

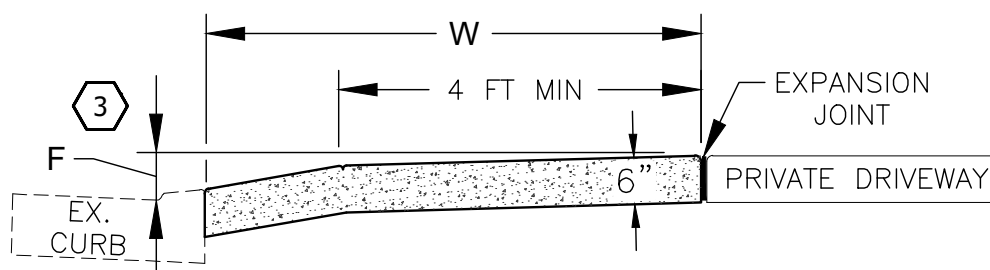
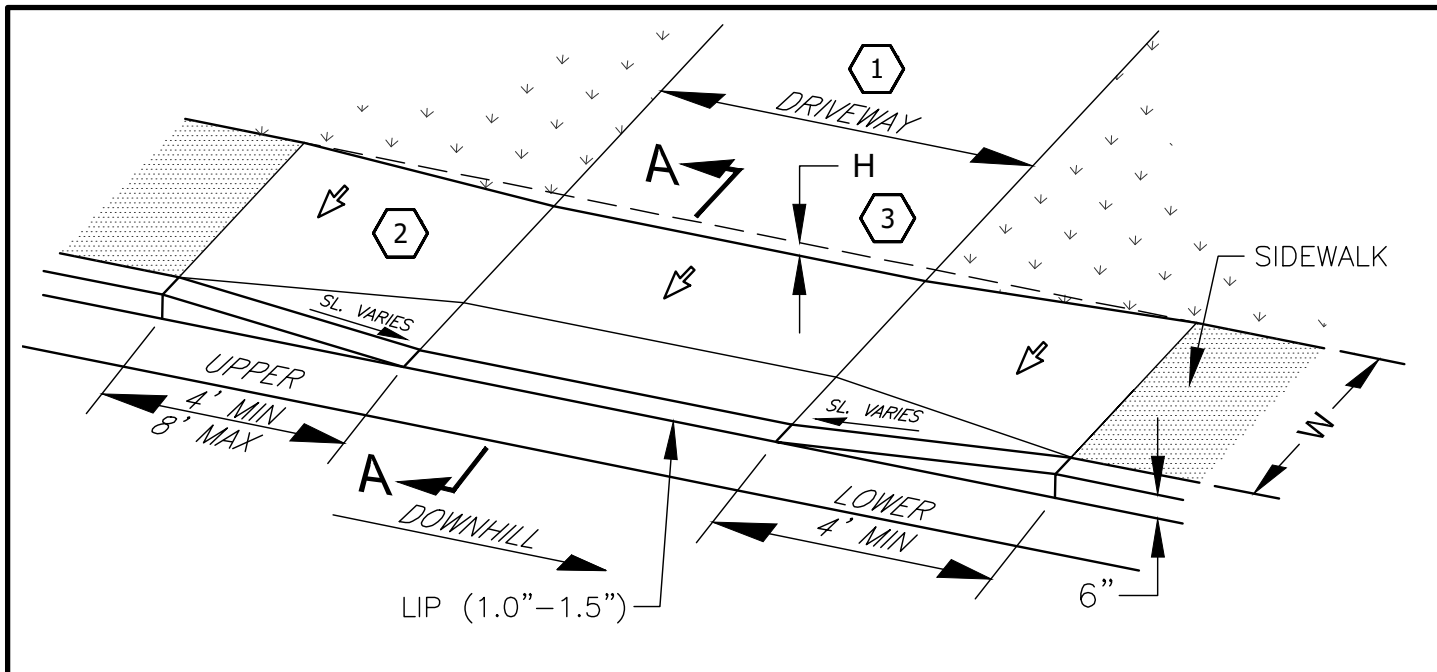
CITY OF SALEM
DEPARTMENT OF PUBLIC WORKS
STANDARD DRAWINGS
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Plan No.	Title	Date
Streets		
301A	Standard Driveway Approach—Curblin Sidewalk	12-27-2019
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308	Mid-Block Walk Entrance Traffic Control	09-15-1999
309	Pavement Patching	09-15-1999
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318	Typical Street Sections	09-15-1999
319	Traffic Island Installation—18" to 24" Wide	03-28-2001
320	Traffic Island Installation—24" to 60" Wide	03-28-2001
321	Traffic Island Installation—Greater than 60" Wide	03-28-2001

**CITY OF SALEM
DEPARTMENT OF PUBLIC WORKS
STANDARD DRAWINGS**

Plan No.	Title	Date
322A	Pavement Marking Details	12-27-2019
322B	Pavement Marking Details	07-13-2016
322C	Pavement Marking Details	07-13-2016
323	Pipe Sign Support Detail	09-12-2011
325	Speed Hump and Striping	09-12-2011
326	Temporary Sign Support	08-16-2017



SECTION A-A

KEYNOTES

- 1 DRIVEWAY WIDTH AS SPECIFIED ON PERMIT OR AS SHOWN ON PLANS.
- 2 7.5% TYPICAL (8.3% MAXIMUM) SLOPE, BUT SHALL NOT REQUIRE THE LENGTH TO EXCEED 8- FEET. APPROXIMATE MINIMUM LENGTHS REQUIRED FOR VARIOUS STREET SLOPES ARE SHOWN IN TABLE BELOW:

STREET SLOPE %	UPPER TRANSITION MINIMUM LENGTH (W=5')
1.0	4.0'
2.0	4.5'
3.0	5.25'
4.0	6.5'
>5.0	8.0'

- 3 DIMENSIONS "F" AND "H" VARY WITH SIDEWALK WIDTH 'W'.

W (FT)	H (INCHES)	F (INCHES)
5	3.25	4.0
6	2.75	4.75
7	2.25	5.5
8		7.0

GENERAL NOTES

- ALL SLOPES ARE RELATIVE TO TRUE HORIZONTAL.
- DRIVEWAY APPROACH AND TRANSITIONS SHALL BE MINIMUM 6-INCH THICK COMMERCIAL GRADE CONCRETE.
- REFER TO SIDEWALK STANDARDS FOR BROOM FINISH; JOINTS; TOOLED EDGES.
- ALL MEASUREMENTS ARE BASED ON 6 INCH CURB REVEAL.
- SLOPE = 1.5% TYPICAL (2.0% MAXIMUM)

CHANGES

REVISED KEYNOTE 2
CHANGED 2% MAX TO HOLLOW ARROW WITH GENERAL NOTE
INCREASED GUTTER LIP

CITY OF SALEM DEPARTMENT OF PUBLIC WORKS

STANDARD PLAN STANDARD DRIVEWAY APPROACH CURBLINE SIDEWALK

APPROVED

[Signature]
CITY ENGINEER

12/27/19

DATE

DRAWN BY

JAK

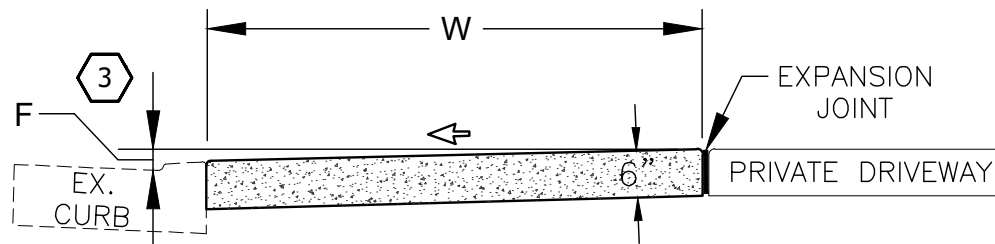
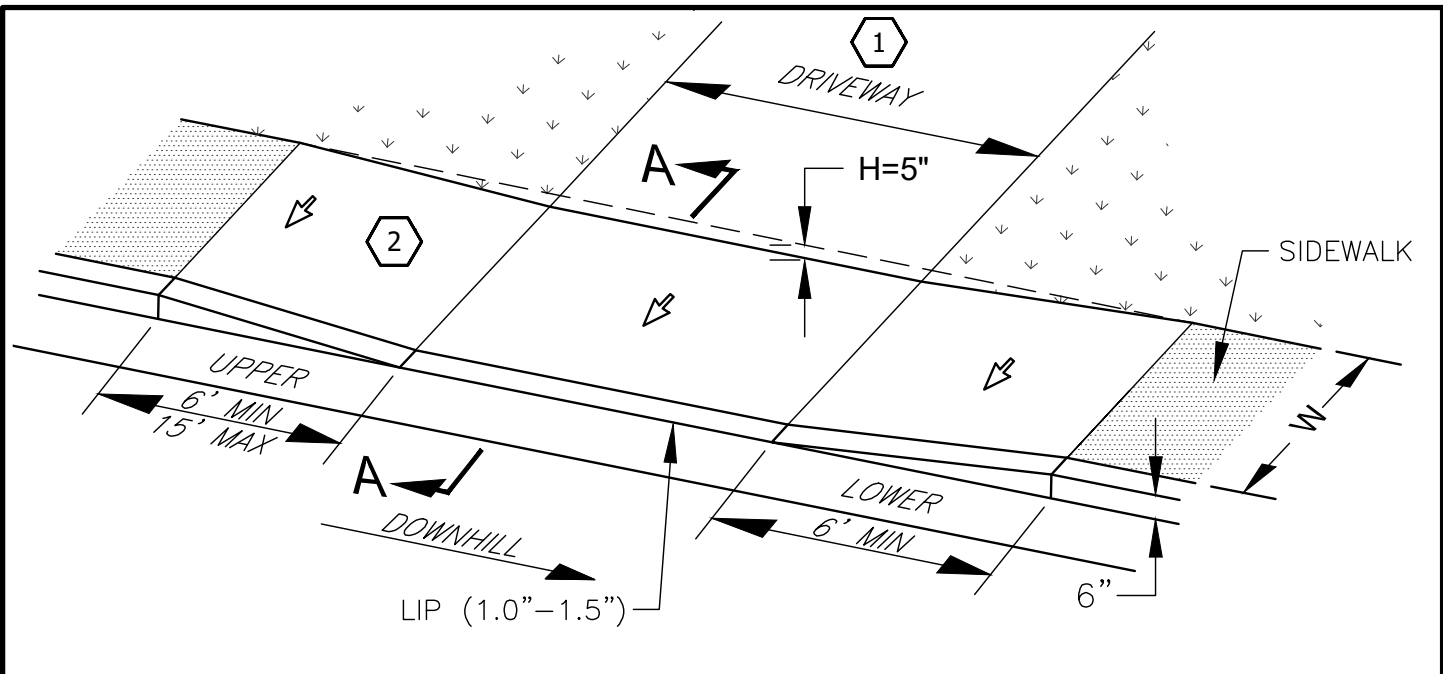
10/2019

CHECKED BY

DEW

10/2019

NO.301A



SECTION A-A

KEYNOTES

- 1 DRIVEWAY WIDTH AS SPECIFIED ON PERMIT OR AS SHOWN ON PLANS.
- 2 7.5% TYPICAL (8.3% MAXIMUM) SLOPE, BUT SHALL NOT REQUIRE THE LENGTH TO EXCEED 15-FEET. APPROXIMATE MINIMUM LENGTHS REQUIRED FOR VARIOUS STREET SLOPES ARE SHOWN IN TABLE BELOW:

STREET SLOPE %	UPPER TRANSITION MINIMUM LENGTH (FEET)
1.0	6.0
2.0	7.0
3.0	8.5
4.0	10.25
5.0	13.5
>5.5	15.0

- 3 DIMENSION "F" VARIES WITH SIDEWALK WIDTH "W".

W (FEET)	F (INCHES)
5	2.25
6	2.5
7	2.75
8	3.0

GENERAL NOTES

- CONSTRUCT THIS APPROACH TYPE FOR COMMERCIAL DRIVEWAYS ON HIGHER SPEED STREETS.
- ALL SLOPES ARE RELATIVE TO TRUE HORIZONTAL.
- DRIVEWAY APPROACH AND TRANSITIONS SHALL BE MINIMUM 6-INCH THICK COMMERCIAL GRADE CONCRETE.
- REFER TO SIDEWALK STANDARDS FOR BROOM FINISH; JOINTS; TOOLED EDGES.
- ALL MEASUREMENTS ARE BASED ON 6 INCH CURB REVEAL.

← SLOPE = 1.5% TYPICAL (2.0% MAXIMUM)

