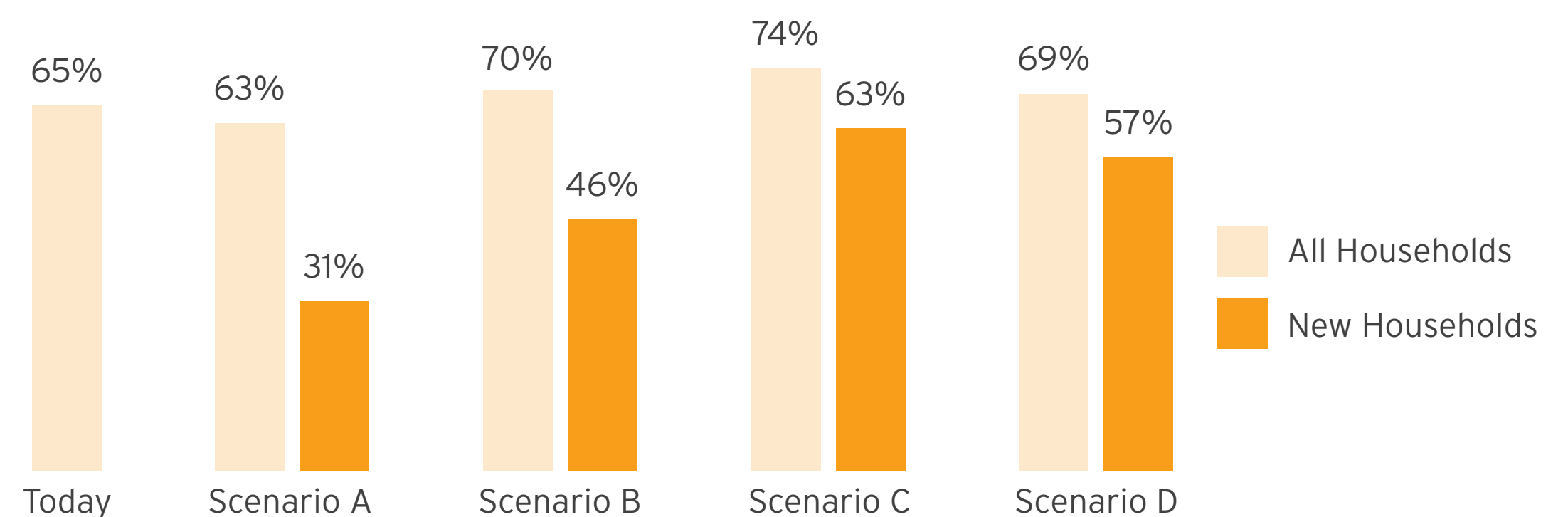
i The average monthly cost of housing in Salem.

Infill Development/Redevelopment



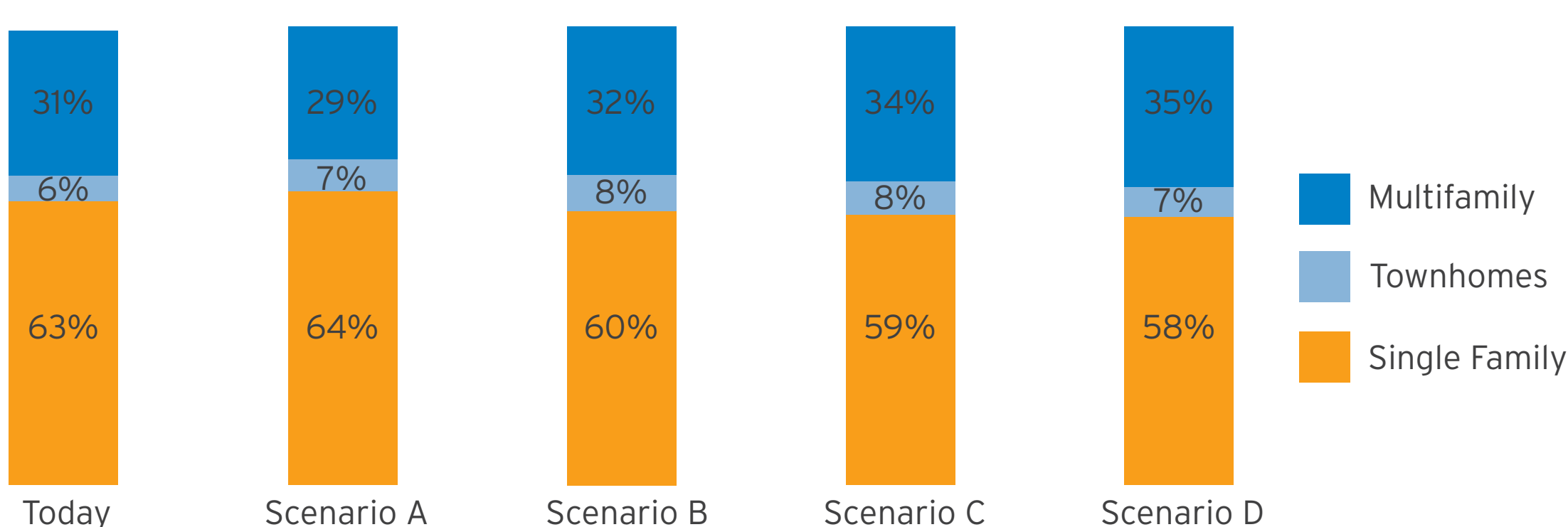
i The percentage of new housing units created from infill (development on partially vacant lots) and redevelopment.

Complete Neighborhoods



i A "complete neighborhood" means people live within walking distance of parks, schools, grocery stores, businesses, and transit. An area was considered complete if it met 5 of the following 7 criteria: street with sidewalk on at least one side, 1/4 mile to a trail or greenway, 1/2 mile to a neighborhood park, 1/4 mile to a bus with frequent service or 1/8 mile to all other stops, 1/2 mile to grocery store, 1/2 mile to business/service cluster, and 1 mile to a public elementary school.