CHANGES

REVISED KEYNOTE 2
CHANGED 2% MAX TO HOLLOW ARROW WITH GENERAL NOTE
INCREASED GUTTER LIP

CITY OF SALEM DEPARTMENT OF PUBLIC WORKS

STANDARD PLAN FULLY LOWERED DRIVEWAY APPROACH CURBLINE SIDEWALK

APPROVED

[Signature]
CITY ENGINEER

12/27/19

DATE

DRAWN BY

JAK

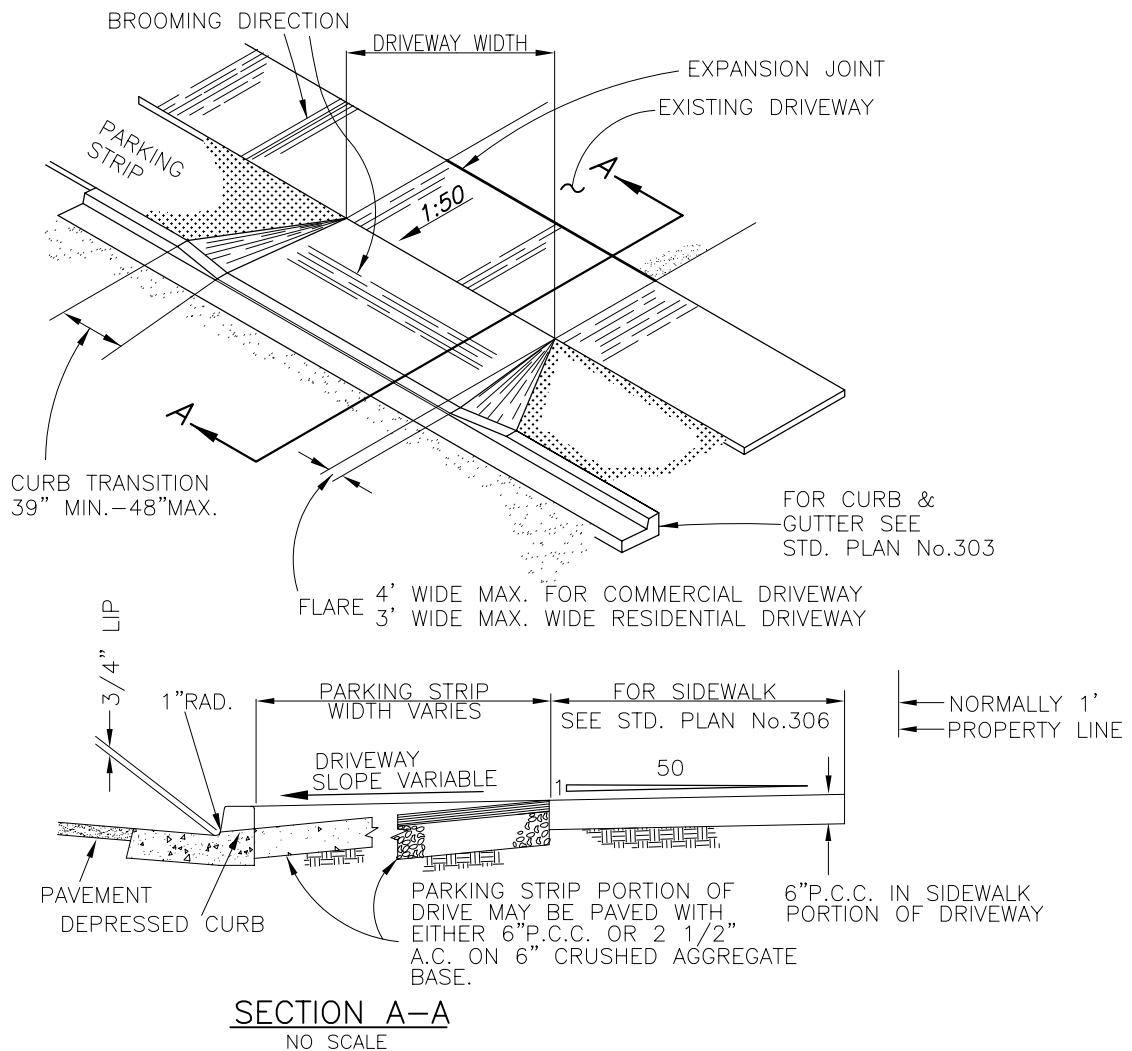
10/2019

CHECKED BY

DEW

10/2019

NO.301B



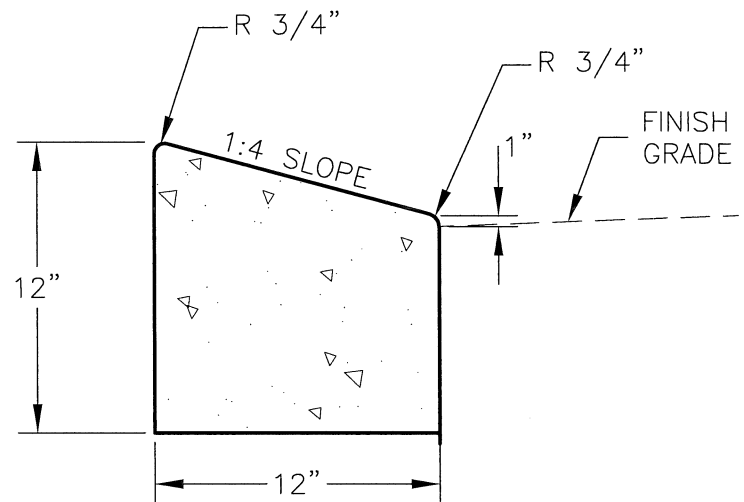
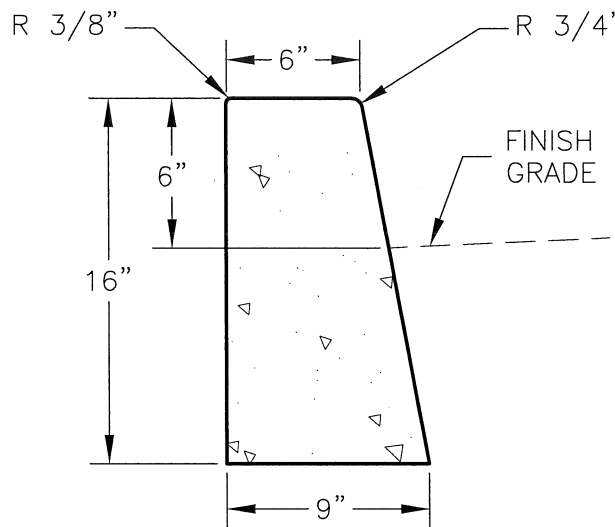
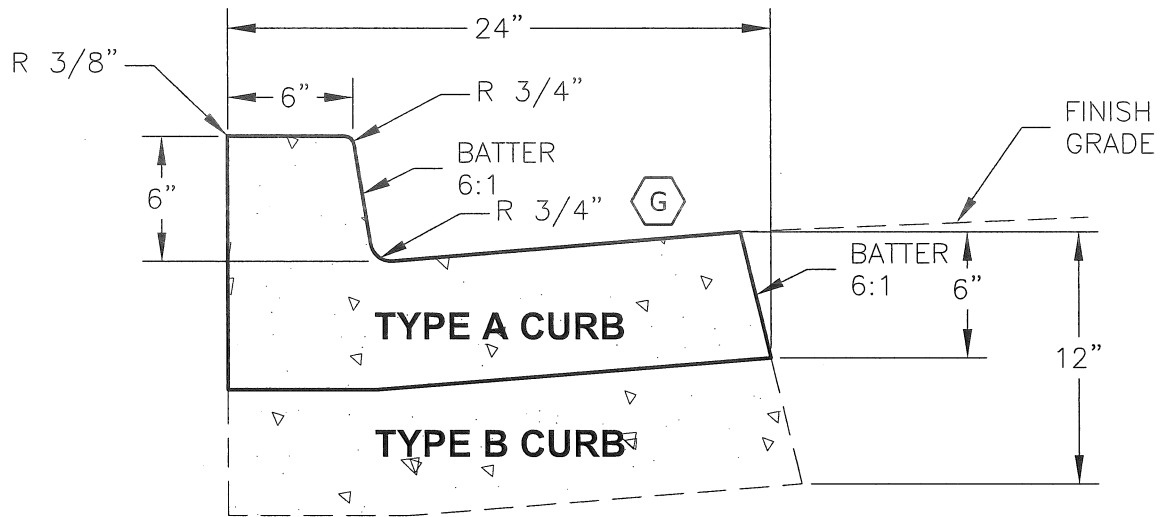
NOTES:

1. DRIVEWAY WIDTH WILL NORMALLY BE SHOWN ON PROJECT PLANS. WHEN NOT SHOWN, WIDTH SHALL BE AS DIRECTED BY THE ENGINEER. IN EITHER CASE, THE DRIVEWAY WIDTH SHALL NOT EXCEED THE LIMITS SET FORTH IN SALEM REVISED CODE CHAPTER 80.
2. SIDEWALKS, INCLUDING THAT PORTION CROSSING THE DRIVEWAY SHALL HAVE TRANSVERSE CONTRACTION JOINTS AT 5' INTERVALS AND TOOL ROUNDED BEFORE BROOMING. ALL EDGES SHALL BE TOOL ROUNDED AND SHINED (3") AFTER BROOMING.
3. WHEN EXISTING DRIVEWAY CANNOT BE MATCHED TO NEW DRIVEWAY WITHIN SLOPE LIMITATIONS SHOWN, ADJUST EXISTING DRIVEWAY—NOT CURB AND SIDEWALK GRADE.
4. EXPANSION JOINTS 1/2"x3 1/2" PREMOLDED JOINT MATERIAL AT LOCATIONS SHOWN.
5. THE DIMENSIONS OF DRIVEWAY APPROACH SHALL NOT BE ADJUSTED WITHOUT SPECIFIC PRIOR (BEFORE FORMING) APPROVAL OF THE INSPECTOR.
6. CONCRETE STRENGTH SHALL BE PER SCS 308.
7. THE 1:50 CROSS-SLOPE OF SIDEWALK IS MEASURED FROM HORIZONTAL.
8. SEE SIDEWALK DETAILS FOR RESTRICTIONS AND SPECIFICATIONS NOT SHOWN.

Approved Karl O. Eubank 5-18-04
City Engineer Date

DELETED ONE EXPANSION JOINT	5/04
CURBLINE DETAIL CONVERTED TO SEPARATE DRAWING	7/99
Description	Date
REVISION	

CITY OF SALEM DEPARTMENT OF PUBLIC WORKS	
STANDARD PLAN DRIVEWAY APPROACH PROPERTY LINE SIDEWALK	
LAST REV. BY: DTN	NO.302
CHECKED BY: DEW	



GUTTER PAN NOTES

- SLOPE 5% NORMAL.
- SLOPE -5% MAX ON HIGH SIDE CURB ON STREETS HAVING SHED SECTION.

GENERAL NOTES

- CURB TYPE WILL BE SHOWN ON PLANS.
- CONSTRUCT EXPANSION JOINTS AT 200 FOOT MAXIMUM SPACING.
- CONSTRUCT CONTRACTION JOINTS AT 10 FOOT SPACING.

CITY OF SALEM DEPARTMENT OF PUBLIC WORKS

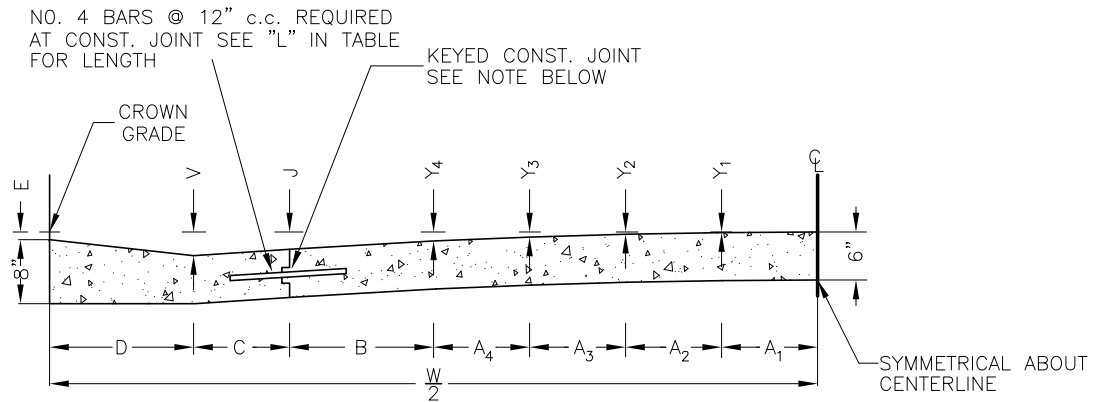
STANDARD PLAN CURBS

CHANGES	DELETE 303B STD. PLAN	CHANGE TITLE AND NUMBER
	CONSOLIDATE TYPE A&B DETAIL	
	ADD GUTTER PAN NOTES	
	ADD TYPE D MOUNTABLE CURB	

APPROVED		12.18.15
	CITY ENGINEER	

DRAWN BY	JAK	2015
CHECKED BY	DEW	2015

NO.303



HALF- SECTION OF P.C.C. ALLEY

		DIMENSIONS																		
		A ₁	A ₂	A ₃	A ₄	A ₅	A ₆	B	C	D	Y ₁	Y ₂	Y ₃	Y ₄	Y ₅	Y ₆	J	V	E	L
ALLEY WIDTH - W	9'	1'-0"	1'-0"					1'-0"	8"	10"	0.012	0.047					0.107	0.160'	0.007	12"
	12'	1'-0"	1'-0"	1'-0"				1'-6"	8"	10"	0.006'	0.024'	0.053'				0.122'	0.160'	0.080'	12"
	14'	1'-0"	1'-0"	1'-0"	1'-0"			1'-6"	8"	10"	0.005'	0.021'	0.048'	0.085'			0.160'	0.200'	0.120'	12"
	16'	1'-0"	1'-0"	1'-0"	1'-0"			1'-6"	1'-0"	1'-6"	0.006'	0.024'	0.053'	0.094'			0.180'	0.250'	0.080'	16"
	20'	1'-0"	1'-0"	1'-0"	1'-0"	1'-0"	1'-0"	1'-6"	1'-0"	1'-6"	0.004'	0.018'	0.040'	0.071'	0.111'	0.160'	0.250'	0.320'	0.150'	16"

NOTES:

- ALL CONC. SHALL BE CLASS 4000 P.S.I. MAX SLUMP 3" UNLESS OTHERWISE SPECIFIED BY THE ENGINEER
- FOR FINISH, CURING AND OTHER REQUIREMENTS SEE SPECIFICATIONS.
- ALLEY MAY BE POURED MONOLITHICALLY OR GUTTER SECTIONS MAY BE PLACED SEPARATELY AS SHOWN, IF THE SECTIONS ARE PLACED SEPARATELY THE CONSTRUCTION JOINT SHALL BE KEYED AND DOWELED

Approved

Karl O. Guter
City Engineer

9-15-99
Date

CITY OF SALEM
DEPARTMENT OF PUBLIC WORKS

STANDARD PLAN

ALLEY DETAIL
PORTLAND CEMENT CONCRETE

DRAWN BY GS

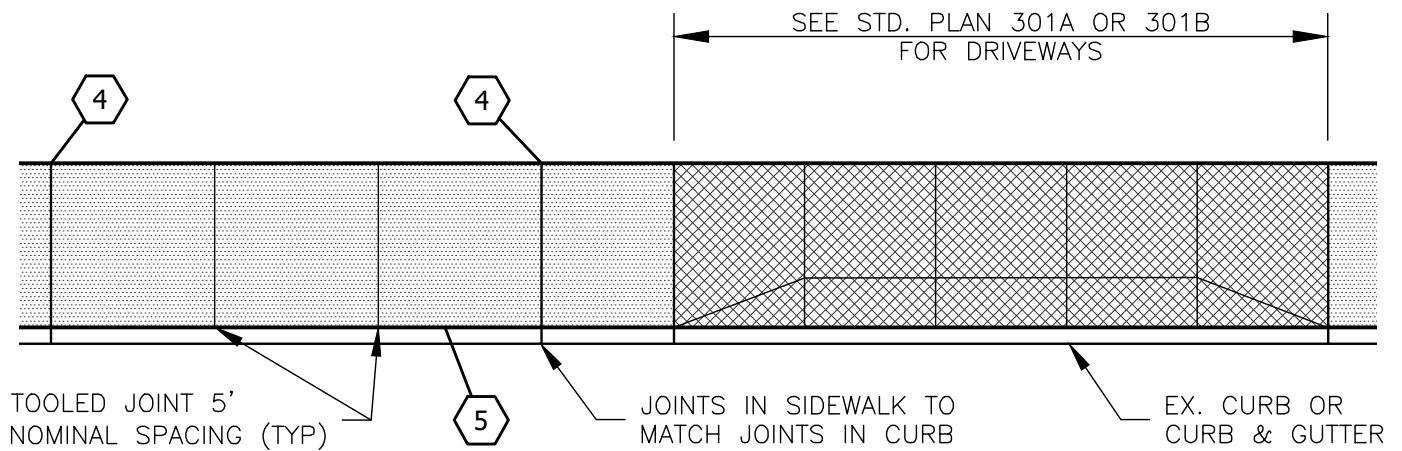
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NO.304

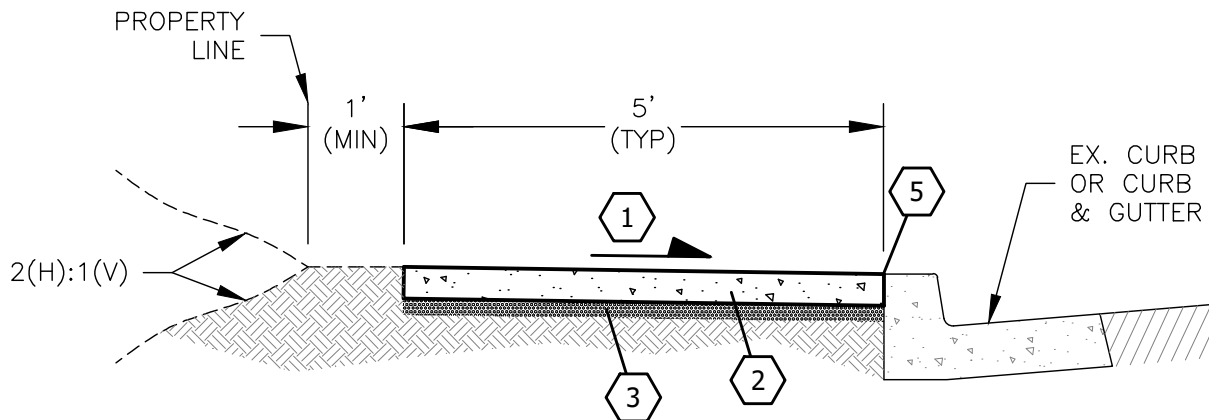
No.	Description	Date	By	Appr
	CONVERT TO CAD DWG.	1/98		
	REVISION			



	NEW PLAN	8/99	I.D.F.
No.	Description	Date	By Appr
REVISION			

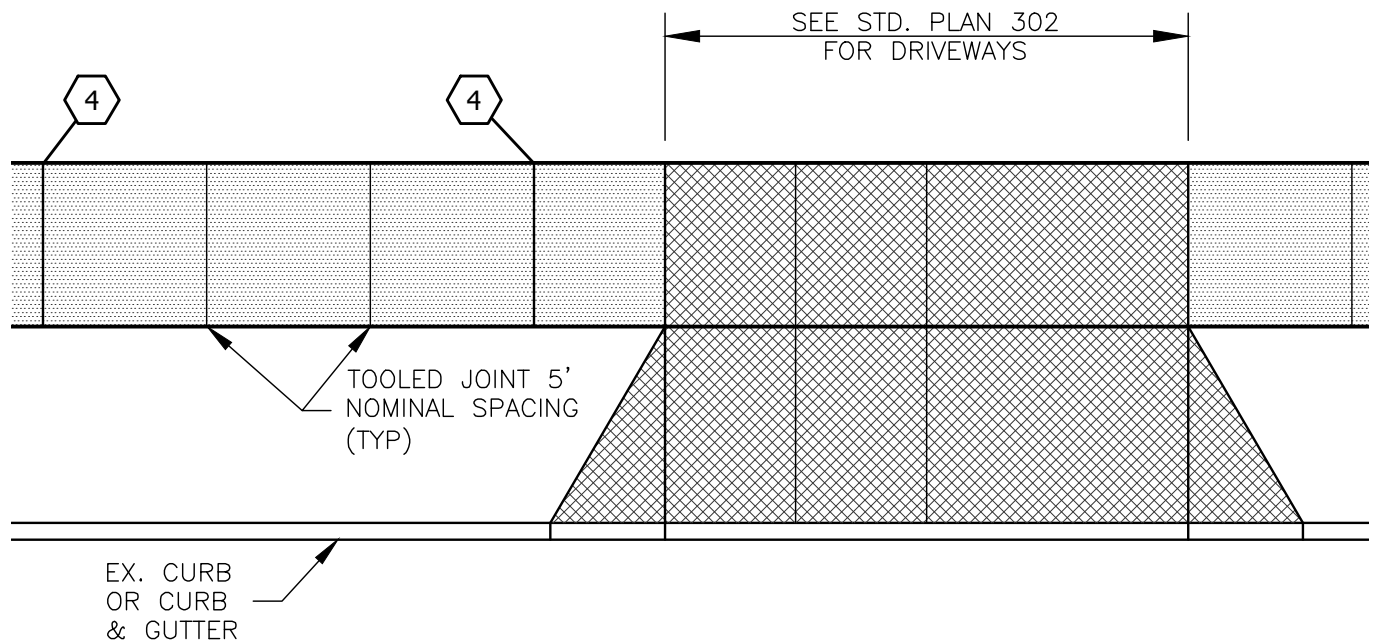


TYPICAL PLAN VIEW - CURB LINE SIDEWALK

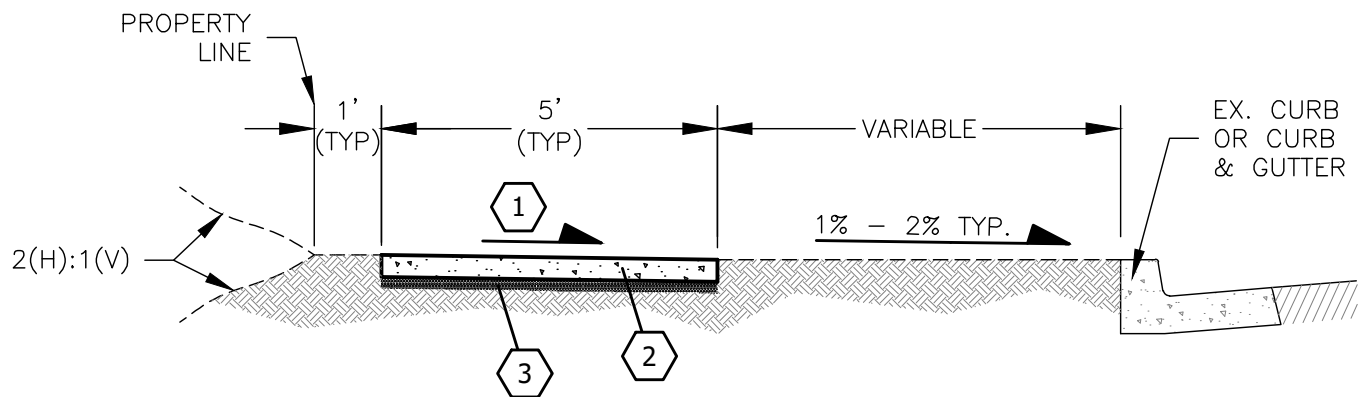


TYPICAL SECTION VIEW - CURB LINE SIDEWALK


KEYNOTES			GENERAL NOTES		
CHANGES	1	SLOPE 1.5% TYPICAL (2.0% MAXIMUM)	<ul style="list-style-type: none"> SIDEWALK WIDTH SHALL BE 5' UNLESS OTHERWISE SHOWN ON PLANS. FINISH SIDEWALK WITH LIGHT TRAVERSE BROOMING. ALL EDGES SHALL BE TOOL ROUNDED AND SHINED (3") AFTER BROOMING. SIDEWALK SHALL HAVE MIN. 4FT UNOBSTRUCTED WIDTH ADJACENT TO FIXED FEATURES SUCH AS MAILBOXES. 		
	2	4" MIN. THICK P.C.C. COMMERCIAL GRADE CONCRETE.			
	3	2" MIN. COMPACTED CRUSHED AGGREGATE.			
	4	CONSTRUCT CONTRACTION JOINTS AT 15' FT MAX SPACING.			
	5	TOOLED COLD JOINT BETWEEN CURB AND SIDEWALK.			
ALL NEW DRAWING			CITY OF SALEM DEPARTMENT OF PUBLIC WORKS		
			STANDARD PLAN CURB LINE SIDEWALK		
APPROVED	<i>[Signature]</i> CITY ENGINEER	12/27/19	DRAWN BY	JAK	10/2019
		DATE	CHECKED BY	DEW	10/2019
					NO.306A

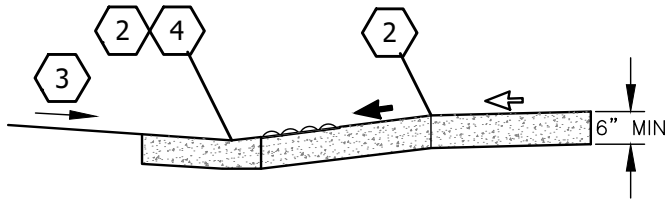
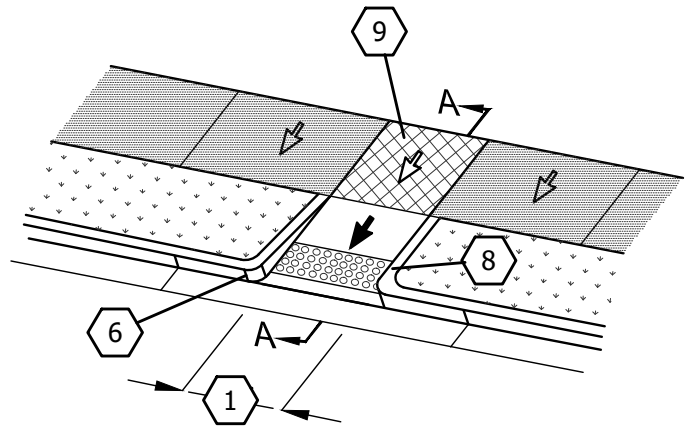
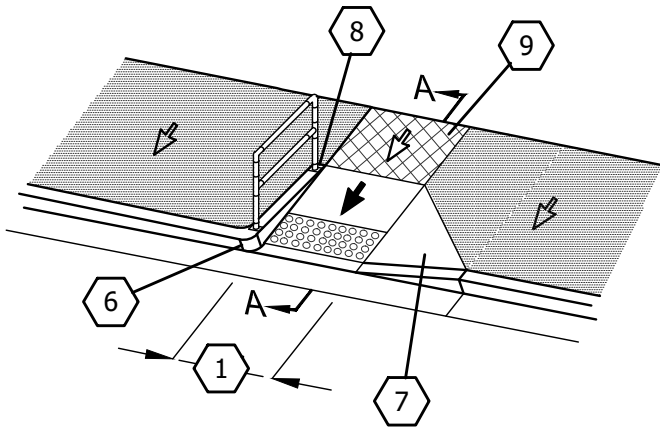


TYPICAL PLAN VIEW - PROPERTY LINE SIDEWALK



TYPICAL SECTION VIEW - PROPERTY LINE SIDEWALK

KEYNOTES				GENERAL NOTES			
<div><div>1</div>SLOPE 1.5% TYPICAL (2.0% MAXIMUM)</div> <div><div>2</div>4" MIN. THICK P.C.C. COMMERCIAL GRADE CONCRETE.</div> <div><div>3</div>2" MIN. COMPACTED CRUSHED AGGREGATE.</div> <div><div>4</div>CONSTRUCT CONTRACTION JOINTS AT 15' FT MAX SPACING.</div>				<div><div>●</div>SIDEWALK WIDTH SHALL BE 5' UNLESS OTHERWISE SHOWN ON PLANS.</div> <div><div>●</div>FINISH SIDEWALK WITH LIGHT TRAVERSE BROOMING. ALL EDGES SHALL BE TOOL ROUNDED AND SHINED (3") AFTER BROOMING.</div> <div><div>●</div>SET SIDEWALK 1FT FROM PROPERTY LINE UNLESS OTHERWISE SHOWN ON PLANS.</div>			
				<div>CITY OF SALEM</div> <div>DEPARTMENT OF PUBLIC WORKS</div>			
				<div>STANDARD PLAN</div> <div>PROPERTY LINE SIDEWALK</div>			
CHANGES	ALL NEW DRAWING			<div>NO.306B</div>			
APPROVED	<div><div></div><div>CITY ENGINEER</div></div>	12/27/19	DATE	DRAWN BY	JAK	10/2019	
				CHECKED BY	DEW	10/2019	



SECTION A-A

KEYNOTES

- 1 STANDARD RAMP WIDTH EQUALS 5- FEET. WITH PRIOR CITY APPROVAL, WIDTH MAY BE REDUCED TO 4- FEET TO AVOID OBSTRUCTIONS.
- 2 GRADE BREAKS AT TOP AND BOTTOM OF RAMP RUNS SHALL BE PERPENDICULAR TO DIRECTION OF RAMP RUN.
- 3 THE COUNTER SLOPE OF THE GUTTER OR STREET AT THE FOOT OF CURB RAMP RUNS, BLENDED TRANSITIONS, AND TURNING SPACES SHALL BE 5% MAX.
- 4 SURFACE SLOPES THAT MEET AT GRADE BREAKS SHALL BE FLUSH.
- 6 RETURNED CURBS SHALL HAVE 6-INCH MINIMUM RADIUS.
- 7 CONSTRUCT FLARED SIDES WITH SLOPE OF 10% MAX, MEASURED PARALLEL TO THE CURB LINE, WHEREVER THE SIDEWALK CROSSES THE CURB RAMP.
- 8 FLARED SIDES ARE PREFERRED, BUT RETURNED CURBS ARE PERMITTED PROVIDED THAT THEY ARE ALIGNED WITH PEDESTRIAN STREET CROSSING AND ARE PROTECTED FROM CROSS TRAVEL BY LANDSCAPING, STREET FURNITURE, RAILINGS, ETC.
- 9 A TURNING SPACE SHALL BE PROVIDED AT TOP OF PERPENDICULAR CURB RAMPS AND BOTTOM OF PARALLEL RAMPS.

LEGEND

- % ALL SLOPE MEASUREMENTS ARE RELATIVE TO TRUE HORIZONTAL.
- ↖ SLOPE = 1.5% TYPICAL (2.0% MAXIMUM)
- ← THE RUNNING SLOPE OF THE CURB RAMP SHALL BE 7.5% TYPICAL (8.3% MAXIMUM), BUT SHALL NOT REQUIRE THE CURB RAMP LENGTH TO EXCEED 15- FEET. (SEE 307.D FOR SLOPES LESS THAN 5%.)
- SEE 307.E FOR DETECTABLE WARNING SURFACE REQUIREMENTS.
- THE TURNING SPACE SHALL BE 48"x48" MINIMUM. IF CONSTRAINED, IT SHALL BE 48"x60" MINIMUM WITH LONGER DIMENSION IN DIRECTION OF PEDESTRIAN STREET CROSSING.

GENERAL NOTES

- GRADE BREAKS ARE NOT PERMITTED ON SURFACE OF RAMP RUNS AND TURNING SPACES.
- ALL RAMPS AND TURNING SPACES SHALL BE MINIMUM 6-INCH THICK COMMERCIAL GRADE CONCRETE.

CHANGES

REVISED KEYNOTE 7
REVISED TURNING SPACE DIMENSIONS
REVISED LEGEND SLOPE ARROW DESCRIPTIONS

CITY OF SALEM DEPARTMENT OF PUBLIC WORKS

STANDARD PLAN

CURB RAMPS (PERPENDICULAR)

APPROVED

[Signature]
CITY ENGINEER

12/27/19

DATE

DRAWN BY

JAK

10/2019

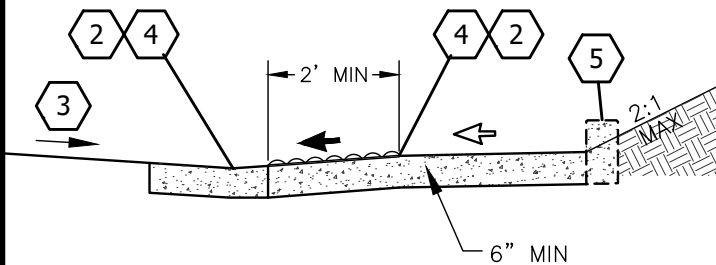
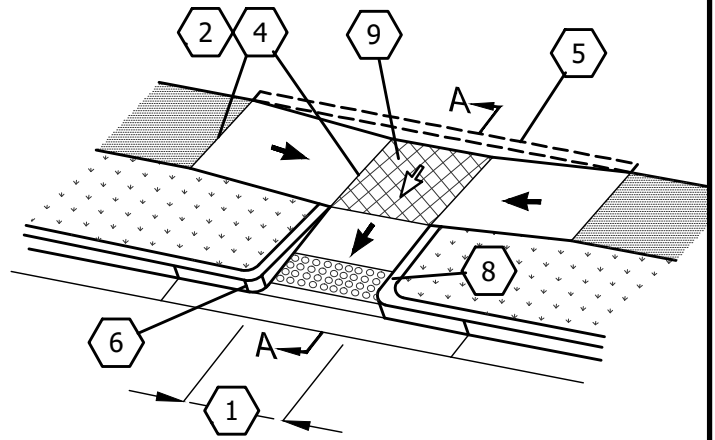
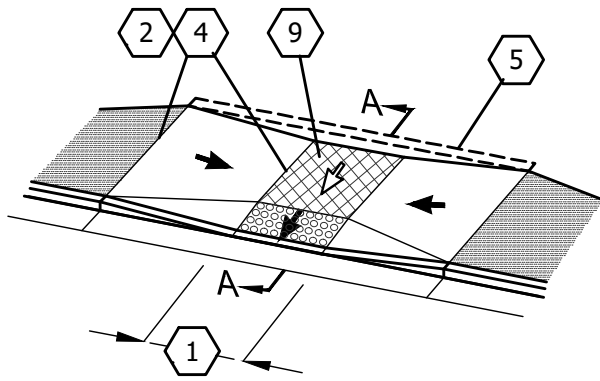
CHECKED BY

DEW

10/2019

NO.307A

<p style="text-align: center;">SECTION B-B</p>	<p style="text-align: center;">SECTION A-A</p>
<p style="text-align: center;">KEYNOTES</p>	<p style="text-align: center;">LEGEND</p>
<div style="display: flex;"> <div style="flex: 1;"> <p>1 STANDARD RAMP WIDTH EQUALS 5- FEET. WITH PRIOR CITY APPROVAL, WIDTH MAY BE REDUCED TO 4- FEET TO AVOID OBSTRUCTIONS.</p> <p>2 GRADE BREAKS AT TOP AND BOTTOM OF RAMP RUNS SHALL BE PERPENDICULAR TO DIRECTION OF RAMP RUN.</p> <p>3 THE COUNTER SLOPE OF THE GUTTER OR STREET AT THE FOOT OF CURB RAMP RUNS, BLENDED TRANSITIONS, AND TURNING SPACES SHALL BE 5% MAX.</p> <p>4 SURFACE SLOPES THAT MEET AT GRADE BREAKS SHALL BE FLUSH.</p> <p>5 LANDSCAPE CURB IS REQUIRED IF DRAWN WITH SOLID LINE, OPTIONAL IF DASHED LINE. VERIFY TURNING SPACE IS 60" MINIMUM IF BUILT.</p> <p>9 A TURNING SPACE SHALL BE PROVIDED AT TOP OF PERPENDICULAR CURB RAMP AND BOTTOM OF PARALLEL RAMP.</p> </div> </div>	<div style="display: flex;"> <div style="flex: 1;"> <p>% ALL SLOPE MEASUREMENTS ARE RELATIVE TO TRUE HORIZONTAL.</p> <p>↖ SLOPE = 1.5% TYPICAL (2.0% MAXIMUM)</p> <p>← THE RUNNING SLOPE OF THE CURB RAMP SHALL BE 7.5% TYPICAL (8.3% MAXIMUM), BUT SHALL NOT REQUIRE THE CURB RAMP LENGTH TO EXCEED 15- FEET. WHEN ON RADIUS MEASURE 15FT MIN. AT BACK OF SIDEWALK. (SEE 307.D FOR SLOPES LESS THAN 5%.)</p> <p> SEE 307.E FOR DETECTABLE WARNING SURFACE REQUIREMENTS.</p> <p> THE TURNING SPACE SHALL BE 48"x48" MINIMUM. IF CONSTRAINED, IT SHALL BE 48"x60" MINIMUM WITH LONGER DIMENSION IN DIRECTION OF PEDESTRIAN STREET CROSSING.</p> </div> <div style="flex: 1; font-size: small;"> <p>CHANGES</p> <p>CLARIFIED 15FT MIN. MEASUREMENT</p> <p>REVISED LEGEND SLOPE ARROW DESCRIPTIONS</p> <p>REVISED TURNING SPACE DIMENSIONS</p> </div> </div>
<p style="text-align: center;">GENERAL NOTES</p>	
<ul style="list-style-type: none"> GRADE BREAKS ARE NOT PERMITTED ON SURFACE OF RAMP RUNS AND TURNING SPACES. ALL RAMPS AND TURNING SPACES SHALL BE MINIMUM 6-INCH THICK COMMERCIAL GRADE CONCRETE. 	<div style="text-align: center;"> <p>CITY OF SALEM</p> <p>DEPARTMENT OF PUBLIC WORKS</p> <p>STANDARD PLAN</p> <p>CURB RAMPS (PARALLEL)</p> </div>
<div style="display: flex; justify-content: space-between;"> <div style="display: flex; align-items: center;"> <p>APPROVED</p> </div> <div style="text-align: center;"> <p>CITY ENGINEER</p> </div> </div>	<div style="display: flex; justify-content: space-between;"> <div style="display: flex; align-items: center;"> <p>12/27/19</p> <p>DATE</p> </div> <div style="display: flex; align-items: center;"> <p>DRAWN BY</p> <p>JAK</p> <p>10/2019</p> </div> <div style="display: flex; align-items: center;"> <p>CHECKED BY</p> <p>DEW</p> <p>10/2019</p> </div> </div> <div style="text-align: right; font-size: 2em; font-weight: bold; margin-top: 20px;"> NO.307B </div>