Housing Mix

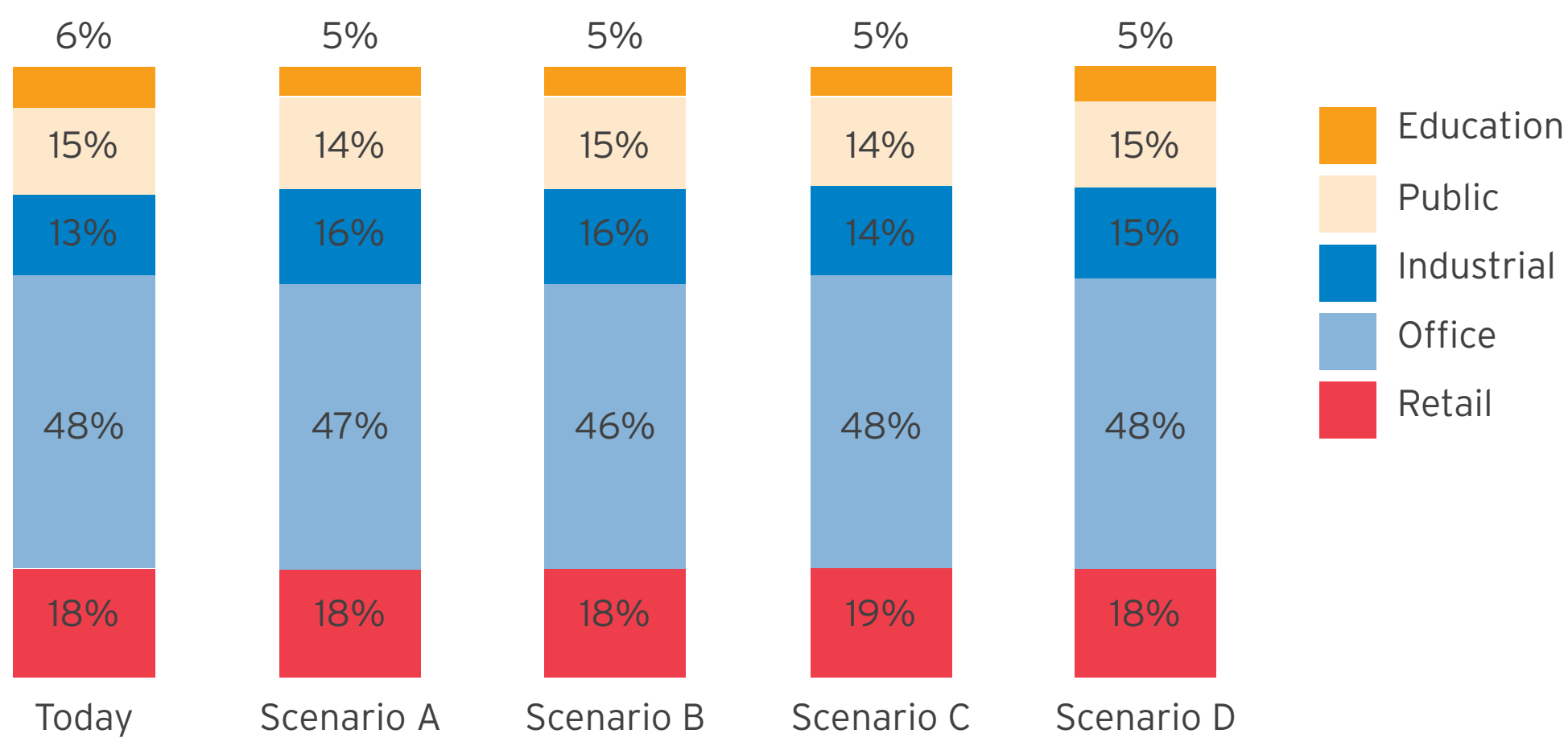


i The percentage of single family, townhome, and multifamily.
*Updated to align the graphics with the preferred scenario results.