SECTION A-A

KEYNOTES

- 1 STANDARD RAMP WIDTH EQUALS 5- FEET. WITH PRIOR CITY APPROVAL, WIDTH MAY BE REDUCED TO 4- FEET TO AVOID OBSTRUCTIONS.
- 2 GRADE BREAKS AT TOP AND BOTTOM OF RAMP RUNS SHALL BE PERPENDICULAR TO DIRECTION OF RAMP RUN.
- 3 THE COUNTER SLOPE OF THE GUTTER OR STREET AT THE FOOT OF CURB RAMP RUNS, BLENDED TRANSITIONS, AND TURNING SPACES SHALL BE 5% MAX.
- 4 SURFACE SLOPES THAT MEET AT GRADE BREAKS SHALL BE FLUSH.
- 5 LANDSCAPE CURB IS REQUIRED IF DRAWN WITH SOLID LINE, OPTIONAL IF DASHED LINE. VERIFY TURNING SPACE IS 60" MINIMUM IF BUILT.
- 6 RETURNED CURBS SHALL HAVE 6-INCH MINIMUM RADIUS.
- 8 FLARED SIDES ARE PREFERRED, BUT RETURNED CURBS ARE PERMITTED PROVIDED THAT THEY ARE ALIGNED WITH PEDESTRIAN STREET CROSSING AND ARE PROTECTED FROM CROSS TRAVEL BY LANDSCAPING, STREET FURNITURE, RAILINGS, ETC.
- 9 A TURNING SPACE SHALL BE PROVIDED AT TOP OF PERPENDICULAR CURB RAMPS AND BOTTOM OF PARALLEL RAMPS.

LEGEND

- % ALL SLOPE MEASUREMENTS ARE RELATIVE TO TRUE HORIZONTAL.
- ↖ SLOPE = 1.5% TYPICAL (2.0% MAXIMUM)
- ↙ THE RUNNING SLOPE OF THE CURB RAMP SHALL BE 7.5% TYPICAL (8.3% MAXIMUM), BUT SHALL NOT REQUIRE THE CURB RAMP LENGTH TO EXCEED 15- FEET. WHEN ON RADIUS MEASURE 15FT MIN. AT BACK OF SIDEWALK. (SEE 307.D FOR SLOPES LESS THAN 5%.)
- SEE 307.E FOR DETECTABLE WARNING SURFACE REQUIREMENTS.
- THE TURNING SPACE SHALL BE 48"x48" MINIMUM. IF CONSTRAINED, IT SHALL BE 48"x60" MINIMUM WITH LONGER DIMENSION IN DIRECTION OF PEDESTRIAN STREET CROSSING.

GENERAL NOTES

- GRADE BREAKS ARE NOT PERMITTED ON SURFACE OF RAMP RUNS AND TURNING SPACES.
- ALL RAMPS AND TURNING SPACES SHALL BE MINIMUM 6-INCH THICK COMMERCIAL GRADE CONCRETE.

CHANGES

CLARIFIED 15FT MIN. MEASUREMENT
REVISED LEGEND SLOPE ARROW DESCRIPTIONS
REVISED TURNING SPACE DIMENSIONS

CITY OF SALEM DEPARTMENT OF PUBLIC WORKS

STANDARD PLAN CURB RAMPS (COMBINATION)

APPROVED

CITY ENGINEER

12/27/19

DATE

DRAWN BY

JAK

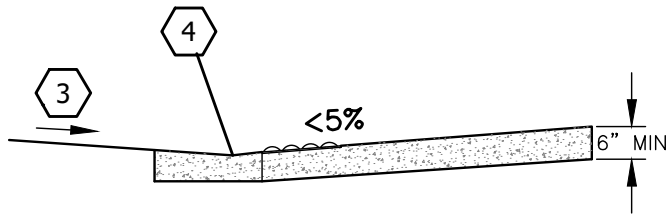
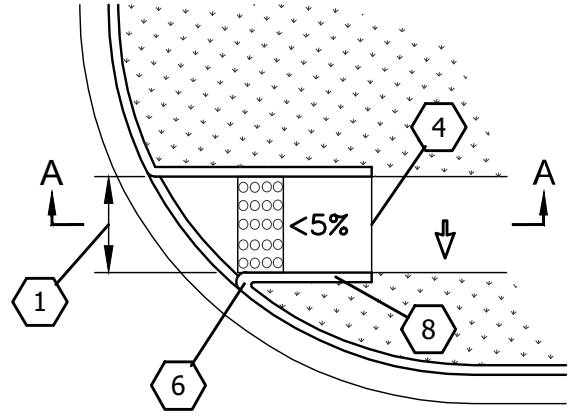
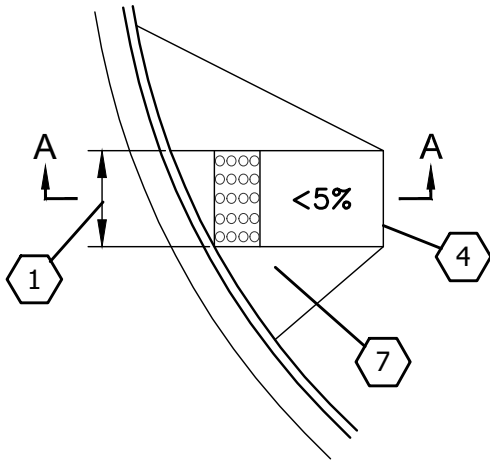
10/2019

CHECKED BY

DEW

10/2019

NO.307C



SECTION A-A

KEYNOTES

- 1 STANDARD RAMP WIDTH EQUALS 5- FEET. WITH PRIOR CITY APPROVAL, WIDTH MAY BE REDUCED TO 4- FEET TO AVOID OBSTRUCTIONS.
- 3 THE COUNTER SLOPE OF THE GUTTER OR STREET AT THE FOOT OF CURB RAMP RUNS, BLENDED TRANSITIONS, AND TURNING SPACES SHALL BE 5% MAX.
- 4 SURFACE SLOPES THAT MEET AT GRADE BREAKS SHALL BE FLUSH.
- 6 RETURNED CURBS SHALL HAVE 6-INCH MINIMUM RADIUS.
- 7 CONSTRUCT FLARED SIDES WITH SLOPE OF 10% MAX, MEASURED PARALLEL TO CURB, WHEREVER THE SIDEWALK CROSSES THE CURB RAMP.
- 8 FLARED SIDES ARE PREFERRED, BUT RETURNED CURBS ARE PERMITTED PROVIDED THAT THEY ARE ALIGNED WITH PEDESTRIAN STREET CROSSING AND ARE PROTECTED FROM CROSS TRAVEL BY LANDSCAPING, STREET FURNITURE, RAILINGS, ETC.

LEGEND

- % ALL SLOPE MEASUREMENTS ARE RELATIVE TO TRUE HORIZONTAL.
- ↖ SLOPE = 1.5% TYPICAL (2.0% MAXIMUM)
- <5% THE RUNNING SLOPE OF A BLENDED TRANSITION SHALL BE LESS THAN 5%.
- SEE 307.E FOR DETECTABLE WARNING SURFACE REQUIREMENTS.

GENERAL NOTES

- PERPENDICULAR GRADE BREAKS AND TURNING SPACES ARE NOT REQUIRED WHEN RUNNING SLOPES ARE LESS THAN 5%.
- THIS DRAWING DEPICTS RUNNING SLOPE ALIGNMENTS CROSSING GUTTER GRADE BREAK ON A SKEWED ANGLE, A CONFIGURATION NOT PERMITTED WHEN RUNNING SLOPES EXCEED 5%.
- ALL RAMPS AND TURNING SPACES SHALL BE MINIMUM 6-INCH THICK COMMERCIAL GRADE CONCRETE.

CITY OF SALEM DEPARTMENT OF PUBLIC WORKS

STANDARD PLAN BLENDED TRANSITIONS

CHANGES REVISED LEGEND SLOPE ARROW DESCRIPTIONS

APPROVED

[Signature]
CITY ENGINEER

12/27/19

DATE

DRAWN BY

JAK

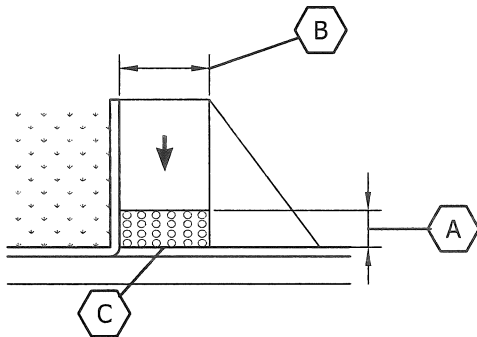
10/2019

CHECKED BY

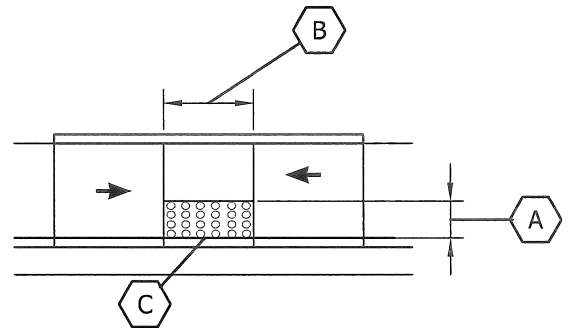
DEW

10/2019

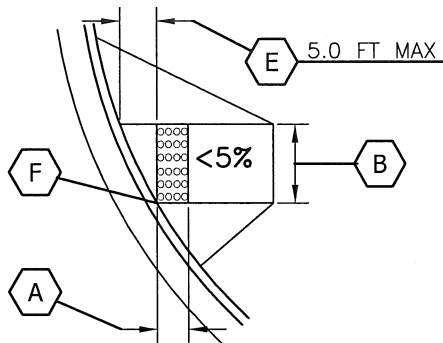
NO.307D



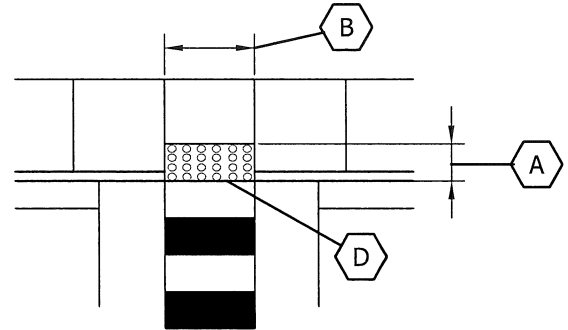
PERPENDICULAR RAMP



PARALLEL RAMP



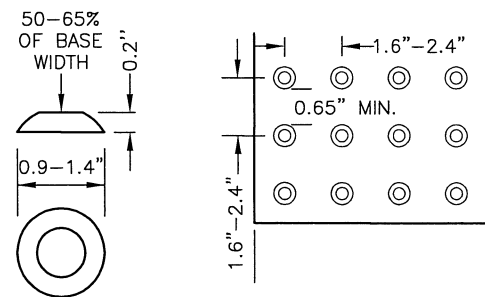
BLENDED TRANSITION



RAISED PEDESTRIAN CROSSING

KEYNOTES

- A** DWS SHALL EXTEND 2.0 FT MIN IN THE DIRECTION OF TRAVEL.
- B** AT CURB RAMPS AND BLENDED TRANSITIONS, DWS SHALL EXTEND THE FULL WIDTH OF THE RAMP RUN (EXCLUDING ANY FLARED SIDES), BLENDED TRANSITION, OR TURNING SPACE.
- C** ON PERPENDICULAR AND PARALLEL CURB RAMPS, PLACE DWS 2" MAX FROM THE BACK OF CURB.
- D** AT RAISED PEDESTRIAN STREET CROSSINGS, DEPRESSED CORNERS, OR AT OTHER LEVEL PEDESTRIAN STREET CROSSINGS, PLACE DWS 2" MAX FROM THE FLUSH TRANSITION BETWEEN THE STREET AND THE SIDEWALK.
- E** IF DIMENSION SHOWN MUST EXCEED 5.0 FT IN ORDER TO KEEP ROWS OF DOMES ALIGNED WITH PATH OF TRAVEL, THEN PLACE DWS PARALLEL TO, AND 2" MAX FROM, THE BACK OF CURB.
- F** AT BLENDED TRANSITIONS, PLACE DWS 2" MAX FROM THE BACK OF CURB.




GENERAL NOTES

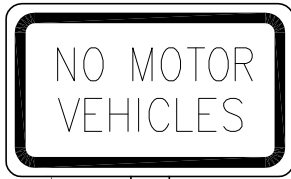
- DETECTABLE WARNING SURFACES (DWS), SHALL CONSIST OF TRUNCATED DOMES ALIGNED IN A SQUARE OR RADIAL GRID PATTERN.
- ALIGN ROWS OF DOMES WITH INTENDED PATH OF TRAVEL.
- COLOR: BRICK RED (FED STD #20109)
- APPROVED PRODUCTS:
 - 1) MASCO CASTinTACT®
 - 2) ADA SOLUTIONS, INC.
 - 3) OR EQUAL, AS PER CITY ENGINEER

CITY OF SALEM DEPARTMENT OF PUBLIC WORKS

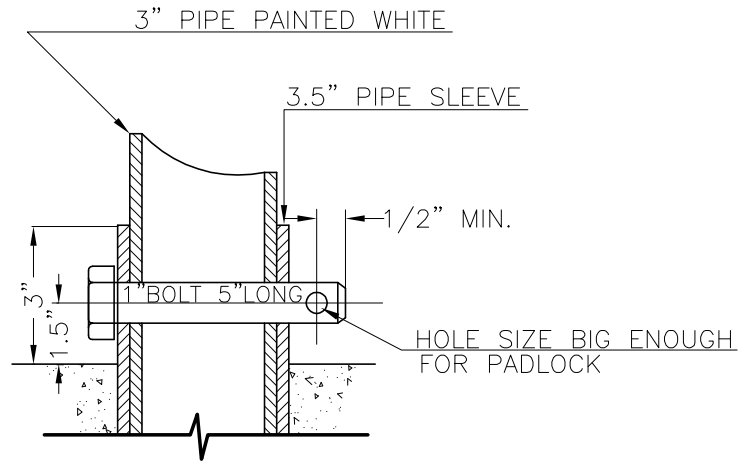
STANDARD PLAN DETECTABLE WARNING SURFACE

APPROVED		5/2015	DRAWN BY	JAK	7/2012	NO.307.E
	CITY ENGINEER	DATE	CHECKED BY	DEW	7/2012	

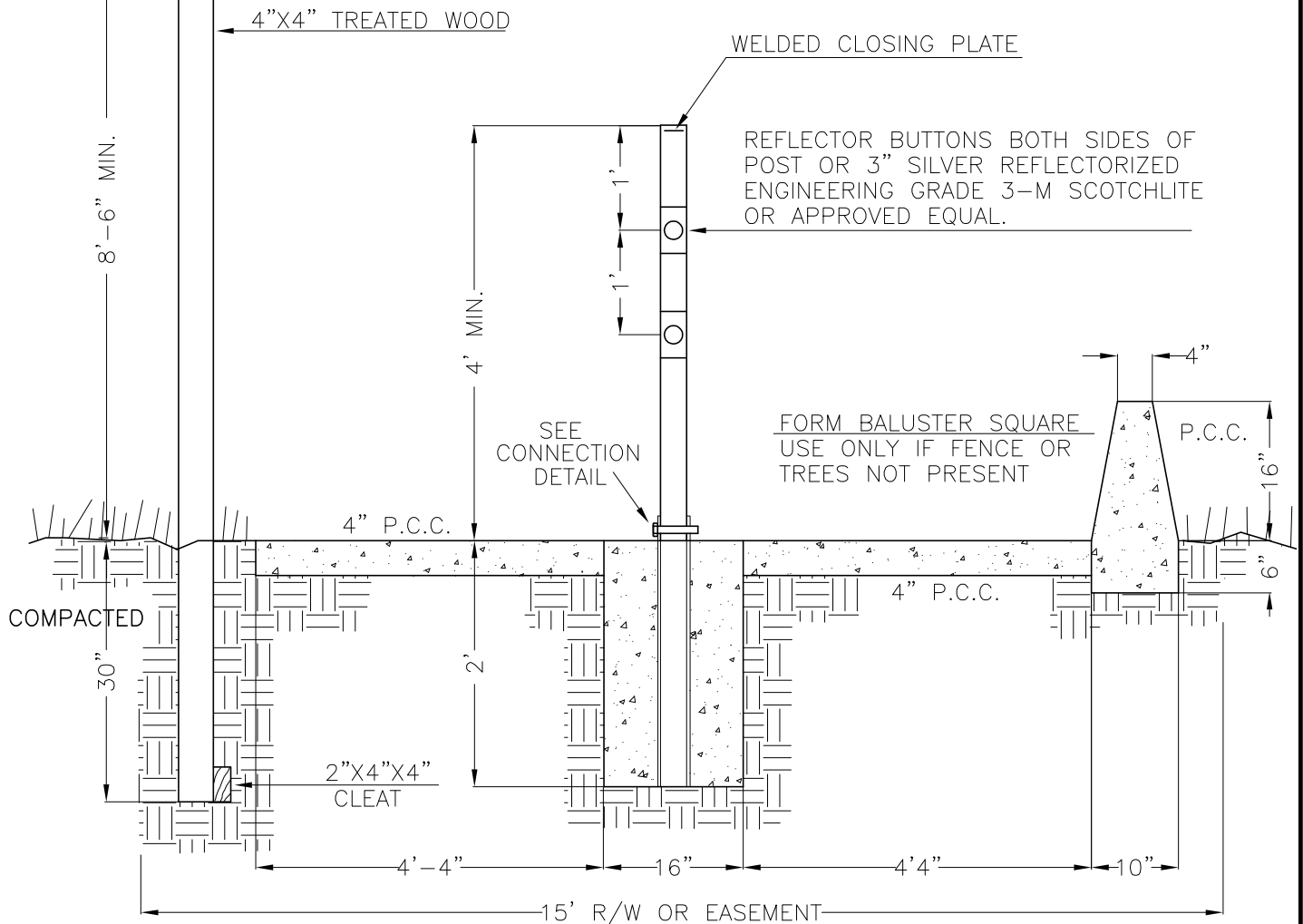
BLACK ON WHITE
REFLECTORIZED ENGINEERING
GRADE 3-M SCOTCHLITE OR
APPROVED EQUAL.



OBR-10-11-18
30"X18"



REFLECTOR POST CONNECTION DETAIL



Approved Karl O. Guter 9-15-99
City Engineer Date

	CONVERT TO CAD DWG.			
No.	Description	Date	By	Appr
	REVISION			

CITY OF SALEM
DEPARTMENT OF PUBLIC WORKS

STANDARD PLAN
MID-BLOCK WALK ENTRANCE
TRAFFIC CONTROL

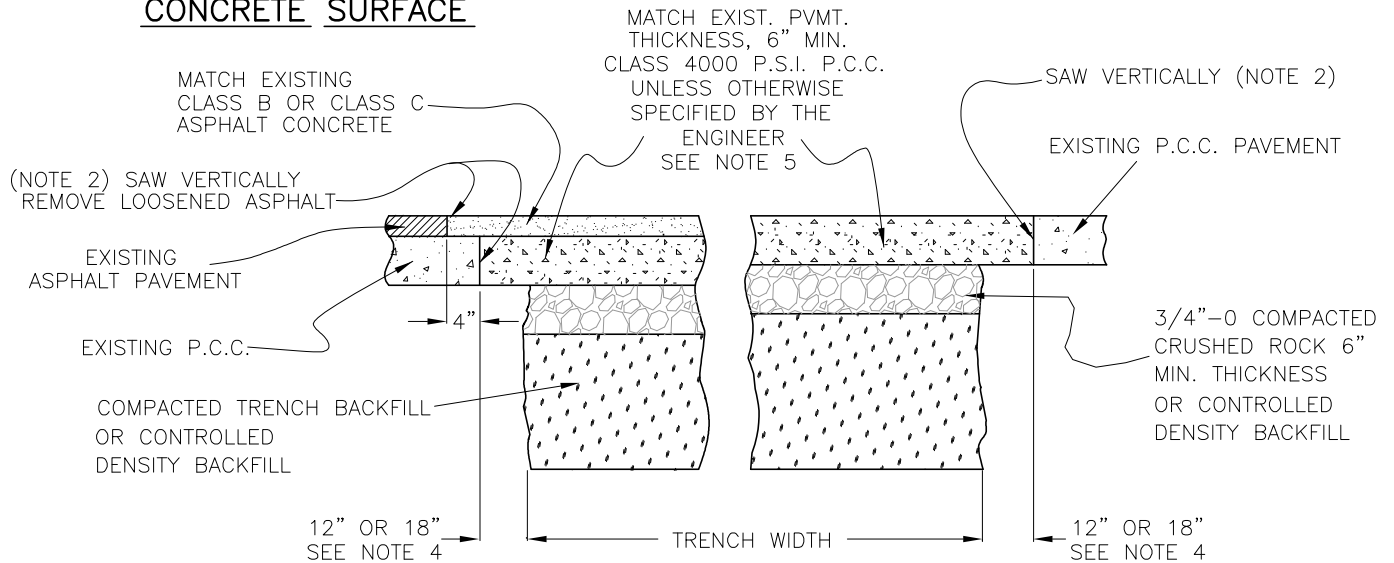
DRAWN BY GS

CHECKED BY RWL

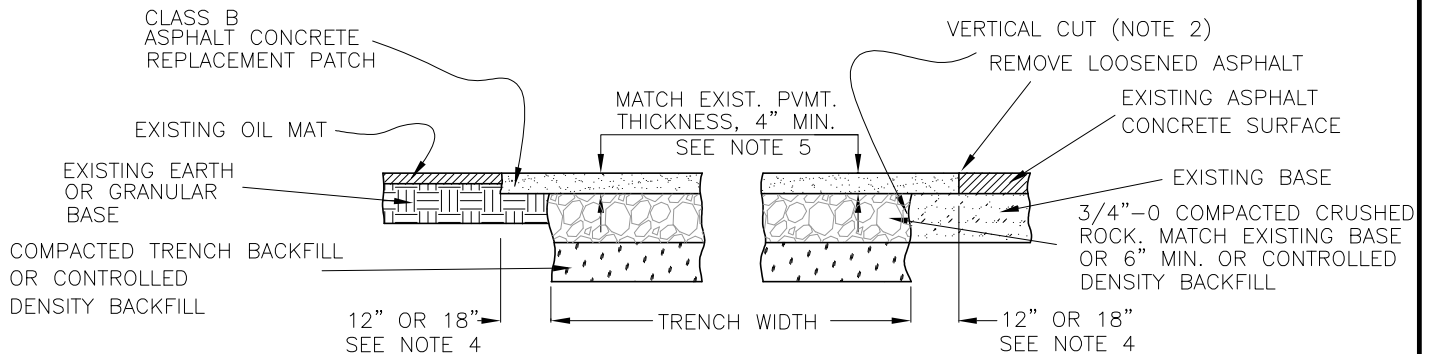
NO.308

HALF SECTION RIGID PAVEMENT WITH ASPHALT CONCRETE SURFACE

HALF SECTION P.C.C. PAVEMENT



TYPICAL PATCH FOR RIGID PAVEMENT



TYPICAL PATCH FOR FLEXIBLE PAVEMENT

NOTES

1. FINAL CUTS IN A.C. PAVEMENT SHALL BE MADE WITH A CONCRETE SAW, MORE THAN ONE CUT MAY BE REQUIRED IN AC PAVEMENTS.
2. CUTS IN P.C.C. PAVEMENT SHALL BE MADE WITH A CONCRETE SAW.
3. 1"-0 CRUSHED AGGREGATE MAY BE SUBSTITUTED FOR 3/4"-0.
4. PAVEMENT REPLACEMENT WIDTH SHALL BE: TRENCH WIDTH PLUS 12 INCHES ON EACH SIDE FOR CONTROLLED DENSITY BACKFILL, AND TRENCH WIDTH PLUS 18 INCHES ON EACH SIDE FOR ROCK BACKFILL.
5. PAVEMENT REPLACEMENT THICKNESS SHALL BE AS SHOWN ABOVE FOR ROCK TRENCH BACKFILL. FOR CONTROLLED DENSITY TRENCH BACKFILL, PAVEMENT REPLACEMENT THICKNESS SHALL BE AS FOLLOWS:

STREET CLASSIFICATION	ARTERIAL	COLLECTOR	LOCAL
PORTLAND CEMENT CONCRETE	8"P.C.C.	7"P.C.C.	6"P.C.C.
A.C. OVER P.C.C.	4"A.C.	4"A.C.	4"A.C.
ASPHALT CONCRETE	4"A.C.	4"A.C.	4"A.C.

Approved Karl O. Guter 9-15-99
City Engineer Date

5	REVISED A.C. CUT NOTE.	4/99	JHC	
4	CONVERT TO CAD DWG.			
3	REVISE FOR C.D.B.	12-94		
2	NOTES	11/81	SDD	RLW
1	REVISED TITLE BLOCK	3/79	DLM	
No.	Description	Date	By	Appr

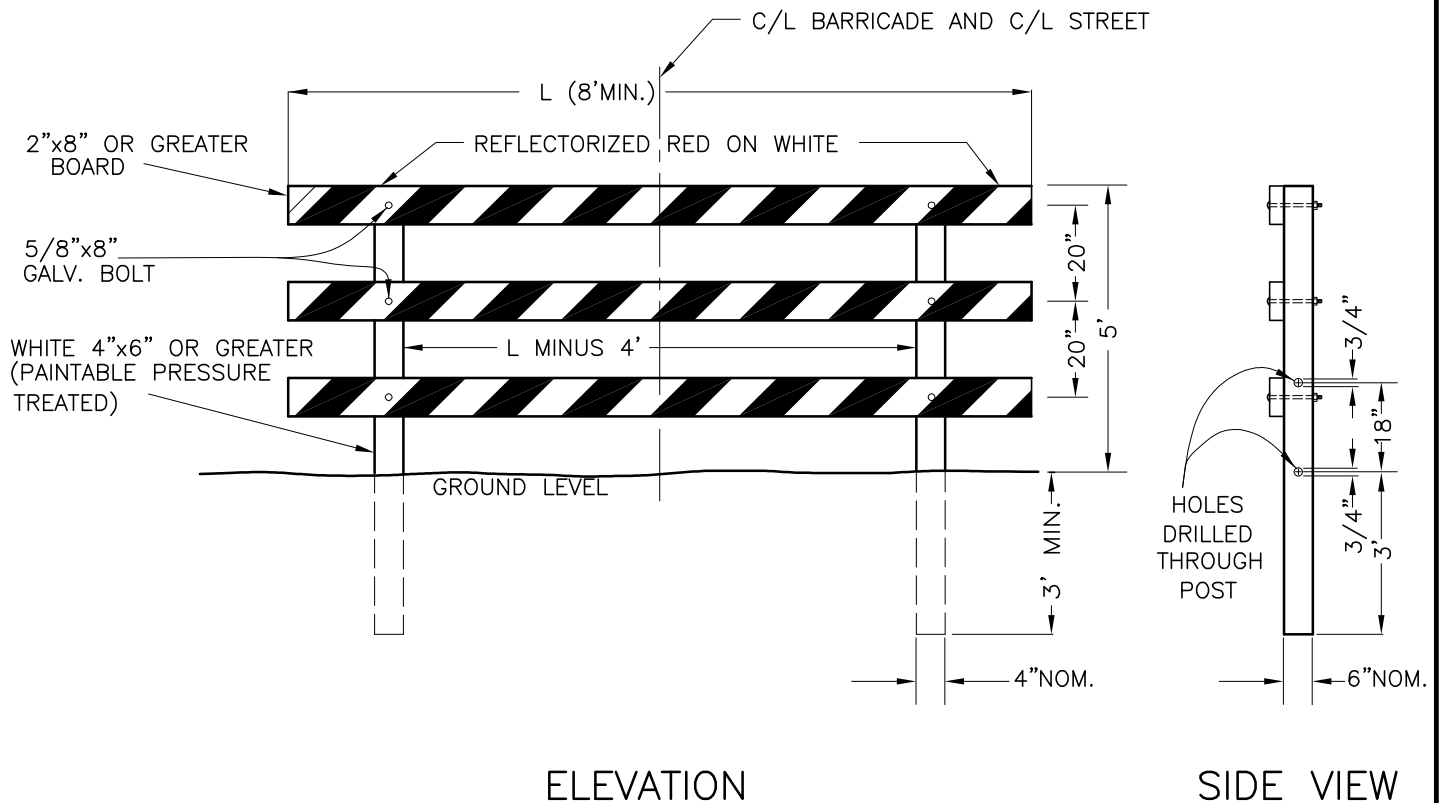
REVISION
CITY OF SALEM
DEPARTMENT OF PUBLIC WORKS

STANDARD PLAN
PAVEMENT PATCHING

DRAWN BY GS

CHECKED BY RWL

NO.309



NOTES:

- REFLECTORIZED SHEETING SHALL BE ENGINEERING GRADE REFLECTIVE SHEETING CONFORMING TO 1991 EDITION OF THE OREGON STATE HIGHWAY DIVISIONS "STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION SECTION 02910"
- THE LENGTH OF THE BARRICADE UNIT "L" AND NUMBER AND POSITIONING OF UNITS WILL BE SHOWN ON PROJECT PLANS.

Approved *Karl O. Guster*
City Engineer

9-15-99
Date

No.	Description	Date	By	Appr
	CONVERT TO CAD DWG.			
	GENERAL	5-97	G.S.	KDG
	BARRICADE LENGTH	9-93	G.S.	KDG
	GENERAL	4-93	G.S.	KDG
REVISION				

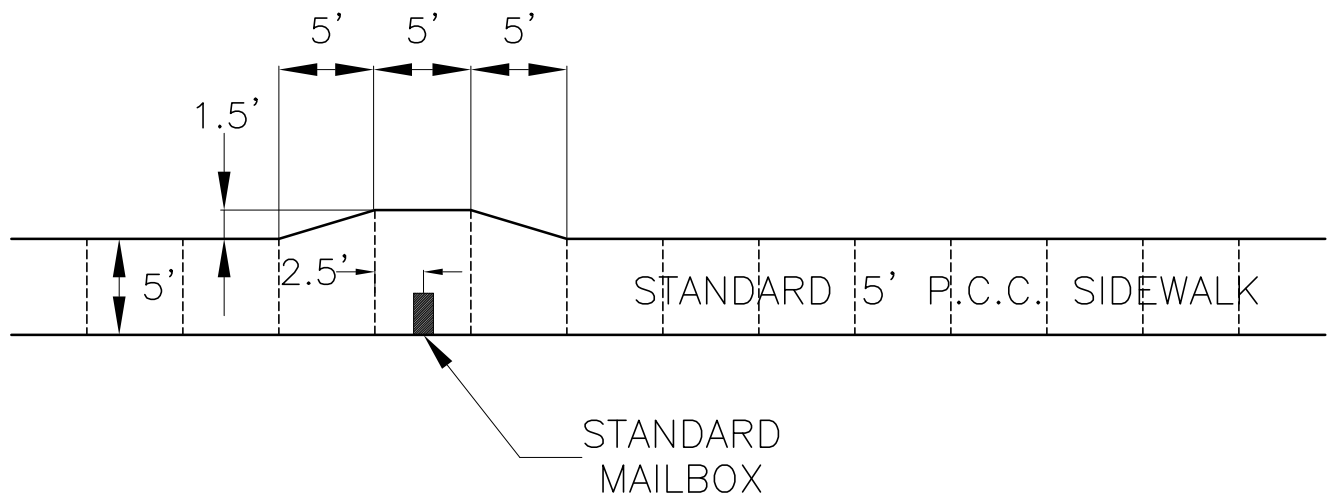
CITY OF SALEM
DEPARTMENT OF PUBLIC WORKS

STANDARD PLAN
PERMANENT BARRICADE

DRAWN BY GS

CHECKED BY RWL

NO.310



NOTE

1. SEE STD. PLAN 306 FOR STANDARD SIDEWALK DETAILS.
2. CONSTRUCT WIDENED SIDEWALK AT ALL MAILBOX LOCATIONS.
3. MAILBOX PLACEMENT SPECIFICATIONS:
 - FRONT FACE OF MAILBOX TO BE SET BACK 6" FROM FACE OF CURB.
 - BASE OF MAILBOX TO BE BETWEEN 38" AND 40" ABOVE CURB, GUTTER OR PAVEMENT GRADE.