SCENARIO INDICATORS

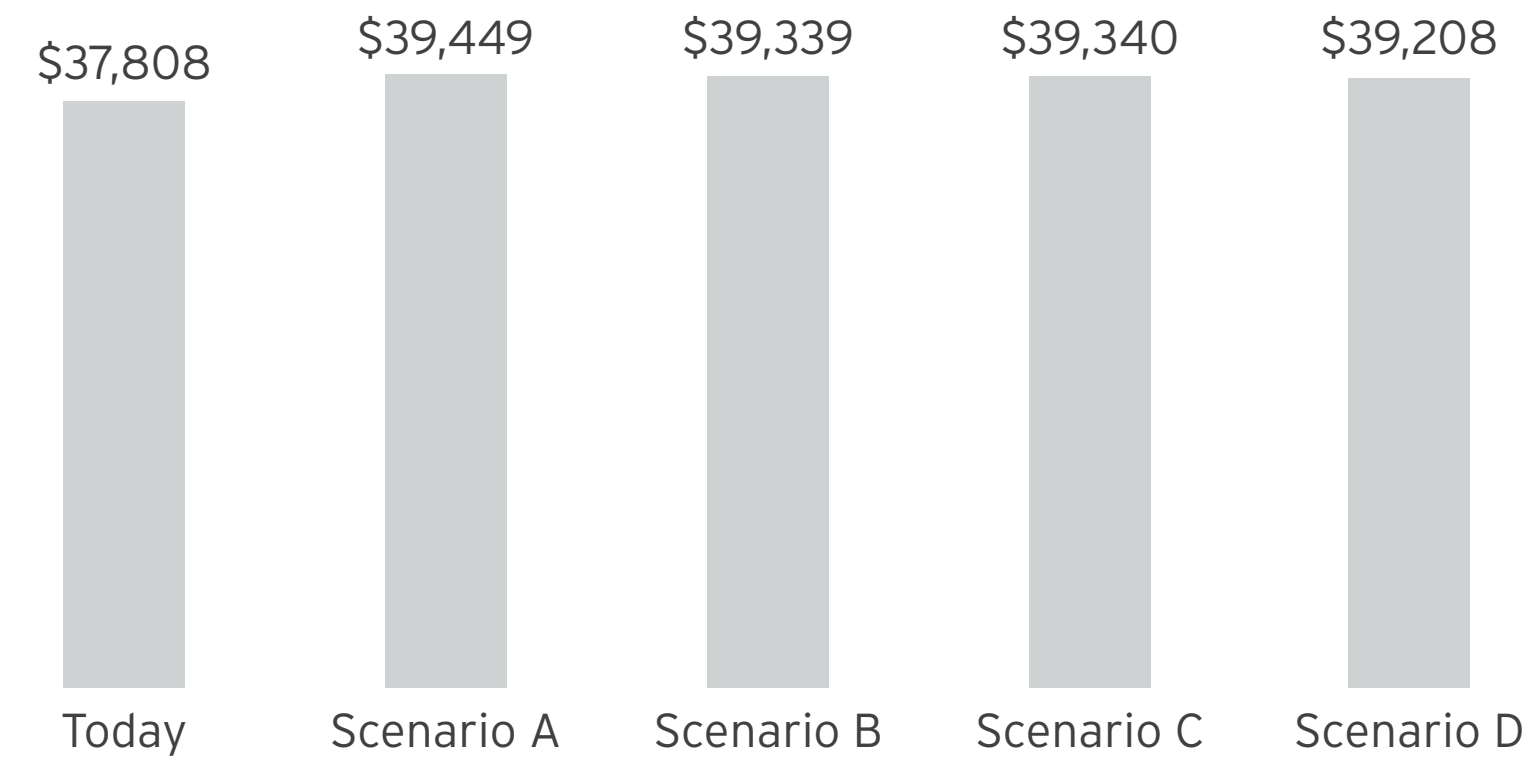
STRONG AND DIVERSE ECONOMY

Employment Mix



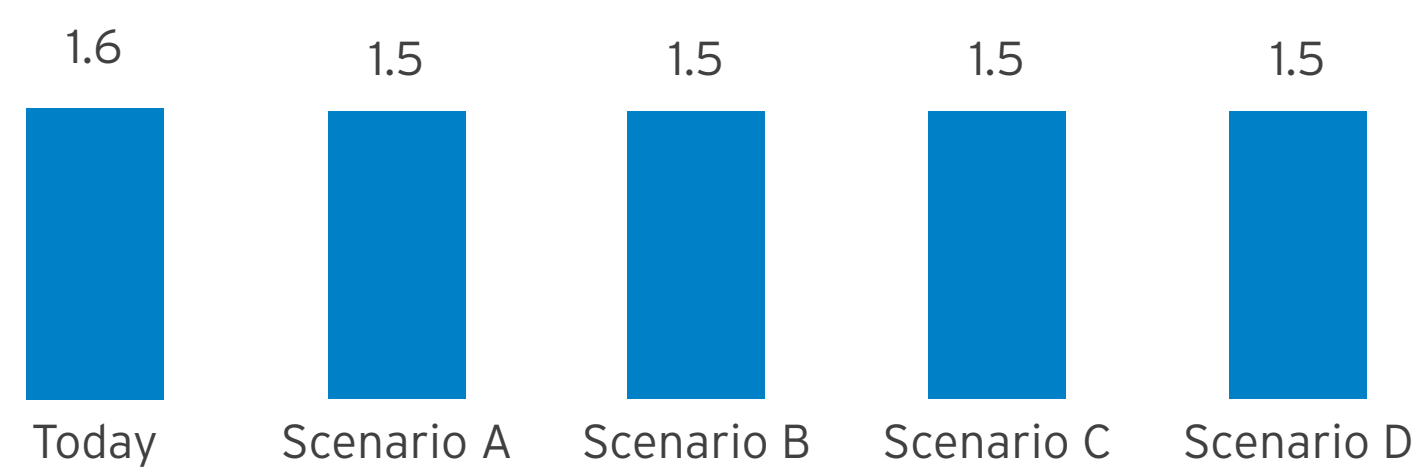
i The percentage of jobs in retail, office, industrial, public sector, and education

Average Wage



i Average annual wages based on today's economy and today's dollar

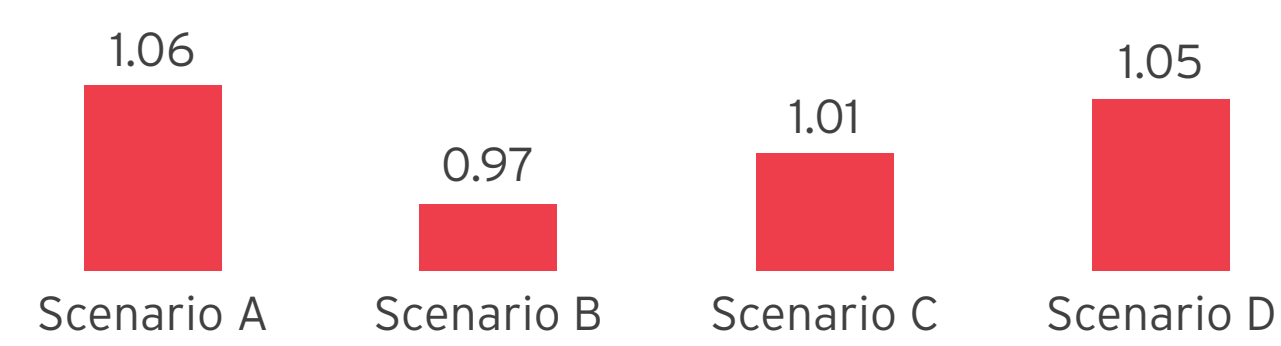
Jobs Housing Balance



i The number of jobs compared to the number of households. A jobs-to-housing ratio of 1.5 jobs to 1 households is recommended by planning scholars.

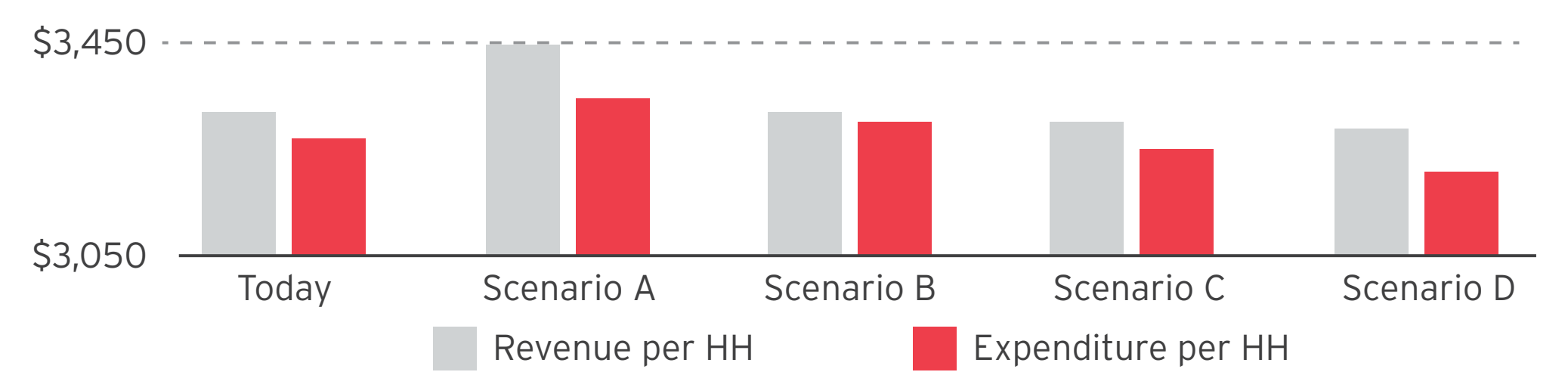
GOOD GOVERNANCE

Revenue-To-Cost Ratio



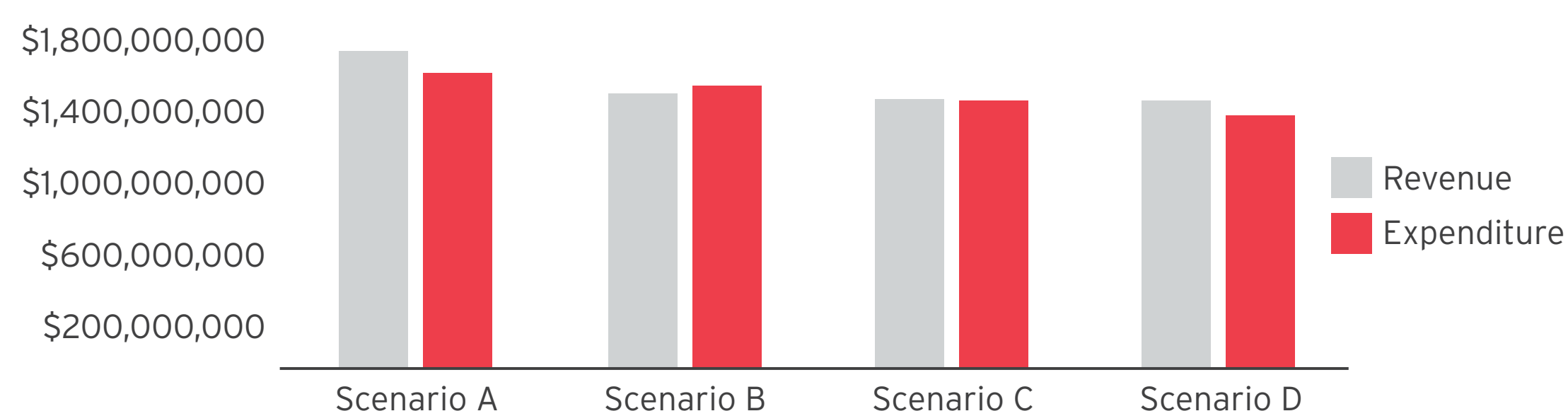
i The total amount of annual revenue compared to the total amount of expenditures. The ratio considers the cost of providing infrastructure, such as pipes and roads, to each person. (Includes all funds and revenue sources, not just general fund)

Annual Level of Service (Expenditures per Capita)



i Annual expenditures and revenue per capita (includes all funds and revenue sources, not just general fund)