Approved

Karl O. Guter
City Engineer

9-15-99
Date

CITY OF SALEM
DEPARTMENT OF PUBLIC WORKS

STANDARD PLAN

WIDENED SIDEWALK AT
MAILBOX LOCATIONS

1.	NEW DRAWING	9/99	JHC	KG
No.	Description	Date	By	Appr
REVISION				

DRAWN BY JHC

CHECKED BY GM

NO.311



NOTE:

BLACK LEGEND ON REFLECTORIZED WHITE (ENGINEERING GRADE) BACKGROUND ON ALUMINUM SHEETING (80 GAUGE). SIGN SHALL BE MOUNTED FLUSH WITH TOP RAIL OF TYPE III BARRICADE AT THE TERMINUS OF STUBBED STREETS.

Approved Karl O. Louder
City Engineer

9-15-99
Date

CITY OF SALEM
DEPARTMENT OF PUBLIC WORKS

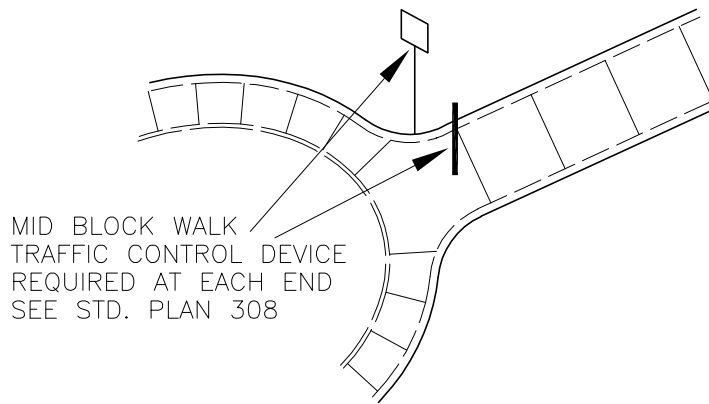
STANDARD PLAN
FUTURE ST. EXTENSION SIGN

DRAWN BY GS

CHECKED BY RWL

NO.313

No.	Description	Date	By	Appr
	PHONE NUMBER CHANGE			
	CONVERT TO CAD DWG.			
REVISION				



9-15-99
Date

CITY OF SALEM
DEPARTMENT OF PUBLIC WORKS

STANDARD PLAN MID BLOCK BIKE/PEDESTRIAN WALKWAY

DRAWN BY GS

CHECKED BY RWL

NO.314

	CONVERT TO CAD DWG.			
No.	Description	Date	By	Appr
REVISION				

Diagram illustrating the cross-section of a driveway apron. The apron is shown as a concrete slab (6" MIN. THICK CONCRETE DRIVEWAY APRON (UNLESS OTHERWISE SHOWN ON PROJECT PLAN)) resting on a layer of crushed rock (3" OF 1"-0" CRUSHED ROCK). The apron is sloped at 2% (2% SLOPE). The diagram also shows a cross-section of the driveway surface with a 2% slope indicated by an arrow and the text "2% SLOPE".

EXISTING CURB/GUTTER

COLD JOINTS

RADIUS AS SHOWN ON PROJECT PLANS

FOR SIDEWALK RAMP
DETAIL SEE STD. PLAN 307

R/W

3" OF 1"-0" CRUSHED ROCK

SE

A

A

W = WIDTH REQUIRED IN THE DEVELOPMENT AS APPROVED.

B

B

CURB EXPOSURE VARYING ON THE CONFIGURATION OF DRIVEWAY BEHIND THE CURB

TYPE C CURB

6" CURB EXPOSURE AT THIS POINT

FOR CURB & GUTTER DETAIL SEE STD. PLAN 303

NOTES:

1. THIS TYPE OF CURB IS NOT TO BE USED FOR HIGH VOLUME TRAFFIC OR LARGE TRUCKS WITHOUT APPROVAL OF THE ENGINEER.
2. ALL CONCRETE SHALL BE 28-DAY COMPRESSIVE STRENGTH OF 4000 P.S.I.

2% SLOPE

1. THIS TYPE OF APPROACH TO BE USED FOR HIGH VOLUME TRAFFIC GENERATORS, OR LARGE TRUCK TRAFFIC WITH PRIOR APPROVAL OF THE CITY ENGINEER.
2. ALL CONCRETE TO HAVE A MINIMUM 28-DAY COMPRESSIVE STRENGTH OF 4000 P.S.I.

Diagram illustrating a cross-section of a driveway apron. The apron is shown as a concrete slab (6" MIN. THICK CONCRETE DRIVEWAY APRON) resting on a layer of crushed rock (3" OF 1"-0" CRUSHED ROCK). The apron is sloped at 2% on both sides, indicated by arrows and the text "2% SLOPE". The diagram also shows a curb on the right side of the apron.

Approved Karl O. Guntz 9-15-99
City Engineer Date

	City Engineer		Date	
	CONVERT TO CAD DWG.			
No.	Description	Date	By	Appr
REVISION				

CITY OF SALEM
DEPARTMENT OF PUBLIC WORKS

STANDARD PLAN

COMMERCIAL CURB RETURNS FOR DRIVEWAY AND ALLEY APPROACH

DRAWN BY GS

CHECKED BY RWL

NO.315



2. THIS IS A TYPICAL DESIGN.
DIMENSIONS SHALL STAY THE
SAME BUT THE CONFIGURATION
CAN VARY.



Approved

Karl O. Gouzer
City Engineer

9-15-99
Date

CITY OF SALEM
DEPARTMENT OF PUBLIC WORKS

STANDARD PLAN

HAMMER-HEAD TURNAROUND

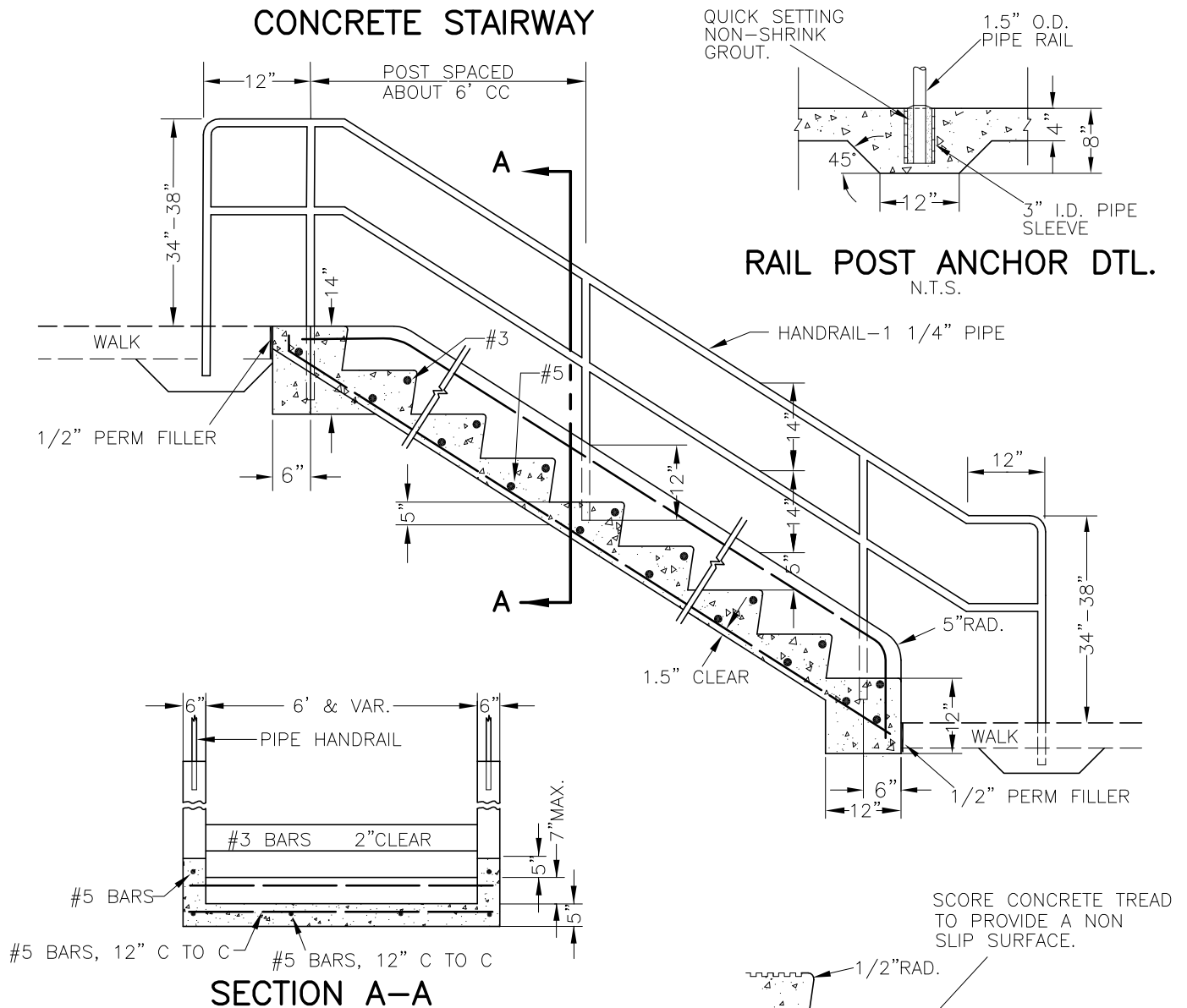
DRAWN BY GS

CHECKED BY RWL

NO.316

	CONVERT TO CAD DWG.			
No.	Description	Date	By	App
REVISION				

CONCRETE STAIRWAY



NOTES:

1. OPEN AND SIDES OF STAIRWAYS, LANDINGS AND RAMPS WHICH ARE MORE THAN 30" ABOVE GRADE SHALL BE PROTECTED BY GUARDRAILS PER UNIFORM BUILDING CODE REQUIREMENTS.
2. THE TOP OF HANDRAILS SHALL NOT BE LESS THAN 34" IN HEIGHT.
3. THE HAND RAIL PORTION OF HANDRAILS SHALL NOT BE LESS THAN 1.25" OR MORE THAN 2" IN DIAMETER, CODE ALL PARTS TO FIT. WELD AND GRIND SMOOTH, HOT GALV AFTER FABRICATION.
4. MAXIMUM VERTICAL RISE BETWEEN LANDINGS WILL BE 12 FEET. NUMBER OF STEPS VARIES. ROUND EDGES OF STEPS AND ALL OTHER EXPOSED EDGES TO 1/2" RAD..

DETAILS OF TREADS

Approved *Karl O. Guter* 9-15-99
City Engineer Date

CITY OF SALEM
DEPARTMENT OF PUBLIC WORKS

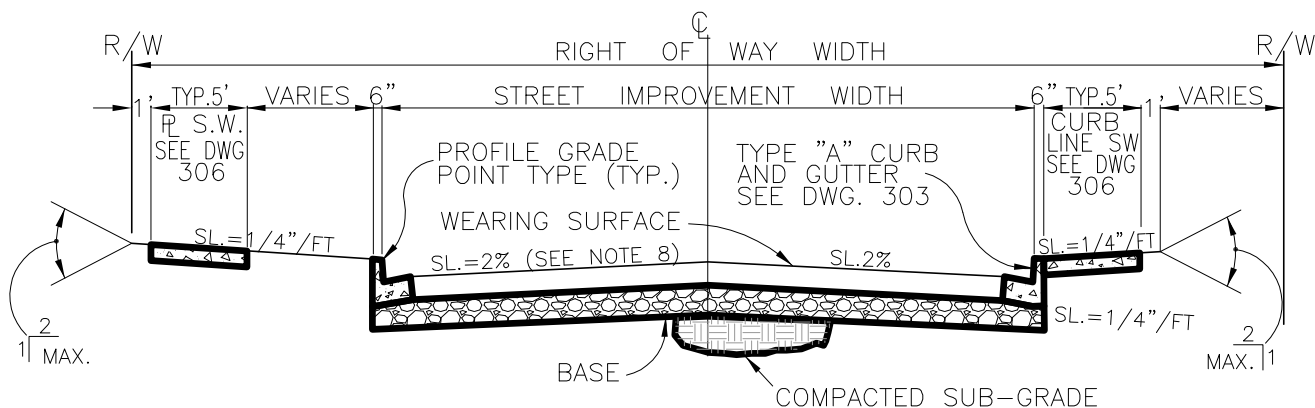
STANDARD PLAN
CONCRETE STAIRWAY

DRAWN BY GS

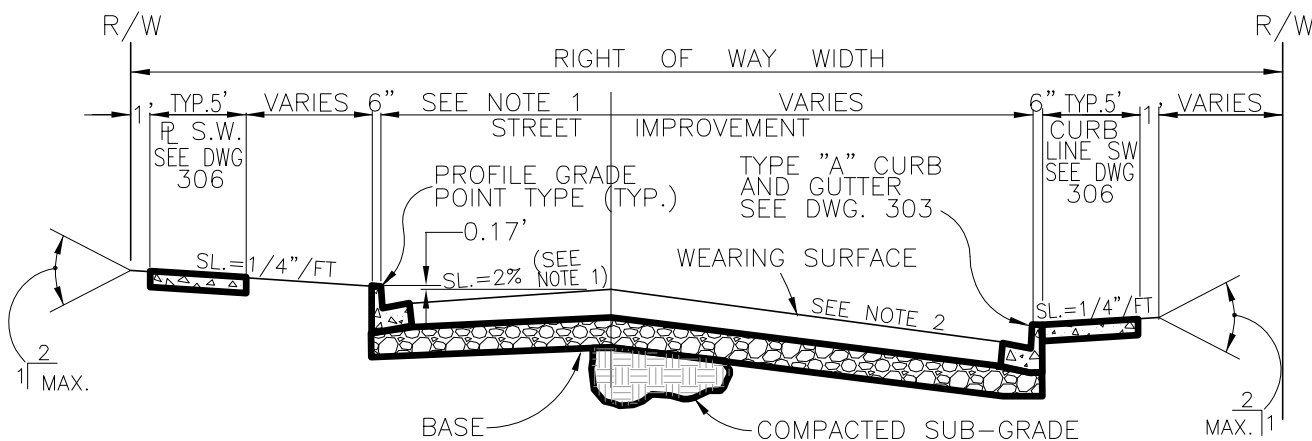
CHECKED BY RWL

NO.317

No.	Description	Date	By	Appr
	UPDATE PER U.B.C.			
	CONVERT TO CAD DWG.			
REVISION				



**TYPICAL STREET SECTION
CROWN SECTION**



**TYPICAL STREET SECTION
TILT SECTION**

NOTES:

1. 0 TO 0.3' TILT HAS A CROWN. 0.3' TO 1' TILT, AS DRAWN, HAS A CROWN THAT IS 0.17' BELOW AND 12' FROM HIGH CURB.
2. MAXIMUM SLOPE: RESIDENTIAL STREET = 6% MAXIMUM SLOPE; COLLECTOR & ARTERIAL = 4%.
3. ALTERNATE DESIGNS TO BE APPROVED BY THE DIRECTOR OF PUBLIC WORKS.
4. FOR STANDARD DRIVEWAY DETAILS SEE STD. PLAN NO.S 301 AND 302.
5. SIDEWALK LOCATION SHALL BE AS APPROVED BY THE DIRECTOR OF PUBLIC WORKS.
6. FOR HIGH SPEED ARTERIAL & COLLECTOR STREETS, DESIGN CROWN TO CORRESPOND WITH LANE CONFIGURATION.
7. FOR CLASSIFICATION (LOCAL, COLLECTOR, ARTERIAL) SEE SALEM TRANSPORTATION SYSTEM PLAN OR ADOPTED SECTOR PLANS.
8. FOR STREET WIDTH IN EXCESS OF 34', CROSS SLOPE SHALL BE A MINIMUM OF 3%.