Property Tax Revenue

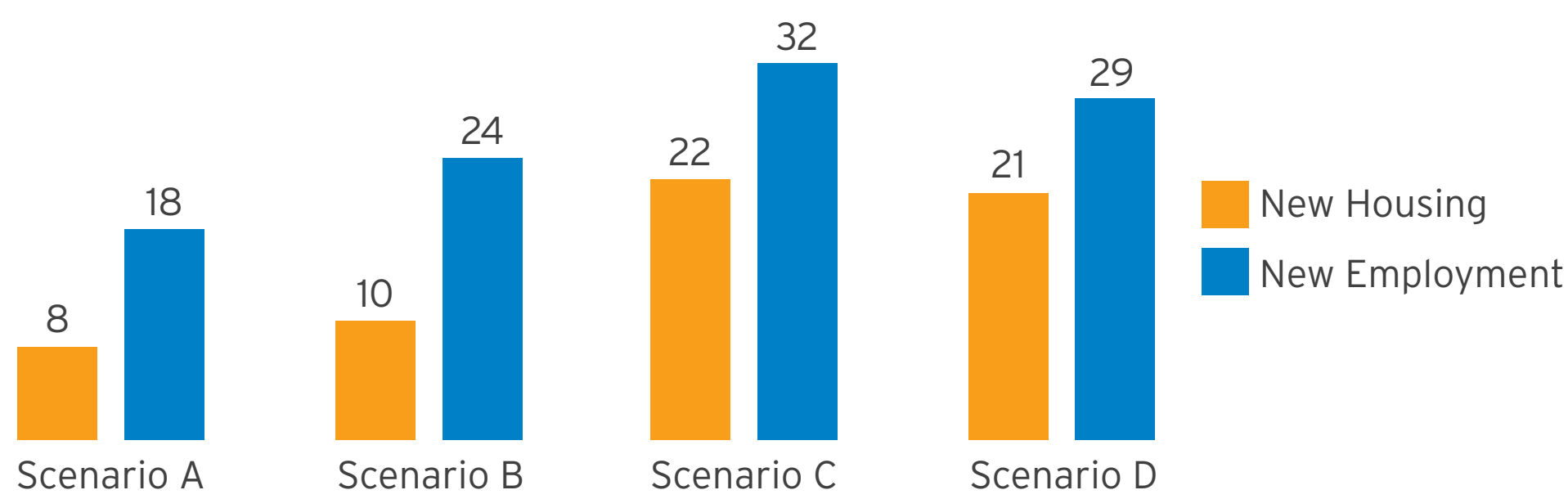


i Annual property tax revenue

SCENARIO INDICATORS

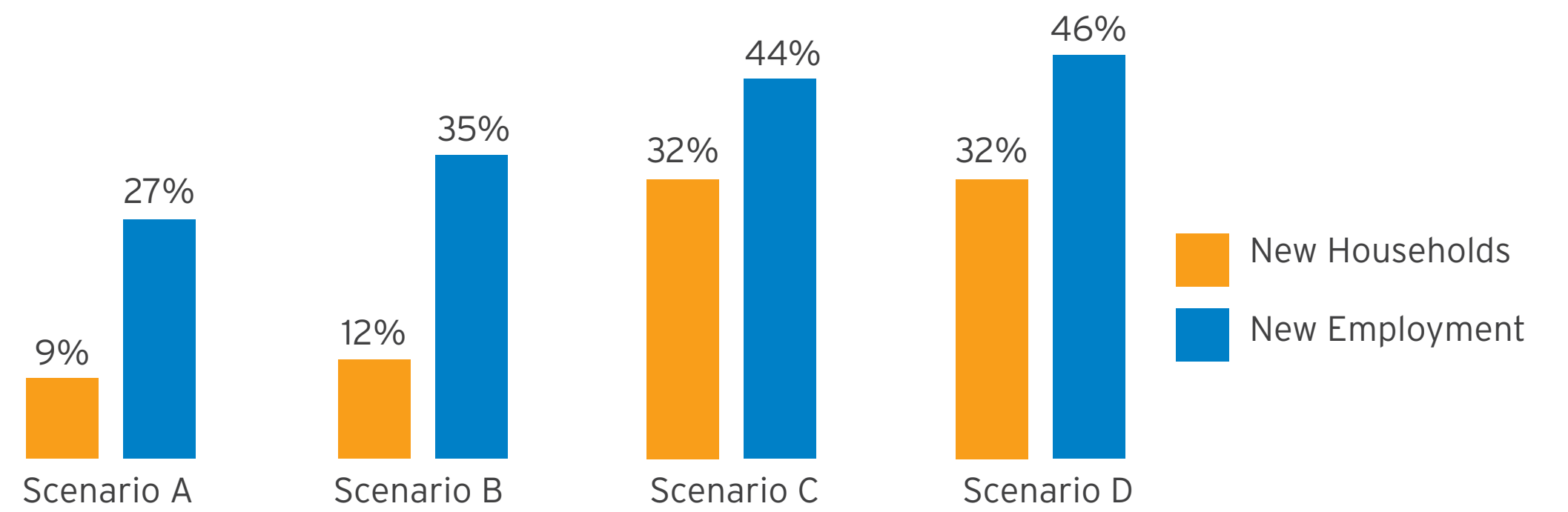
SAFE, RELIABLE, EFFICIENT INFRASTRUCTURE

Walk and Transit Friendliness



i The score is based on ease of walking, access to transit, proximity to a variety of land uses, and other factors that allow for a range of travel choices. The score is between 0 and 100. A higher score means non-drivers can safely access the places they need to go.

Access to Frequent Transit



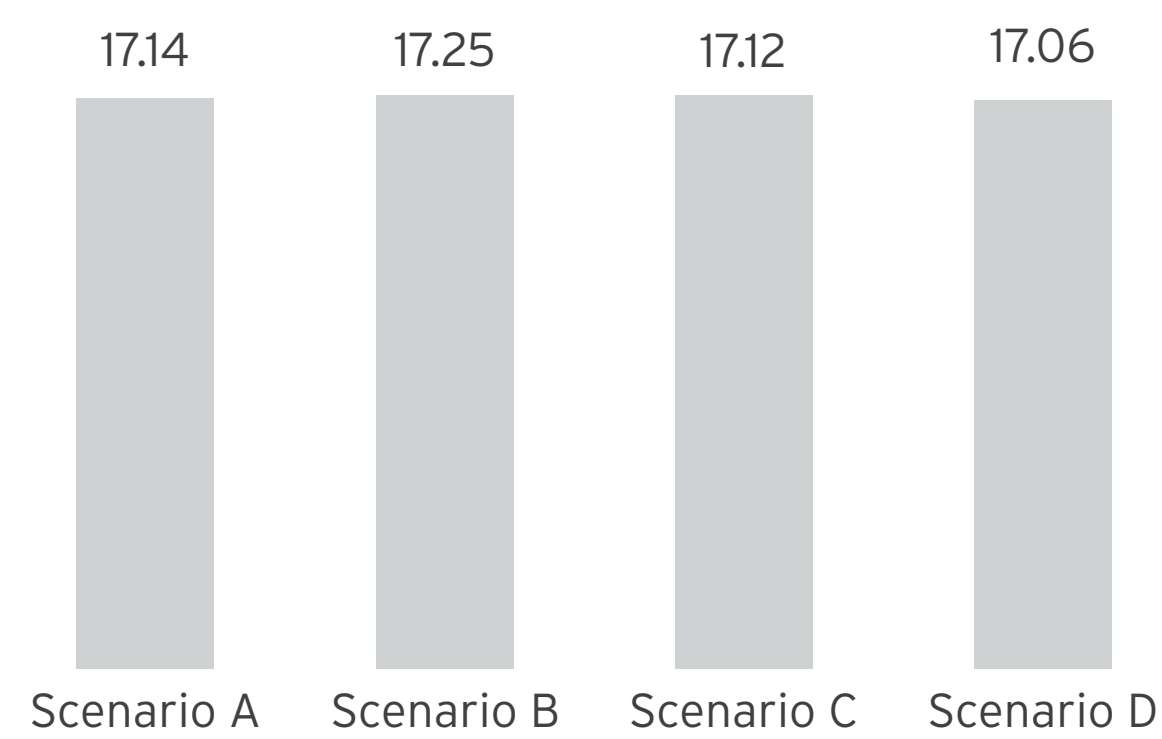
i The percentage of new households and jobs within ¼ mile of bus stops with 15-minutes service. Salem's goal is to have at least 10.5% of new housing units within ¼ mile walking distance of a Cherriots bus stop with 15-minute service.

Mode Split

	State	Scenario A	Scenario B	Scenario C	Scenario D
Vehicle	82%	83.3%	83.1%	82.2%	81.7%
Bus	2%	1.9%	2.0%	2.2%	2.6%
Bike	3%	1.5%	1.5%	1.6%	1.6%
Walk	10%	13.0%	13.2%	13.9%	14.0%

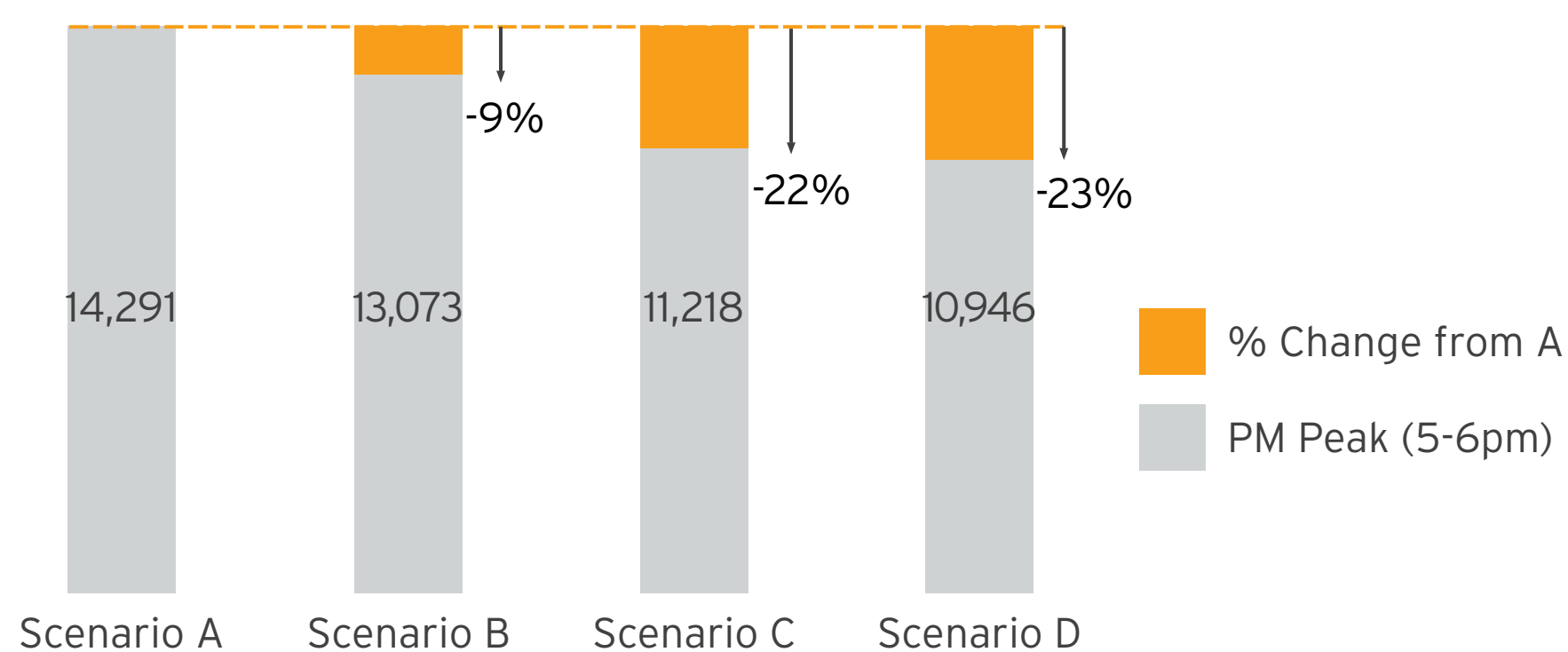
i *Data includes all trips, not just trips to work
The percentage of all trips by bike, foot, car, and bus. By 2030, Salem is aiming for 5% of commute trips to work by bike and 11% by foot.

Vehicle Miles Traveled per Capita



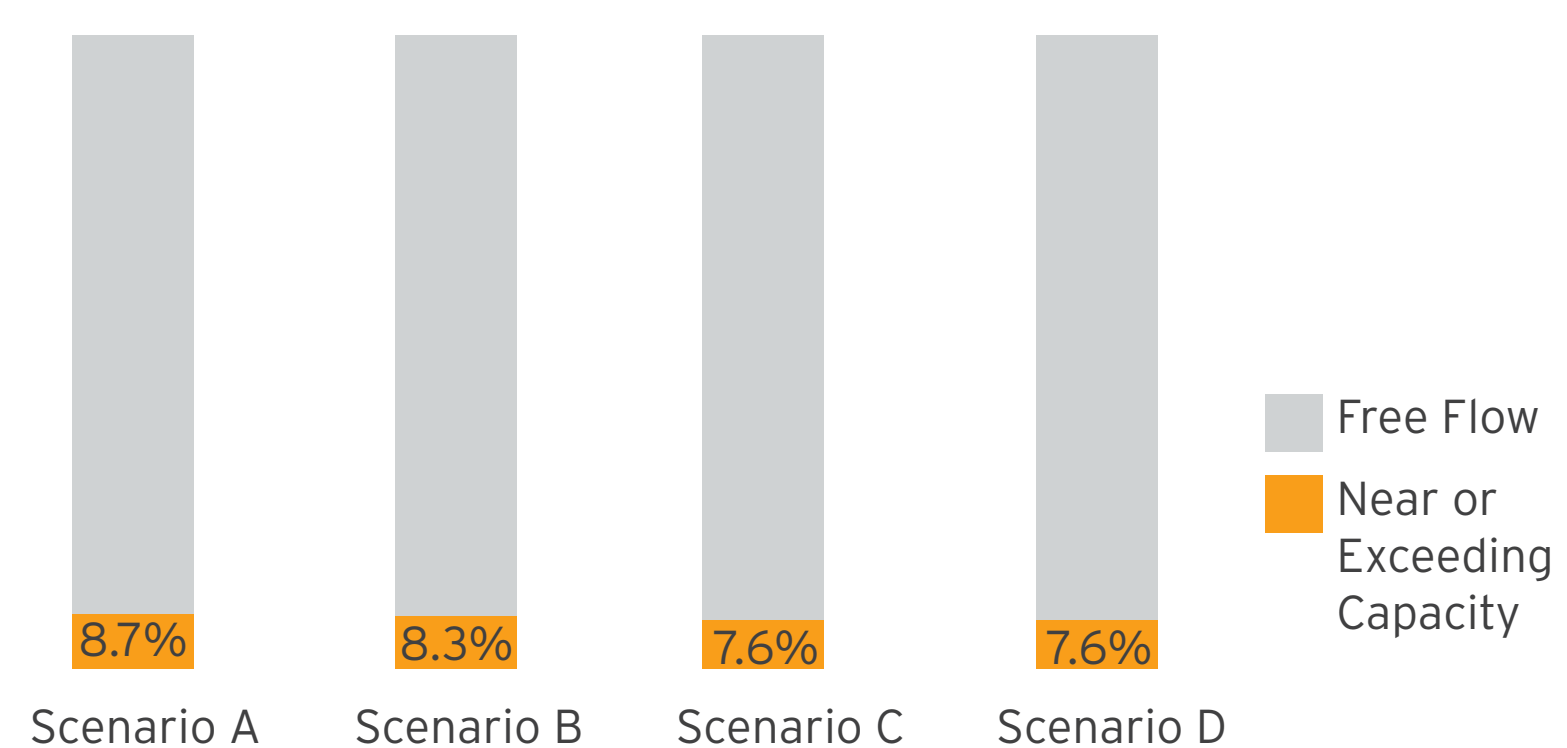
i The daily amount of vehicle travel per person.