Approved Karl O. Guster 9-15-99
City Engineer Date

CITY OF SALEM
DEPARTMENT OF PUBLIC WORKS

STANDARD PLAN
TYPICAL STREET SECTIONS

2	REVISED NOTES	6-99	I.D.F.	
1	CONVERT TO CAD DWG.			
No.	Description	Date	By	Appr
REVISION				

DRAWN BY GS
CHECKED BY RWL

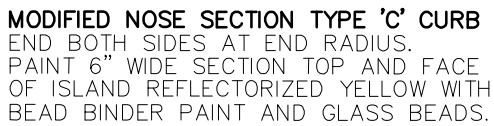
NO.318



NO.319

NO.320

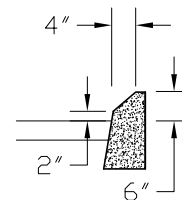
4" THICK, 3000 PSI
CONCRETE OVER 3/4"-0
AGGREGATE BASE



DETAILS

Diagram of a flagpole with dimensions. The pole has a base, a section with 5 black and white stripes, and a top section. Dimensions are given in inches: 4 inches for the top section, 2 inches for the first stripe section, 3 inches for the second stripe section, and 4 inches for the third stripe section.

POSTS SHALL BE ROUND 42" TALL,
BRIGHT YELLOW WITH FOUR 3M HIGH
INTENSITY REFLECTIVE BANDS



MODIFY THE NOSE OF THE ISLAND
SECTION AS SHOWN FOR BOTH
TYPE 'C' CURB AND EXTRUDED
ISLANDS GREATER THAN 24" WIDE.

1. Prior to construction the Engineer shall submit a proposed landscaping plan and cross section for approval by Public Works.
2. Ground cover landscaping requires a minimum of 8" of planting soil mix bedding above either natural subgrade or imported topsoil compacted at 80% over natural subgrade.
3. Tree landscaping requires a bedding of either natural subgrade or imported topsoil compacted at 80% over natural subgrade.
4. Match the existing cross slope of the roadway at each end of the island. Entire length of landscaped island section to be at 8.33% slope from top of curb grade.

Karl O. Gruen
City Engineer

Date _____

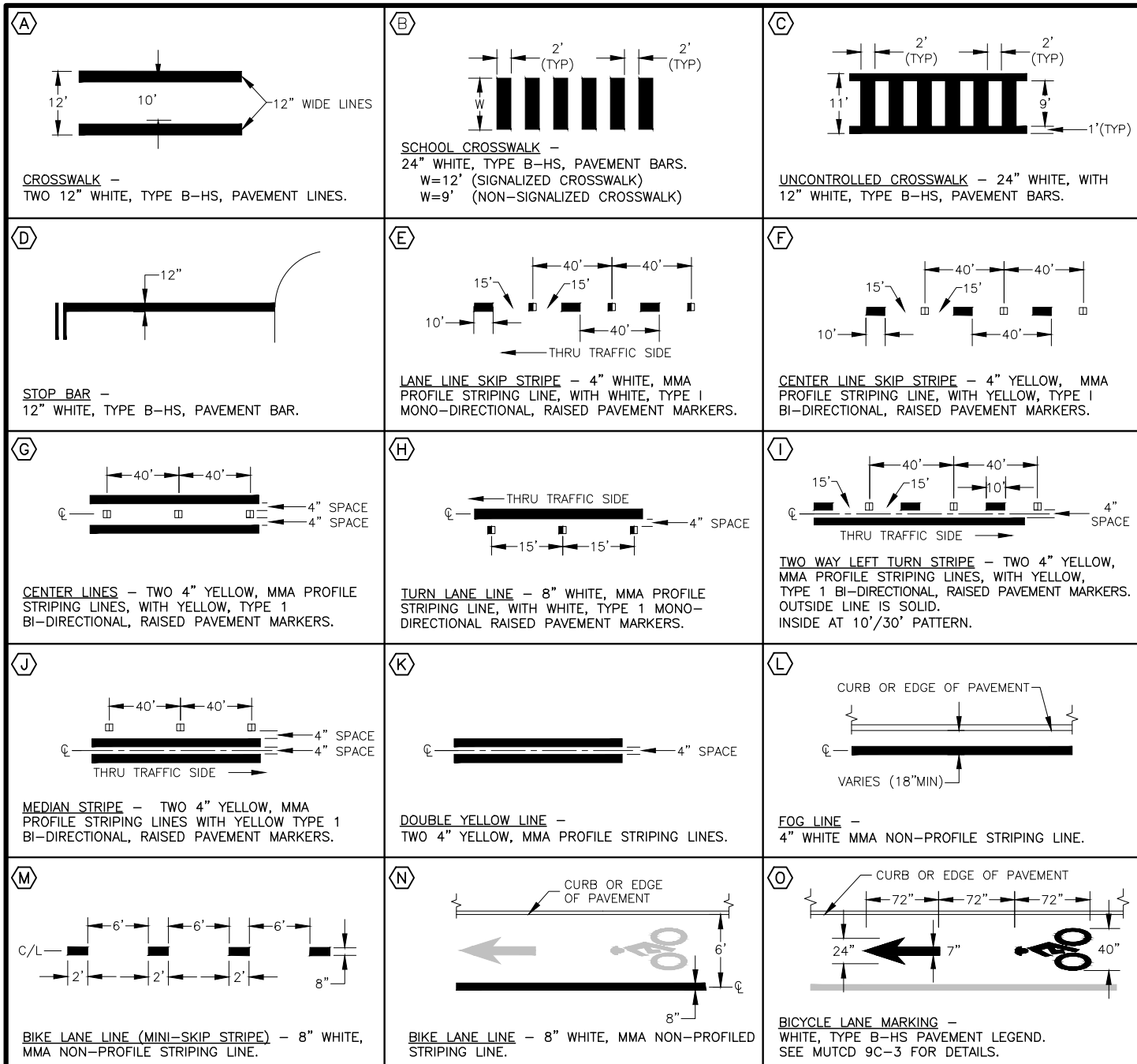
STANDARD PLAN

TRAFFIC ISLAND INSTALLATION GREATER THAN 60" WIDE

CHECKED BY L.H.G

NO.321

No.	Description	Date	By	Appr.
REVISION				

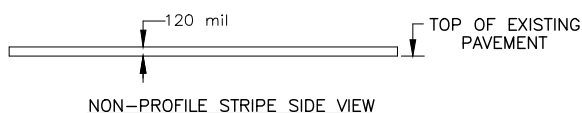
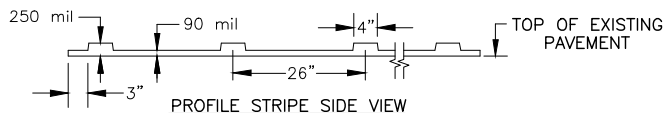


LEGEND

TYPE B-HS PAVEMENT MARKINGS - PREFORMED, FUSED THERMOPLASTIC FILM THAT HAS INTERMIXED REFLECTIVE ELEMENTS WITH FACTORY INSTALLED CRUSHED GLASS OR AGGREGATE ON THE SURFACE.

MMA PAVEMENT MARKINGS

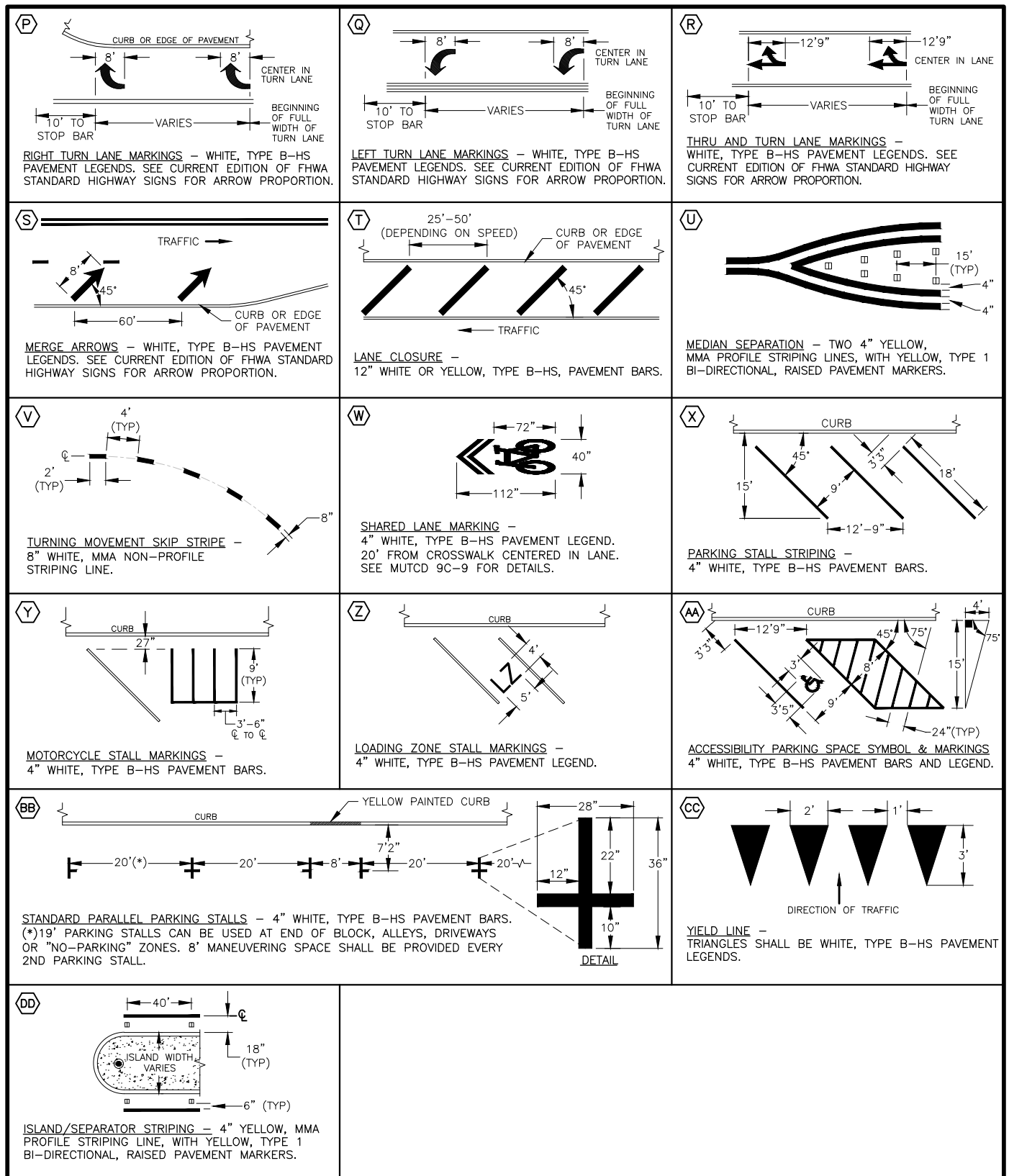
METHYL METHACRYLATE BY GRAVITY AND EXTRUSION METHOD, TO FULL WIDTH SHOWN, IN A SINGLE APPLICATION. PAVEMENT MARKINGS SHALL BE INTERMIXED REFLECTIVE ELEMENTS AND PLACED TO APPLICABLE THICKNESS SHOWN BELOW.



NOTE:

1. ALL PAVEMENT MARKING DESIGNS AND INSTALLATION SHALL MEET OR EXCEED THE SPECIFICATIONS CONTAINED IN THE LATEST EDITION OF THE OREGON DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION INCLUDING ANY SUPPLEMENTAL GUIDES REFERENCED OR SPECIFIED AND ALL SPECIAL PROVISIONS AND ADDENDUMS TO THESE SPECIFICATIONS.

CHANGES	UPDATE BICYCLE LANE MARKING & MINI SKIP STRIPE TO 2009 MUTCD. 9/2019		CITY OF SALEM		
			DEPARTMENT OF PUBLIC WORKS		
			STANDARD PLAN		
PAVEMENT MARKING DETAILS			NO.322A		
APPROVED	 CITY ENGINEER	12/27/19	DRAWN BY	JAK	10/2019
		DATE	CHECKED BY	KDH	10/2019



LEGEND

SEE SHEET 322A FOR STANDARD NOTES AND LEGEND.

CITY OF SALEM DEPARTMENT OF PUBLIC WORKS

STANDARD PLAN

PAVEMENT MARKING DETAILS

CHANGES

NEW DRAWING

APPROVED

CITY ENGINEER

7/13/16

DATE

DRAWN BY

CHECKED BY

DTN

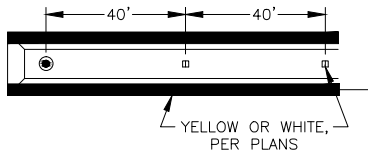
KDH

7/2016

7/2016

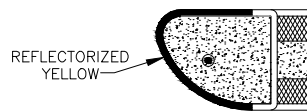
NO.322B

EE



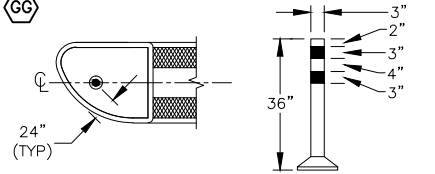
TRAFFIC SEPARATOR AND TRANSITIONS PAINTING –
REFLECTORIZED PAINT LINES, WITH TYPE 1
BI-DIRECTIONAL, RAISED PAVEMENT MARKERS.

FF



ISLAND PAINTING
WHEN WIDTH IS GREATER THAN 24" –
PAINT THE RAISED MEDIAN NOSE CURB
REFLECTORIZED YELLOW.

GG



TUBULAR MARKERS – POSTS SHALL BE ROUND,
TYPE 3 SURFACE MOUNTED, BRIGHT YELLOW WITH
TWO HIGH INTENSITY REFLECTIVE BANDS.

LEGEND

SEE SHEET 322A FOR STANDARD NOTES AND LEGEND.