Vehicle Hours of Delay



i The total amount of extra time (in hours) it takes to travel on all roads from 5 to 6 p.m. compared to the amount of time it takes when the roads are not congested. This provides a general indication of traffic congestion during the afternoon peak travel period.

Volume to Capacity

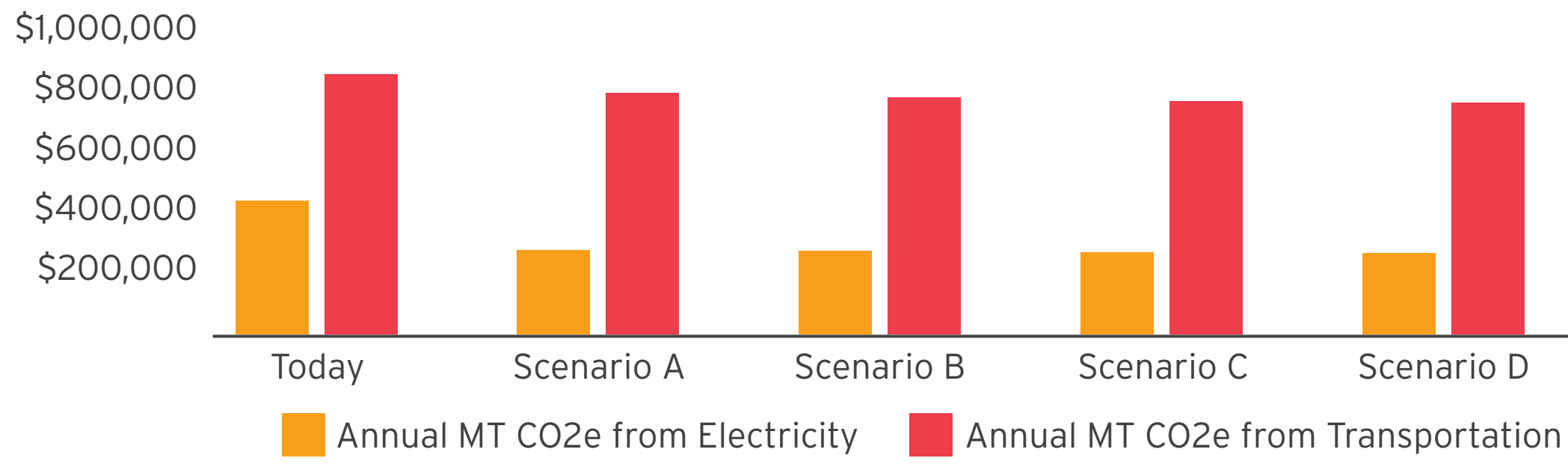


i The percentage of roadway miles where the number of vehicles on the roads are close to or exceeding the number of vehicles the roads were designed to handle. This provides a general indication of traffic congestion during the afternoon peak travel period from 5 to 6 p.m.

SCENARIO INDICATORS

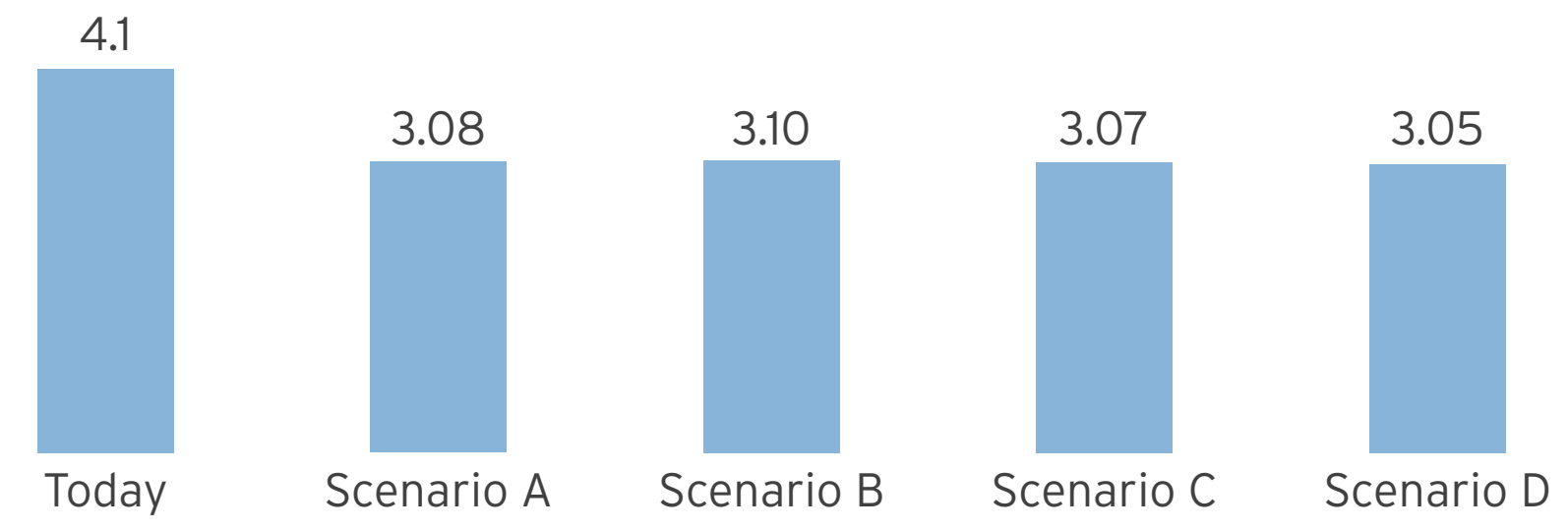
NATURAL ENVIRONMENTAL STEWARDSHIP

Greenhouse Gas Emissions: Electricity and Transportation



i Annual metric tons of CO2e from electricity use and transportation.
*Updated to correct previously-inaccurate data.

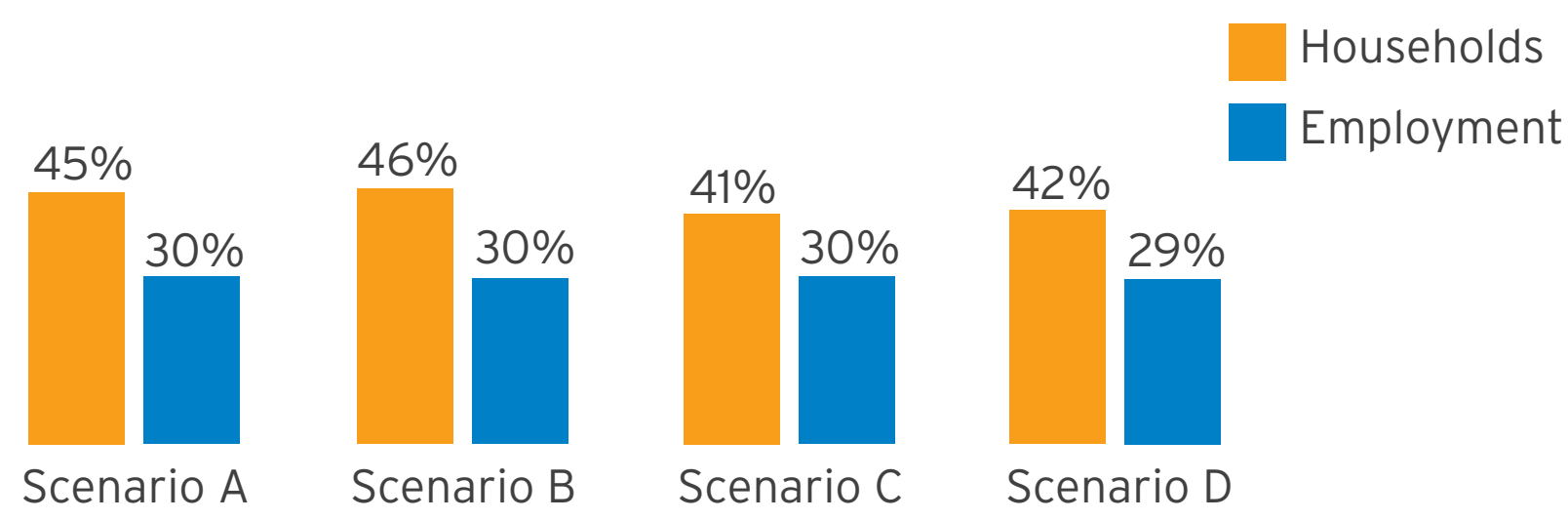
Air Pollution from Travel



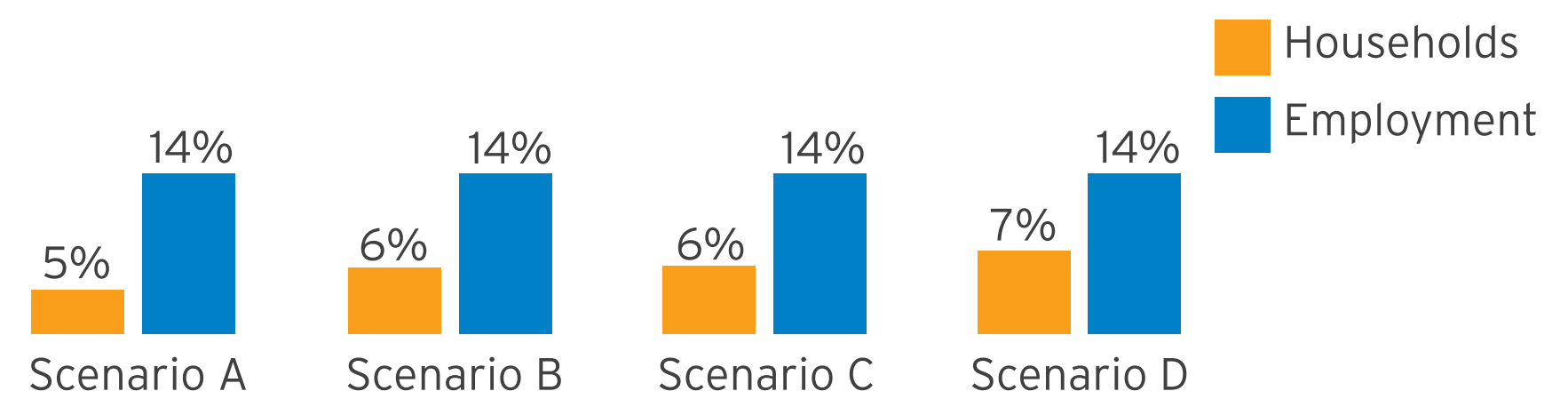
i Annual metric tons of CO2e per capita emissions from transportation.
*Updated to correct previously-inaccurate data.

Development in Environmentally Sensitive Areas

Slopes (>10%)



Floodplains



i The percentage of development on floodplains and on land with slopes more than 10 percent.
*Updated to correct previously-inaccurate data and to align the graphics with the preferred scenario results.