**CITY OF SALEM
DEPARTMENT OF PUBLIC WORKS**

STANDARD PLAN

PAVEMENT MARKING DETAILS

CHANGES

NEW DRAWING

APPROVED

CITY ENGINEER

7/13/16

DATE

DRAWN BY

CHECKED BY

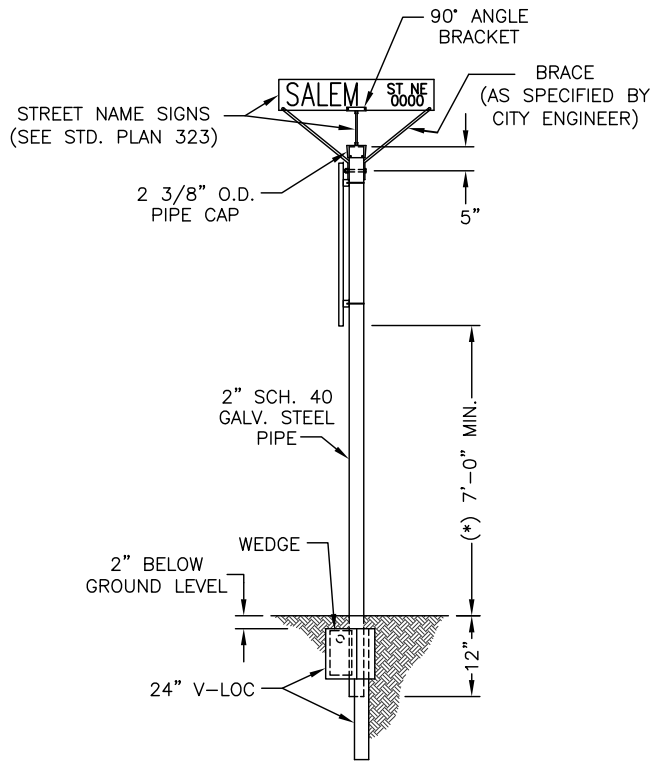
DTN

KDH

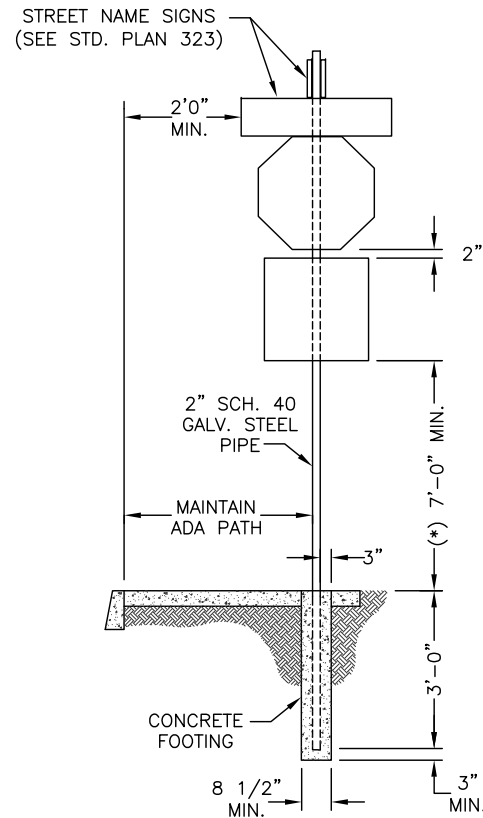
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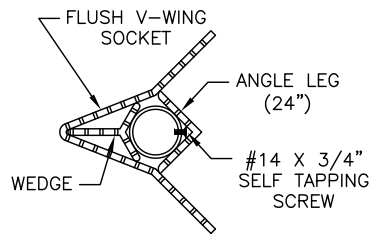
NO.322C



PIPE SIGN SUPPORT DETAIL (SOFTSCAPE)



PIPE SIGN SUPPORT DETAIL (HARDSCAPE)



POST MOUNTING SOCKET

NOTES:

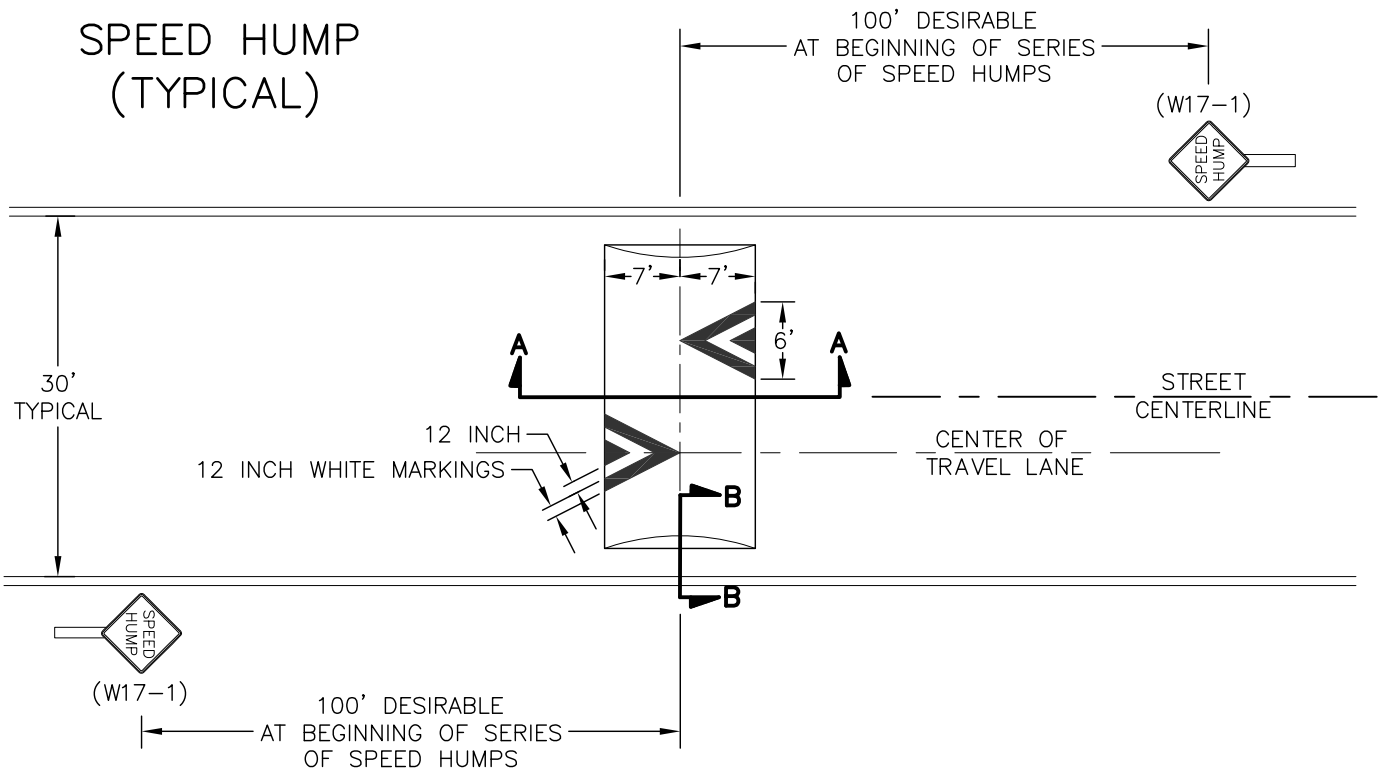
1. HOT DIP GALVANIZE AFTER FABRICATION
STANDARD 2" WELDED STEEL PIPE CONFORM
TO THE ASTM 'SPECIFICATIONS FOR WELDED
STEEL PIPE' A120 AND A123 FOR GALVANIZING.
 2. ALL PIPE SHALL BE CAPPED AS APPROVED BY
ENGINEER WHEN STREET NAME SIGNS ARE NOT
REQUIRED.
 3. MOUNT PIPE IN V-LOC ANCHOR. (V-LOC IS A
METAL SLEEVE ANCHOR SYSTEM TO SUPPORT
THE POST).
 4. INSTALL V-LOC WITH TOP 2" BELOW GROUND
LEVEL.
 5. V-LOC SHALL BE TAMPED IN FIRMLY FOR
SUPPORT.
 6. USE OF BOLTED FLANGE CONNECTION ONLY BY
APPROVAL OF THE CITY ENGINEER.
 7. BREAKAWAY DEVICES REQUIRED IN RAISED
ISLAND AREAS.
- * 7'-0" IS MINIMUM HEIGHT TO BOTTOM OF
LOWER SIGN IN URBAN AREAS.
8'-0" IS MINIMUM HEIGHT WHEN SIGNS ARE
PLACED ABOVE A BIKE PATH.

Approved *James L. Smith* 9/12/11
City Engineer Date

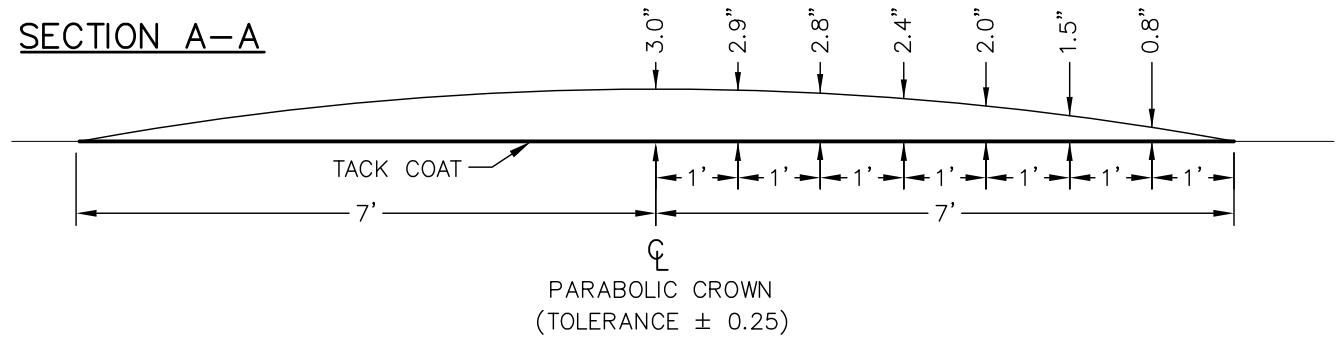
No.	Description	Date
	REVISION	

CITY OF SALEM DEPARTMENT OF PUBLIC WORKS	
STANDARD PLAN PIPE SIGN SUPPORT DETAIL	
DRAWN BY DTN	NO.323
CHECKED BY BAV	

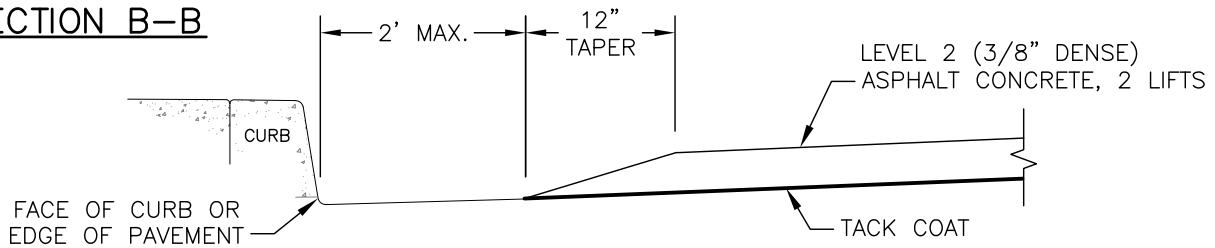
SPEED HUMP (TYPICAL)



SECTION A-A



SECTION B-B



NOTES:

1. WHITE PAVEMENT MARKINGS TO BE PREFORMED THERMOPLASTIC.
2. MAXIMUM STREET GRADE MUST BE LESS THAN 8%.
3. USE ONLY ON LOCAL STREETS THAT MEET NEIGHBORHOOD TRAFFIC MANAGEMENT PROGRAM REQUIREMENTS.
4. MINIMUM OF 2 SPEED HUMPS PER STREET.

Approved

James B. Smith
City Engineer

9/12/11
Date

CITY OF SALEM
DEPARTMENT OF PUBLIC WORKS

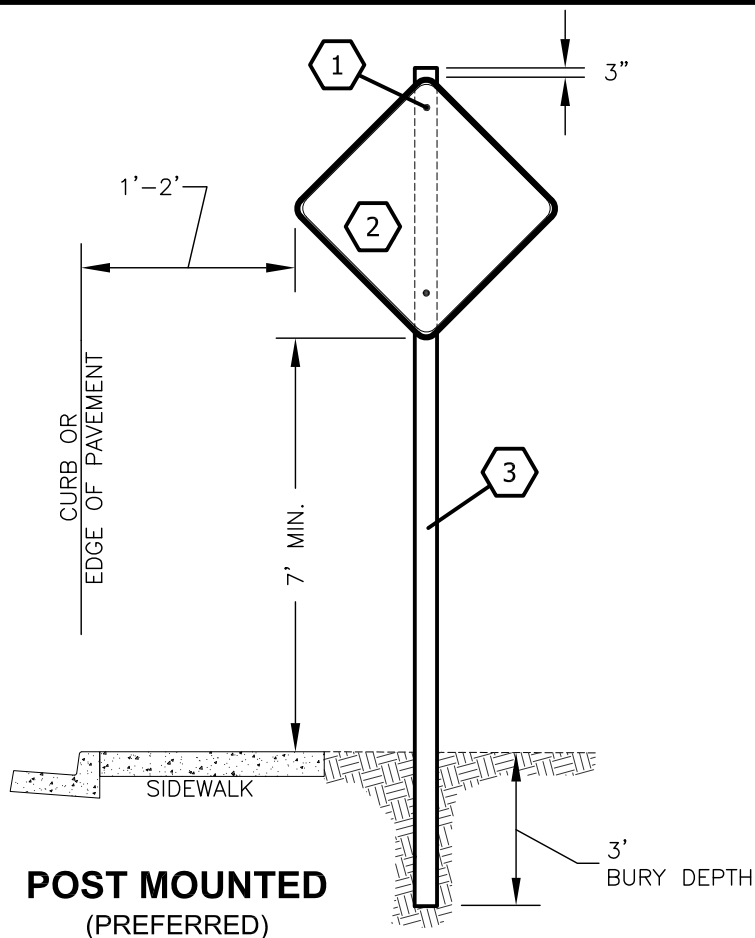
STANDARD PLAN
SPEED HUMP AND STRIPING

No.	Description	Date
	REVISION	

DRAWN BY DTN

CHECKED BY BAV

NO.325

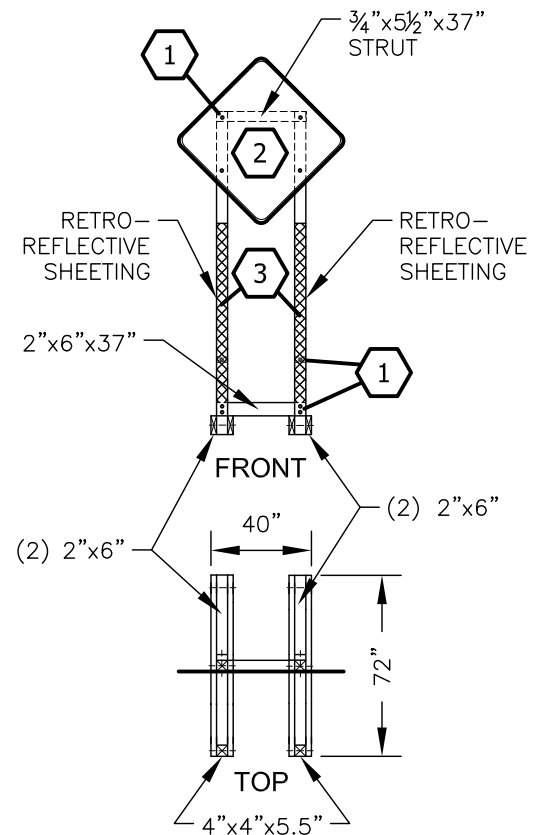
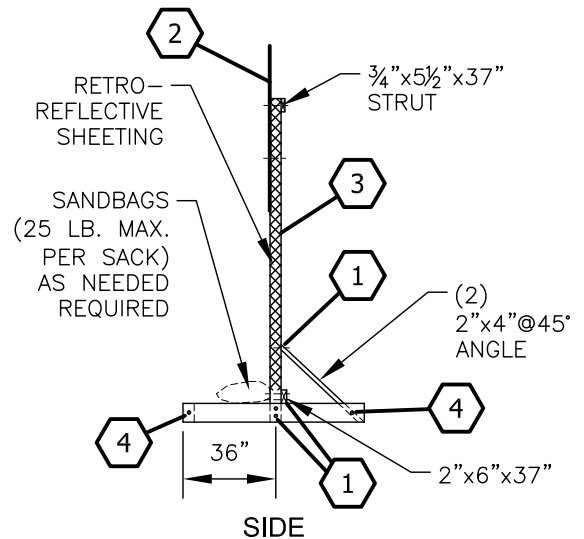


KEYNOTES

1. INSTALL WITH (2) $\frac{5}{16}$ " x 2 $\frac{1}{2}$ " LAG BOLTS MINIMUM PER SIGN.
2. 30"x30" SIGN TYPICAL (CITY)
36"x36" ARTERIAL COLLECTOR
3. 4"x4"x12'
4. (1) $\frac{3}{8}$ " x 3 LAG SCREW PER SIDE

NOTES:

- DOUBLE POST (TSS) SHALL ONLY BE USED WITH PERMISSION OF THE PROJECT MANAGER.
- WHEN NOT IN USE DO NOT TIP OVER (TSS). TURN AWAY FROM TRAFFIC OR COVER SIGN.
- SIGNS SHALL COMPLY WITH AMERICAN TRAFFIC SAFETY SERVICE ASSOCIATION ATTSSA QUALITY GUIDELINES FOR TRAFFIC CONTROL DEVICES.
- DO NOT PLACE OR STACK SANDBAG MORE THAN 24" ABOVE THE GROUND.



DOUBLE POST (TSS)
(BY PERMISSION ONLY)

GENERAL NOTE:

AVOID LOCATING SIGN SUPPORTS IN AREAS DESIGNATED FOR BICYCLE OR PEDESTRIAN TRAFFIC. WHEN (TSS)'S OR POST MOUNTED SIGNS ARE LOCATED ON A SIDEWALK OR BICYCLE FACILITY, INSTALL SECONDARY SIGN (RIDER) AT A MINIMUM HEIGHT OF 7' FROM TOP OF SIDEWALK OR BICYCLE FACILITY TO BOTTOM OF RIDER.

CITY OF SALEM DEPARTMENT OF PUBLIC WORKS

STANDARD PLAN TEMPORARY SIGN SUPPORTS

APPROVED

Kenneth Kuemzi
CITY ENGINEER AIC

8/16/17

DATE

DRAWN BY

JAK

8/2017

CHECKED BY

JPJ

8/2017

NO.326